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11/29/93

DGA91-006
Redmond Downtown Plan

ORDINANCE NO. 1756

ORIGINAL

AN ORDINANCE OF THE CITY OF REDMOND, WASHINGTON, ADOPTING THE DOWNTOWN PLAN, DGA 91-006 AND AMENDING, ADDING AND REPEALING CERTAIN SECTIONS OF THE REDMOND MUNICIPAL CODE AND COMMUNITY DEVELOPMENT GUIDE IN ORDER TO IMPLEMENT THE DOWNTOWN PLAN.

WHEREAS, the City Staff, the Planning Commission, and the City Council have conducted an extensive study of the Redmond City Center in order to determine whether the City's land use regulations can be revised to create an economically vital and pedestrian-friendly retail, office, entertainment and residential neighborhood in the City's downtown, and

WHEREAS, the Planning Commission and City Council have each held public hearings concerning proposed revisions to the current goals, policies, zoning designations, development and land use regulations, transportation and park systems as they relate to the City Center neighborhood, and

WHEREAS, the City Council has determined to implement the Downtown Plan through the adoption of the goals, policies and regulations set forth in this ordinance, NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF REDMOND, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. Downtown Plan Adopted. The City Council hereby approves those amendments, additions, and deletions to and from the Redmond Municipal Code and Community Development Guide as

set forth on the attached Exhibits B through K, otherwise known as the City of Redmond Downtown Plan, DGA 91-006. The Redmond Municipal Code and Community Development Guide are hereby amended, added to, and repealed from as provided in said Plan and exhibits. Exhibits B through K are attached to this ordinance and incorporated herein by this reference as if set forth in full.

Section 2. Severability. If any section, sentence, clause or phrase of this ordinance should be held to be invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity or constitutionality of any other section, sentence, clause or phrase of this ordinance.

Section 3. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum, and shall take effect five (5) days after passage and publication of an approved summary thereof consisting of the title.

CITY OF REDMOND


MAYOR ROSEMARIE IVES

Mayor Pro tempore, Warren Appleton

ATTEST/AUTHENTICATED:


CITY CLERK, DORIS SCHAIBLE

Ordinance No. 1756

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY: 

FILED WITH THE CITY CLERK:	11/29/93
PASSED BY THE CITY COUNCIL:	12/7/93
SIGNED BY THE MAYOR:	12/7/93
PUBLISHED:	12/12/93
EFFECTIVE DATE:	12/17/93
ORDINANCE NO. <u>1756</u>	

CITY CLERK COPY
DO NOT REMOVE

DRAFT

DOWNTOWN PLAN

(REVISED)

November 23, 1993

Amended and Adopted
12/7/93

by
CITY COUNCIL
EFFECTIVE
12/21/93

DOWNTOWN PLAN 11/19/93

20B.85

20B.85.130 CITY CENTER

~~The City Center is roughly bounded by Education Hill and Bear Creek on the north, Bear Creek on the south and east, and the Sammamish River on the west. The intent of the City Center Plan is to blend Redmond's natural setting with economic activities to create a fiscally healthy, unique and attractive downtown area. It is important that the City Center be the focus of the community and as such the Plan be a statement about the community's aesthetic, functional and cultural aspirations.~~

~~FUNCTION: The long term economic vitality of the City Center is critical to the aesthetic as well as functional objectives of Redmond. It must not only be an employment center, but must be a lively and safe place to be. The plan provides for a variety of uses, and clusters them in complimentary groupings. This is intended to consolidate and promote a higher density and concentrated retail core with increased activity. This concentrated retail grouping is designed to create a focus to the City Center.~~

~~DESIGN AND AESTHETICS: It is the intent of the plan to retain key natural features, and large open spaces such as the Sammamish River, Bear Creek and park areas. This would be achieved through a landscaping program based on the concept of the downtown as an urban park designed to support a commercial and employment center of regional scale. This concept will be implemented by "soft" or informal landscaped areas on the edges of the City Center which act as transitions to the adjacent neighborhoods and Marymoor Park.~~

~~Walkways located throughout the downtown will be designed to connect to these edges and to link both formal and informal gathering places within the commercial area. These edges should be designed to provide a sense of entry to this area. Walkways and other landscaped areas should emphasize the use of native plant materials to help retain the sense of this area as a valley floor when viewed from the adjacent hillsides.~~

~~Consistent with this image the bicycle theme should be strengthened and expanded on. Bicyclists will be encouraged to enter into the downtown area from the Sammamish River Trail.~~

The City Center neighborhood contains Redmond's downtown, the future and historic center of

employment and entertainment, shopping, and recreation for the City and the surrounding community. Redmond's policies and programs for the City Center neighborhood promote economic activities and community development which create an economically vital, attractive, and pedestrian oriented downtown area. The downtown, designated as the City Center neighborhood, is bounded by the Sammamish River on the west, Bear Creek on the south and east, and Education Hill and N.E. 90th Street to the North.

FUNCTIONS AND ACTIVITIES:

Redmond's downtown is intended to remain the major central place in the City, providing community identity through activities and aesthetics. The City's vision for downtown relies upon long term economic vitality and growth to provide a variety of retail, office and residential uses. The City's vision for downtown is based on community workshops and forums which indicate strong agreement that downtown Redmond should be:

- *a city center which meets residents' needs for employment, shopping, recreation, cultural and nightlife opportunities and community activities;*
- *a place oriented to pedestrians and bicycles, with through traffic routed around the downtown;*
- *an urban area enhanced by the rich natural setting, emphasized through open space, landscaping and a focus on the Sammamish River and trail.*

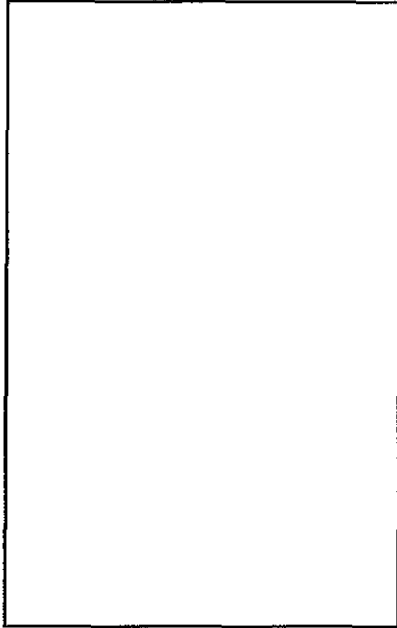
LAND USE AND URBAN DESIGN:

The City Center plan provides for land uses clustered in complimentary groupings, including:

- *A concentrated retail core centered around Leary Way and Cleveland Street will accommodate significantly increased comparison shopping opportunities in a pedestrian-friendly environment;*
- *A mid-rise office district in the center of downtown, stretching from the Sammamish River to Anderson Park, will accommodate employment growth in professional and business services as well as complimentary retail and residential uses;*
- *Residential districts at the periphery of the downtown will provide for high quality living*

environments within walking distance of jobs, shopping and the regional transit system;

- Low rise retail districts at three entrances to the downtown will provide for everyday shopping needs such as groceries, pharmacies, and other convenience retail goods and services.



While each activity grouping has a primary emphasis on either retail, office or residential, mixed uses are also allowed and encouraged, with design guidelines to assure compatibility with the primary intended use.

The Plan recognizes the downtown as a single neighborhood, less than one square mile in size, which contains a series of sub-neighborhoods or districts. These individual districts will continue to develop as distinctly different places within the downtown, characterized by different heights, building design, land uses, with an emphasis on distinctive entrance corridors, street patterns, roadway designs, landscaping and amenities. This variety at a relatively small scale maintains a legible place, a human scale, and diversity which draws people to the distinct districts.

TRANSPORTATION:

A variety of transportation choices will significantly increase mobility to and from and within downtown Redmond, with continued reliance on automobile travel, but increased reliance and opportunities for pedestrian travel or travel by transit and bicycles. The transportation improvements envisioned for

Downtown also provide mobility between all activity nodes within Downtown. For example, office workers will find an easy path to shopping or the river; residents can walk less than 1/2 mile to stores, work, parks and trails.

The balance of retail, office and residential land uses downtown contribute to travel by foot and bike, as well as increased transit service. Specific improvements envisioned by the plan include:

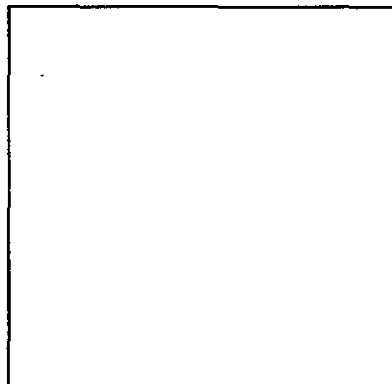
- bicycle lanes for commuters and to link downtown with the Sammamish River Trail will be added as the city develops.

- the system of sidewalks and mid-block pedestrian routes will be completed.

- urban design will invite pedestrian and bicycle travel. Future development will create visual interest and convenience by siting buildings close to the sidewalk; trees, bollards, and parking lanes will separate pedestrians from traffic; slower traffic, frequent pedestrian crossings, and amenities such as benches, awnings and pedestrian scale lighting will create a pedestrian-friendly environment.

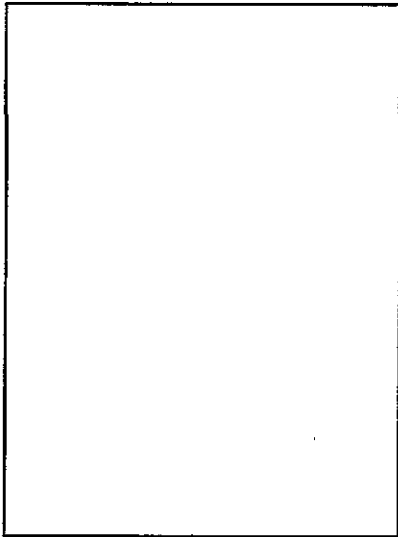
- increased transit service will be promoted, with a regional transit station in Old Town. Transit service will include high speed links to other urban centers and local service linking neighborhoods to downtown and the transit center. Park and ride lots on the outskirts of the city will replace the downtown lot, to intercept auto trips; and high occupancy vehicle lanes will increase mobility for transit and carpools.

- measures to calm traffic on the local street system, as well as completion of the long-planned bypass roads will restore safe speeds for downtown users while allowing through traffic to bypass the downtown. A return to the two-way system ~~may~~ ^{will} be explored after bypass roads are funded.



PARKS AND AMENITIES:

The Downtown Plan relies upon large open spaces such as the Sammamish River, and Bear Creek, as well as abundant landscaping and small parks to create a sense of downtown as an urban park set in a rich, natural environment. Street trees and parks in the center of town complement and soften the hardscape, while informal landscaped areas on the edges of downtown serve as transitions to the adjacent neighborhoods and Marymoor Park. Walkways, trails and vista points throughout the downtown enable people to enjoy the natural beauty of the river, the valley floor and views of surrounding hillsides and mountains.



Parks and plazas will become increasingly important as the downtown develops, providing openings to contrast with the enclosure provided by three to eight story buildings set on the street front. The two rivers, which surround and define edges of downtown, will be a major emphasis of park and trail improvements. Major parks already planned for Bear Creek and the Old Golf Course will be enhanced with new park and trail improvements essential to the vision for downtown. The vision for downtown parks includes:

* ● a cohesive system of parks, activity nodes such as public plazas and gathering places, and linking paths between the Sammamish River, other parks and points of interest;

● enhancements to the Sammamish River, such as bays and pools to return the river to a more natural shape; access to the river in controlled locations to limit bank degradation while also providing human enjoyment of the water; and

increased vegetation to enhance its fisheries, wildlife and beauty;

● additions to the Sammamish River Park to provide more amenities and resting places for users and more links with the city, including bridges across the river; new parks on both sides near the trestle; separation of high speed bicyclists and equestrian uses from slow speed walkers; and plazas or activity nodes that allow users to stop, rest and enjoy;

● a large, urban central park near the Sammamish River, to allow more interaction between the two parks, and draw activity between the two uses;

● a sculpture garden at the Dudley Carter house;

● a botanical garden and trails on the west side of the Sammamish River, and other viewpoints, plazas and trails throughout the downtown to invite outdoor activity and enjoyment of the City's natural features.

IMPLEMENTATION

The Plan requires public and private actions to create the future, including private developments that provide for desired activities and quality design, and public investments to beautify city facilities, attract walkers and bikers, and encourage activity downtown.

Cultural events and public art, which can draw people to the heart of the City, will be emphasized through public programs and location of civic facilities. Civic places and facilities, such as a community center, cultural arts facilities, the Redmond Saturday Market, the transit station and similar attractions are important to meet the recreation and entertainment goals of city residents, as well as attracting people to the downtown.

City actions designed to encourage private investments include improving the transportation system and adding amenities. The Plan includes options for flexibility in most regulations to invite and allow creative proposals. Policies also support the operation and expansion of existing businesses, regardless of changes in permitted land use. An administrative policy of responsiveness and flexibility will be fostered, to form partnerships that help achieve the City's vision for downtown.

CITY CENTER GOALS AND POLICIES:

85.130(10) City Center Goal - Promote the development of the City Center Downtown Redmond as the primary commercial activity center and destination location for the City and greater Redmond area.

(a) **Policy** - Provide a location for a variety of office, service, retail, shopping and living residential, and cultural opportunities.

(ab) **Policy** - Encourage and promote the City Center as a location for both large and small business which are compatible with the long term economic health of the community.

(bc) **Policy** - Encourage the development of a major comparison center area in downtown Redmond which would expand business opportunities, provide additional revenue and be a positive influence in the development of the City.

(cd) **Policy** - Require comparison shopping center proposals to integrate the shopping facilities visually, functionally, and through pedestrian connections into the City Center.

(ce) **Policy** - Encourage the development of housing in and adjacent to the City Center to support business activities and increase the vitality of the area.

(f) **Policy** - Encourage a mix of housing choices to create variety in residential opportunity and maintain a jobs/housing balance within the downtown neighborhood, to make the downtown a "people place" in the early morning, daytime and evening hours.

(eg) **Policy** - Encourage clustering of associated business uses to reduce conflict among uses and to increase convenience for businesses and users by as follows:

■ Encourage comparison retail and associated uses at the center of the downtown area to encourage promote development of a concentrated shopping area which acts provides a wide variety of goods and services, and serves as an active, people-oriented focus to the downtown and City.

■ Provide for convenience retail centers at entrances to Downtown to provide for grocery stores and other convenience stores while reducing the need for through traffic.

■ Encourage residential districts on the periphery of Downtown to provide housing near jobs, services and transit. +

■ Encourage pedestrian retail and associated uses at the center of the downtown area to encourage development of a concentrated shopping area which acts as an active, healthy downtown and city.

■ Encourage development of concentrations of office uses and support services located adjacent to the retail core. +

■ Allow mixed use throughout Downtown commercial areas with guidelines to assure compatibility with the primary business use. +

85.130(10) City Center Goal - Encourage development of a unique, attractive and economically healthy downtown that reflects Redmond's history and natural setting and offers a variety of service, office, retail, residential, cultural and recreational opportunities.

85.130(05) City Center Goal - Utilize Redmond's history, natural setting and growth opportunities to develop a unique, attractive and economically healthy City Center offering a variety of service, office, retail shopping, living, cultural and recreational opportunities.

(ea) **Policy** - Encourage City Center Encourage development of downtown Redmond as the focus of retail, office and service uses for the Redmond Planning Area. *

(ab) **Policy** - The City Center Neighborhood is designated the an activity urban center under the Countywide Planning Policies and classified as a secondary metropolitan center for the purpose of implementing the King County Subregional Plan in Vision 2020, thereby establishing the downtown as a priority areas for the concentration of employment and commercial growth and the allocation of transportation funding.

(bc) **Discourage expansion** Retain existing of City Center boundaries and encourage redevelopment and infill within those boundaries.

(d) Encourage office and retail developments that increase public enjoyment of the rivers and other natural features.

(e) *Encourage retention of historic buildings and businesses of community significance, through incentives, flexible regulations, and administrative procedures.*

85.130(15) City Center Goal - Provide incentives to encourage private investment and business vitality in downtown Redmond.

(a) Policy - *Apply flexible regulations that encourage creative proposals consistent with City Center design goals.*

(b) Policy - *Favorably consider land use changes or plan amendments which allow projects consistent with the vision for City Center.*

(c) Policy - *Develop and implement downtown facility plans that provide adequate infrastructure for desired growth.*

(d) Policy - *Encourage public and private sector cooperation in providing capital investment such as parking, and street improvements which contribute to retaining existing businesses and encouraging new business to locate in the City Center.*

(e) Policy - *Initiate a capital improvement strategy to implement pedestrian improvements, beautification projects, parks and civic facilities in the downtown.*

85.130(20) City Center Goal - Increase transportation mobility within the downtown and provide for convenient transit, pedestrian and bicycle routes to and from Downtown.

(ha) Policy - *Encourage commuter traffic to use bypass opportunities which will reduce traffic on City Center streets and allow easy access to businesses.*

(hb) Policy - *Encourage City Center employees to use transit, car pools, bicycles and other forms of transportation which reduce congestion and free up space for customer parking.*

(hc) Policy - *Encourage shared, clustered parking to reduce the total number of overall parking stalls needed and increase the economic and aesthetic potential of the area.*

85.130(2520) City Center Goal - Foster Redmond's image as Bicycle Capital of the Northwest.

(a) Policy - *Provide bicycle facilities such as connections to the Sammamish River Trail, bicycle*

racks in conjunction with new developments, bike lanes on key streets and signage at key points.

(b) Policy - *Encourage bicycle races and other cycling related activities in the City Center.*

85.130(1525) City Center Goal - Encourage public and private development to retain and enhance the natural features in the City Center area Downtown Redmond to create a unique and attractive urban park commercial park setting which will establish the image and character for development throughout the City.

(a) Policy - *Encourage landscaping plans which can thrive in urban settings, conserve water, are informal, retain desirable trees and use native plant materials.*

(b) Policy - *Encourage informally designed landscaping and open space on the edges of City Center to act as a transition to adjacent neighborhoods and Marymoor Park.*

(c) Policy - *Special architectural and landscaping features should be developed at all major entrances to the City Center.*

(d) Policy - *Development adjacent to the Sammamish River, Bear Creek and other park areas should be designed to complement and enhance these areas by such techniques as:*

■ *Fronting buildings and associated entrances on the park.*

■ *Providing convenient public access to the park.*

■ *Complementing the park with additional landscaping, plazas and other pedestrian features.*

■ *Avoid locating parking lots, auto-oriented signing and service areas toward the park space.*

■ *Use creative design concepts and construction methods to protect natural features and systems while integrating uses which benefit from special natural features.*

(e) Policy - *Use landscaping and other forms of screening to soften the appearance of parking lots, backs of buildings and service areas from public streets, walkways and adjacent hillsides.*

(f) Policy - Existing parks in the City Center Neighborhood should be retained and enhanced. ~~Opportunities to acquire additional parks and open space shall be~~ *New parks should be added along the river, in the center of the office district and at viewpoints east of downtown, consistent with Chapter 20B.50 PARKS, RECREATION AND OPEN SPACE of the Community Development Guide.*

(g) Policy - *Work with King County to develop park, trail, and bike plans for the Sammamish River that serve both City residents and users from the broader region.*

85.130(2030) City Center Goal - Encourage the development of the City Center as the cultural focus of the greater Redmond area.

(a) Policy - Encourage the development of a performing arts center *and a community center* in the City Center.

(b) Policy - Encourage the development of public art features with all development.

(c) Policy - Create a program which locates public art features in key locations in the City Center.

(d) Policy - *Provide programs which foster the arts and attract people to the Old Town and Mixed Use Shopping Center downtown.*

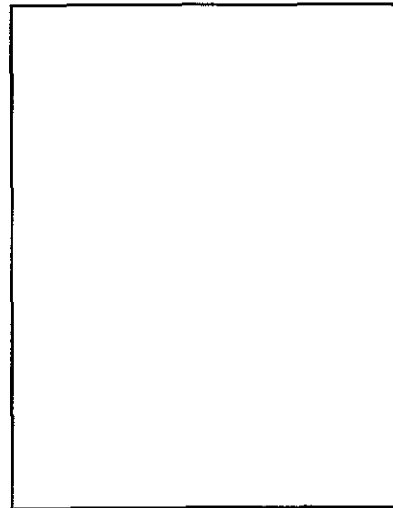
GOALS AND POLICIES FOR RETAIL DISTRICTS:

Comparison Shopping - Old Town and Mixed Use Shopping Design Areas:

Downtown Redmond contains two districts planned for extensive retail activity and comparison shopping: the Old Town Design Area, and the Mixed Use/Shopping Center Design Area. The two districts are adjacent and will be linked through pedestrian and bicycle connections and visual features.

Old Town

85.130(35) Old Town Design Area Goal - *Promote the continued use and development of Old Town as a comparison retail shopping area which retains its historic village character.*



(a) Policy - Actively support economic development measures that serve to retain and promote the growth of *business existing retail business*, and attract new businesses that are compatible with the scale of the design area.

(eb) Policy - Encourage a variety of economic activities *such as stores, restaurants and offices* that will provide for ~~pedestrian~~ *active uses in the area* ~~Old Town~~ during the ~~day~~ *evenings* as well as the ~~evening~~ *daytime*.

(c) Policy - Encourage retention of existing businesses and buildings of special community or historic significance through programs and administrative practice which encourage preservation and reinvestment. *

(bd) Policy - Encourage pedestrian-oriented retail businesses to locate in Old Town.

(ee) Policy - ~~Encourage~~ *Provide* zoning for mixed use developments ~~possibilities~~ which incorporate retail or office uses on the ground floor with service or housing on upper stories. *

(df) Policy - Develop economic incentive programs that will encourage historic redevelopment of structures and provide new business opportunities.

85.130(5040) Pedestrian Design Area Old Town Design Area Goal - Maintain and enhance pedestrian activities in *Old Town* to emphasize the human scale of the area.

(a) Policy - Ensure that public improvements in the area (streets, walkways, lighting and landscaping) complement the pedestrian activity. ~~in the design area~~ *Old Town.*

(b) Policy - Encourage the installation of benches, kiosks and other street furniture which provide a unifying element and aid in developing the historical character and pedestrian scale of the area.

(c) Policy - ~~Encourage the use of financial mechanisms available to fund the installation of~~ Provide pedestrian-related sidewalk improvements and parking lots/structures. (public, private or combination).

(gd) Policy - Work with Burlington Northern Railroad to develop a landscaping and pedestrian treatment for the railroad right-of-way that allows easy access along and across it and reduces its visual impact on the area.

(he) Policy - Encourage the development of a parking program for this area that:

- Minimizes on-site parking to allow development of a compact shopping district.

- Provides for on-street parking.

- Encourages structured parking within or adjacent to this design district.

85.130(5545) Pedestrian Design Area Old Town Design Area Goal - Encourage and promote the pedestrian use of outdoor spaces. ~~in Old Town.~~

(a) Policy - Encourage the use of parks, plazas and other or "people places" in or along the streetscape.

(b) Policy - Create a visual pedestrian features, e.g., fountains, squares, sculptures or other focal points that will draw people to parks or "people places" along the streetscape.

(c) Policy - Encourage strong pedestrian linkages with other existing and potential commercial development in the City Center.

85.130(6050) Pedestrian Design Area Old Town Design Area Goal - Maintain and encourage an attractive and high quality environment for Old Town enhance the traditional downtown character which includes pedestrian oriented store fronts and pedestrian scaled streetscapes through specific attention to architectural detail, components of the streetscape and the relationships between them.

(a) Policy - Maintain height limitations in Old Town that are appropriate to the pedestrian environment.

(b) Policy - Encourage new infill development in Old Town which that is compatible in design and complements the to the historic character of the area.

85.130(65) Pedestrian Design Area (Old Town) Old Town Design Area Goal - Adopt criteria and standards for Old Town which encourage a balance between the need to realize economic viability and the achievement of development complimentary to the historical character and function of Old Town.

(a) Policy - Create a design district overlay zone in Old Town to evaluate new development.

(b) Policy - Develop an Old Town Design Plan to address specific street treatments for gateway areas.

(c) Policy - Develop a sign program which reflects the Old Town character of the area and pedestrian nature of the district.

85.130(55) Pedestrian Design Area Old Town Design Area Goal - Emphasize the natural and manmade features of Old Town and the surrounding area which achieve a balanced, pleasant urban space.

(a) Policy - Encourage Redmond's historic character of architectural styles predominant during the late 19th century and early 20th century.

(b) Policy - Encourage an intensified planting program in Old Town through the use of special median plantings, flower baskets and/or other seasonal plant displays.

85.130(75) Pedestrian Design Area (Old Town) Village West Design Area Goal - Create opportunities for economic development that will promote a healthy business environment in Old Town.

85.130(8060) Pedestrian Design Area (Old Town) Design Area Goal - Encourage the development and redevelopment of the area in a manner which complements the maintains a character and scale of the existing consistent with its role as the historic buildings Old Town center and focal point of the community.

(a) Policy - Use incentives and regulations to Encourage the preservation and restoration of structurally sound historic buildings in a manner which is consistent with the Old Town character of the area.

*

(b) Policy - Encourage public and private sector partnerships for the historic redevelopment of buildings.

(c) Policy - Allow flexibility in zoning and building codes to encourage the preservation and rehabilitation of buildings of historic value.

85.130(8565) Pedestrian Design Area Old Town Design Area Goal - Encourage the balance of function between the need for through traffic opportunities as well as the re-definition of Old Town as a traffic destination and needs for Treat Old Town as a destination which emphasizes pedestrian safety while also accommodating through traffic at safe speeds, to make the design area viable for retail and entertainment uses.

(a) Policy - Avoid 4-lane arterials through Old Town.

(b) Policy - Provide on-street parking.

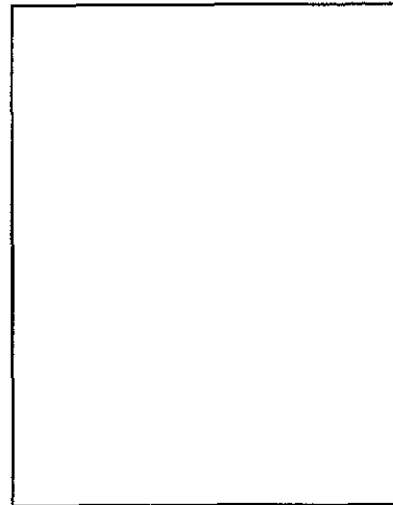
(c) Policy - Integrate public transit, bus stops and information kiosks with streetscape.

(d) Policy - Develop automobile by-pass routes around the downtown core.

(e) Policy - Develop and implement traffic calming circulation designs which protect and encourage pedestrian activities and bicycles.

Mixed Use Shopping Center

85.130(9070) Mixed Use/Shopping Center Design Area Goal - Encourage the development of a regional shopping center/mixed use complex that will provide a focus for the existing downtown and contribute to the social and economic base of the City.



(a) Policy - The development should include a mix of uses such as residential, office, hotel, cinema, transitional retail businesses, open space and recreational activities along with a shopping center.

(b) Policy - This Design Area should have a regional shopping center of 750,000 to 850,000 square feet of gross leasable area.

(c) Policy - This Design Area shall have a maximum buildable area of not more than 1,375,000 square feet.

~~(d) Policy - Require preannexation zoning for the unincorporated portion of the Shopping Center Design Area which includes:~~

- open space/park areas
- street patterns
- building heights and scale
- parking
- landscape areas
- transportation mitigation measures
- environmental mitigation measures
- design criteria
- integration plans with existing City Center

~~(e) Policy - Preannexation zoning should include a conceptual master plan for the entire Shopping Center Design Area.~~

*

~~(f) Policy - Prior to annexation, the City should receive letters of intent to locate in the Mixed Use/Shopping Center Design Area from three major department stores.~~

(gd) Policy - Unless site plan approval is obtained or a complete conforming building permit application is submitted within three years after rezoning, preannexation zoning and master plan approval, such actions shall be subject to review and changes by the City Council.

(he) Policy - Retail uses should be directly linked visually and functionally to other retail uses in the City Center.

(if) Policy - Encourage development of a compact center which minimizes use of land area and generates a high level of pedestrian activity between the center and existing downtown.

(ig) Policy - At least 80% of parking for the shopping center/town square complex should occur in parking structures.

(jh) Policy - Uses in the center should be oriented externally as well as internally by using outward facing building facades, malls, entrances and other design techniques.

(li) Policy - Parking structures should be enclosed with retail or office uses on the exterior or where this enclosure is not feasible the visual impact should be softened with landscaping or screening.

(mj) Policy - Provide for circulation, land use and parking linkages with the existing downtown which will attract or encourage shoppers into the downtown.

(nk) Policy - Minimize parking impacts by requiring structured parking and screening of parking areas.

(ol) Policy - Locate parking facilities in a manner that will reduce large areas of parking and encourage shared parking with existing downtown uses.

(pm) Policy - The Justice White House and other features of historic significance should be preserved. generates a high level of pedestrian activity between the center and existing downtown.

(qn) Policy - Assure that the size and scale of the center are compatible with the scale of the City Center and that economic activities are interrelated.

85.130(9575) Mixed Use/Shopping Center Design Area Goal - Provide transportation facilities that will minimize impacts on the existing system from new development, integrate the area with the City Center and improve traffic circulation.

(a) Policy - Improvements which are necessary to maintain a level-of-service standard of D (average weekday trips) on off-site roadways affected by a regional center should be completed prior to opening of a regional center to mitigate transportation impacts consistent with Chapter 20B.60.030(05) of the Community Development Guide.

(b) Policy - Streets should not be wider than 3 to 4 travel lanes and five lanes at intersections between the design area, and area targeted for integration with the downtown.

(c) Policy - Provide a continuous landscaped pedestrian/bicycle trail system constructed through the design area which is linked with the regional trail system, downtown and Marymoor Park.

(d) Policy - Pedestrian circulation plans should encourage walking within the site and to external locations.

(e) Policy - Incorporate a transportation implementation program in the design area regulations which address bicycle, pedestrian, truck and automobile circulation, including construction traffic.

85.130(10080) Mixed Use/Shopping Center Design Area Goal - Manage and regulate development in a manner that minimizes physical impacts to the natural environment.

(a) Policy - Retain indigenous vegetation, particularly adjacent to Bear Creek and the Sammamish River.

(b) Policy - Preserve existing natural features, particularly mature trees and stream courses.

(c) Policy - Preserve a minimum of 75 percent of all trees within the cluster along Leary Way at the northwest corner of the Design Area.

(d) Policy - Assure that a monitored and maintained storm water disposal system adequately controls runoff, eliminates direct discharge to streams, and removes pollutants prior to discharge, consistent with requirements of Chapter 20E.75 of the Community Development Guide, "Storm Water Management".

85.130(10585) Mixed Use/Shopping Center Design Area Goal - Create opportunities for recreation and leisure activities that complement other uses in the City Center and generate pedestrian activity.

(a) **Policy** - A minimum of 60 acres of recreation open space (including floodway) shall be retained in this Design Area. This is not to include building entryways and miscellaneous building and parking lot landscaping. At least 25 acres in one parcel, exclusive of floodway, shall be maintained and dedicated to the City or controlled by other methods that would permanently assure the open space. This downtown park shall serve as a visual amenity and usable recreation open space.

(b) **Policy** - Encourage development of plazas, pedestrian malls and other amenity open spaces, including a facility for public recreation, that promote outdoor activity and encourage pedestrian circulation between the retail center and the balance of the City Center area.

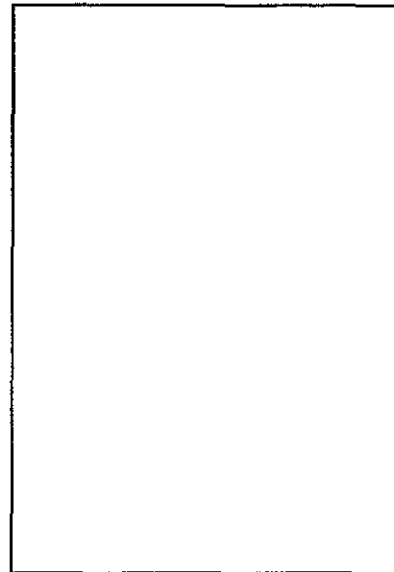
(c) **Policy** - Provide pedestrian and bicycle connections between the existing downtown and Marymoor Park.

(d) **Policy** - Develop a pedestrian circulation plan that encourages walking within the site and to external locations.

~~**85.130(30) North End, West End, and Bear Creek Convenience Commercial Design Areas Goal**~~ - Provide low scale centers for convenience retail business as well as other compatible commercial, service and office uses that require convenient vehicular access and significant parking.

Convenience Commercial - Valley View, Trestle and Bear Creek Design Areas:

85.130(90) Valley View, Trestle, and Bear Creek Convenience Commercial Design Areas Goal - Encourage development and retention of grocery stores, pharmacies, dry cleaners and other convenience retail businesses in downtown Redmond in locations convenient to nearby residential and employment areas.



(a) **Policy** - Provide for development of convenience commercial centers located at entrances to downtown, to provide ready bicycle, pedestrian and vehicular access from both residential and employment areas and reduce the need to travel through the downtown.

(b) **Policy** - Provide for sufficient parking for retail businesses to meet normal parking demand while avoiding excessive paving and underused land.

(c) **Policy** - Provide for location of complimentary and compatible service and residential uses in mixed use buildings within convenience commercial centers.

85.130(95) Valley View, Trestle, and Bear Creek Convenience Commercial Design Areas Goal - Provide for convenient vehicular access to and within shopping areas while avoiding congestion on arterial streets.

(a) **Policy** - Encourage business driveway access on local streets rather than the arterials wherever feasible.

(b) **Policy** - Encourage joint use of driveways and parking to minimize vehicle turning conflicts and reduce overall parking needs.

(c) **Policy** - Separate and/or buffer walkways from vehicular circulation areas.

(d) **Policy** - Avoid the creation of lots which increase the number of driveways on Redmond Way, Cleveland Street, Avondale Road and the Redmond-Woodinville Road.

85.130(100) Valley View, Trestle, and Bear Creek Convenience Commercial Design Areas Goal - Provide for low scale commercial areas which are attractively designed and reflect the importance of their position as entrances to the City Center.

(a) Policy - Avoid the orientation of service areas, the back of buildings and unscreened parking toward parks, walkways, arterials and freeway areas.

(b) Policy - Recognize the importance and a potential to create a strong visual image for the neighborhood by improving the appearance of Redmond Way and the Redmond-Woodinville Road at the entrances to the City Center.

(c) Policy - Work with Burlington Northern to enhance the appearance of the railroad bridge trestle over Redmond Way to act serve as an entrance feature.

85.130(102) Trestle Design Areas Goal - Provide for ongoing retail business while also anticipating a transition to more intensive uses.

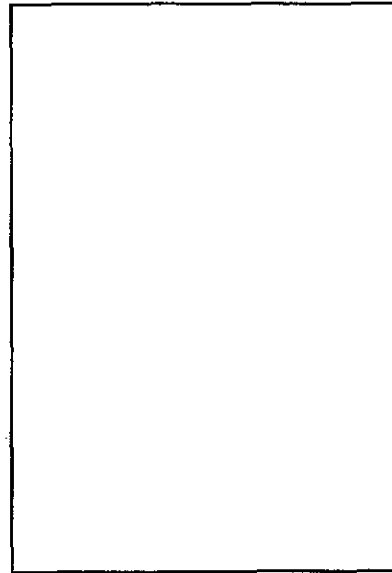
(a) Policy - Provide for the transition of that portion of the Trestle district which is east of 160th to uses and design standards similar to the Town Square office district.

(b) Policy - Retain the low scale of buildings adjacent to the Sammamish River, while encouraging conversion to river-oriented uses.

GOALS AND POLICIES FOR OFFICE DISTRICTS:

The City Center Neighborhood contains four distinct office districts, all intended to provide for significant employment growth in professional, business, health and personal services. The distinct districts provide for variations in design and development standards, and minor variations in land use where appropriate.

85.130(105) Town Square, Sammamish Trail, Leary, and River Bend Design Areas Goal - Provide for concentrated economic activity and employment in downtown Redmond in an attractive, mid-rise urban park environment with excellent transportation options.



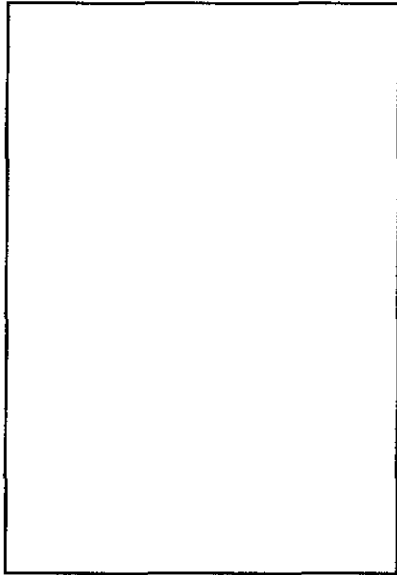
(a) Policy - Encourage retention, location and expansion of professional, financial and commercial office land uses, for personal and business services.

(b) Policy - Provide opportunities for complimentary retail uses at ground level and residential development on the upper floors in mixed use structures.

(c) Policy - Regulate building height, design and open space to provide transitions between design areas, and to minimize impacts on adjacent residential or lower scale districts.

(d) Policy - Provide a system of public and private open spaces, parks, bicycle routes and trails to provide outdoor areas within a densely populated employment center.

85.130(45110) Pedestrian Anderson Park Design Area Goal - Provide an area for pedestrian-oriented retail activities which visually and functionally complements and benefits from the retail shopping in the Old Town area and proposed Regional Shopping Center. Provide an area that is pedestrian-oriented and allows retail, office, and residential uses in an urban village context that is complimentary to adjoining Old Town and adjacent residential districts.



~~(a) Policy - Encourage development of office and/or mixed use projects (including residential or office uses above ground floor) within the Design Area that are complimentary and conducive to an urban village/neighborhood and which continue a pattern of physical structures similar in scale and rhythm to the Anderson Park Area.~~

~~(a) Policy - Provide a visually significant and direct connection to the adjacent shopping center and Old Town to allow these areas to function as a unified retail area~~

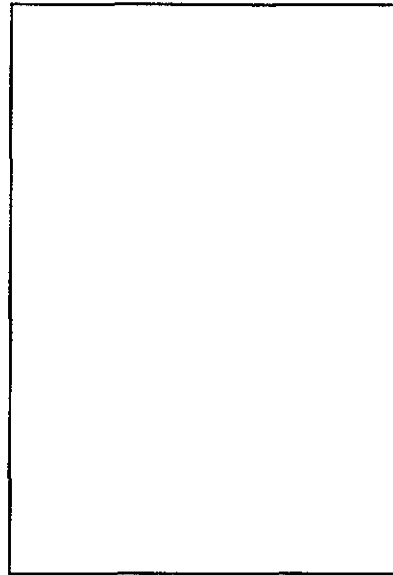
~~(d) Policy - Provide for pedestrian amenities such as benches, kiosks, plazas, public art and wide sidewalks.~~

~~(e) Policy - Encourage window and merchandise displays next to pedestrian walkways.~~

~~(f) Policy - Encourage the modulation of buildings facing the walkways to provide areas for outdoor restaurants, displays and gathering areas.~~

~~■ Encourages development and signing adjacent to the Old Town Village West area to complement rather than dominate the design objectives of that area.~~

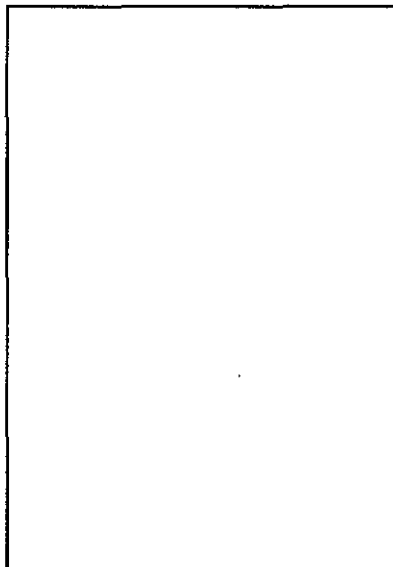
85.130(115) Sammamish Trail Design Area Goal - Encourage development of a lower scale, primarily office area within walking distance from transit and amenities such as the Sammamish River, parks, and the retail Design Areas.



(a) Policy - Encourage innovative developments that focus on and enhance the environment of the Sammamish River by providing open spaces, pedestrian walkways, bicycle and equestrian trails, and by designing buildings to front on both City streets and the Sammamish River Park.

(b) Policy - Establish a design/use overlay area along the Sammamish River that guides and encourages development of pedestrian and bicycle oriented uses along the river.

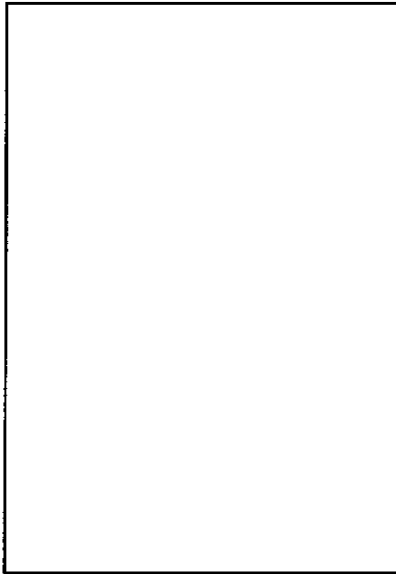
85.130(120) Leary Design Area Goal - Create a mixed use office, residential and retail area with a village character, pedestrian links to the core of City Center and which retains significant natural features.



(a) Policy - Encourage development in a master plan which utilizes and preserves a significant stand of large trees in the design area.

(b) Policy - Enhance pedestrian access from this design area to Old Town and Mixed Use/Shopping Center Design Areas, and the Sammamish River.

85.130(125) River Bend Design Area Goal - Encourage continuation of retail stores and services while providing for a transition to mid-rise office uses in the long term future.



(a) Policy - Allow a broad range of uses to provide optimum flexibility and potential for new development and expansion of existing conforming and compatible businesses.

(b) Policy - Encourage development which provides low cost space for start up businesses, expansion of existing auto service businesses and attractively sited and developed auto retail businesses.

85.130(130) River Bend Design Area Goal - Recognize the emerging role of this district as an entrance to the City Center by enhancing the appearance.

(a) Policy - Ensure landscape and design standards include adequate provisions to guide development of attractive buildings and yard areas.

(b) Policy - Ensure that land use and development standards address land use compatibility issues between adjoining and adjacent uses.

(c) Policy - Showcase auto sales businesses and other businesses with large outdoor display and storage elements through exemplary siting, building design, and landscaping.

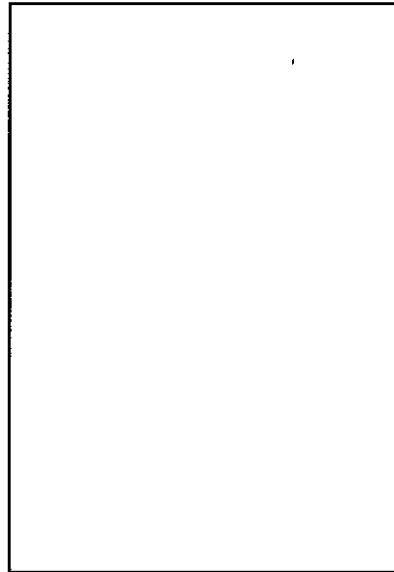
GOALS AND POLICIES FOR RESIDENTIAL DISTRICTS:

85.130(135) North Point, Carter, Foothill and East Hill Residential Design Areas Goal - Promote the development of high quality residential districts which provide desirable long term living environments for a variety of age and economic groups.

(a) Policy - Encourage a variety of well-designed housing styles and densities.

(b) Policy - Provide incentives to develop affordable housing and senior housing.

(c) Policy - Provide for commercial developments within or adjacent to residential buildings when design and operations are managed to prevent conflicts between uses.



85.130(140) North Point, Carter, Foothill and East Hill Residential Design Areas Goal - Create residential districts with buildings whose design, density, height and bulk are appropriate in relationship to the site and specific surroundings.

(a) Policy - Apply development standards and guidelines to promote aesthetically pleasing, private, safe, and comfortable housing, through design and open space.

(b) Policy - Ensure appropriate landscape design and installation in multi-family and commercial yards to enhance and maintain comfortable and appealing residential neighborhood environments.

(c) Policy - Ensure that designs are aesthetically pleasing through the Site Plan Review process.

(d) Policy - Provide for active recreation in individual projects and city parks.

(d) Policy - Provide for a mix of office and residential uses in the East Hill Design Area, with uses and buildings designed to maintain and complement the design area's residential character.

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Mixed Use/Residential Design Areas:

85.130(145) Foot Hill and East Hill Residential Design Areas Goal - Create areas which allow a variety of commercial and residential uses in mixed use buildings or side-by-side that maintain a residential look and feel in terms of design and character, in order to promote and maintain a comfortable living environment while also providing for non-residential uses.



(a) Policy - Ensure that building and site designs for non-residential uses are compatible with and complimentary to a residential character.

(b) Policy - Provide development standards and guidelines which encourage the development of non-residential uses that are complimentary to and compatible with residential development standards.

(c) Policy - Provide for a mix of retail, office and residential uses in the Foothill Design Area, provided uses are designed to reflect and complement the current residential character.

DOWNTOWN PLAN 12/3/93

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20C.10.230(1005) CITY CENTER PERMITTED LAND USES AND SITE REQUIREMENTS - The chart entitled, "City Center Permitted Land Uses and Site Requirements" is incorporated as a part of the subsection. It indicates what land uses are permitted within the Design Areas different districts shown on the Zoning Districts Map, in conjunction with the general land use definitions in Section 20C.10.230(15) and other restrictions related to these uses. ~~The intent of each of the Design Areas is as follows:~~ The districts are established as follows:

(a) CC-1 - The CC-1 district provides for a full range of retail uses such as specialty and comparison shopping, eating and entertainment establishments, as well general and professional services, and residential uses. The district is intended to continue the historical role of the area as the city's traditional center of commerce, culture and civic life.

(b) CC-2 - The CC-2 district is intended to provide for a major regional shopping center facility and associated uses such as offices, other retail activities, residential, theaters, and a hotel.

(c) CC-3 - The CC-3 district provides for retail stores and services likely to be visited frequently for everyday needs, such as grocery stores, restaurants, drug stores, dry cleaners, florists and similar uses. Residential and service uses are also allowed.

(d) CC-4 - The CC-4 district provides for a range of employment uses such as financial and professional services, public administrative offices, health services, and other activities normally conducted in multi-story office structures. The district is intended as an employment intense area, but also provides for supporting retail, service, residential and entertainment uses located within walking distance of each other. Residential uses are allowed and encouraged in mixed use projects with ground floor retail shops or offices below the residential units. Retail uses are permitted on the ground floor throughout this district.

(e) CC-5 - The CC-5 district encourages an eventual transition to mid-rise office uses to add to the employment base of the City Center Neighborhood, but also provides continued opportunities for automobile and boat sales, some retail uses, and residential uses.

(f) CC-6 - The CC-6 district encourages and provides for quieter, moderate density residential environments within walking distance to jobs, transit, retail, entertainment, and personal services. Professional services within buildings of residential design character are allowed in the district as well.

20C.10.230(9510) CITY CENTER DESIGN OVERLAY AREAS

The City Center Neighborhood is divided into four thirteen Overlay Design Areas entitled, "Valley View", "Bear Creek", "Trestle", "Sammamish Trail", "Town Square", "Old Town", "Leary", "River Bend", "North Point", "East Hill", "Carter", "Foothill", "Convenience Commercial", "Office", "Pedestrian" and "Mixed Use/Shopping Center". These areas are further delineated from the Zoning Districts Map in order to create sub-neighborhoods with unique design characteristics. They are shown on by the map entitled "City Center Design Areas", included in this section. The intent of each of the Design Areas is as follows:

(a) Old Town - This Design Overlay Area is established to be a center of pedestrian-oriented retail activity in the City Center Neighborhood. The design regulations shaping development in the area provide for an urban village pattern and rhythm which encourages narrow ground floor store fronts, small blocks, narrow streets, mixed use residential/office/service buildings, and pedestrian scale architecture. The regulations encourage the enhancement of existing older buildings with small ground floor retail spaces, by encouraging narrow bay spacing complimentary to pedestrian activity and interest, and encourages office space above them. The pedestrian nature of the area is emphasized through lower parking requirements and plans for creation of parking lots/structures in a few central locations at the edge of this area.

(d)(1) Mixed Use/Shopping Center - ~~The intent of this Design Area is to provide for a major regional shopping center facility and associated uses such as offices, other retail activities, residential, theaters and a hotel.~~ The Design Overlay Area is ~~designed~~ established to focus activity adjacent to the existing center of the City Center District and provide for pedestrian and automobile integration between the two areas.

~~(b) Convenience Commercial Districts:~~

(b) Valley View, Trestle, and Bear Creek Convenience Commercial - This Design Area ~~is provide for retail merchandise and services of daily consumption such as entertainment, food visited by stores, dry cleaners, florists and similar uses.~~ These areas are located near entrances to the City Center and are oriented primarily established to allow convenient to automobile access. Residential and office uses above retail shops are also allowed and encouraged, but not required.

~~(c) Office Districts:~~

~~(b) Mid Rise Office~~ The office design areas provide for uses such as financial and professional services, public administrative offices, health services, residential uses and other activities normally conducted in higher density multi-story structures. Residential uses are encouraged in mixed use projects with ground floor retail and offices below the residential units. The Office Design Area is separated into two sub-classifications, Urban and Campus, which provide for a differentiation in intensity of use, particularly building height, near the center of the City Center District.

(c) Town Square Office. This design area provides mid-rise office development of six to eight stories adjacent to an urban park. Mid-rise office and mixed-use developments with active ground floor arcades and pedestrian oriented shop fronts are encouraged.

~~(e) Pedestrian~~ The Pedestrian Design Area is an area of mixed retail, service and residential uses which provide a focus of pedestrian activity between the shopping center and the office areas. It incorporates the historical area (Old Town) and links the shopping center to other parts of the City Center. The intent of this Design Area is to limit automobile use to accommodate pedestrian activity and encourage recreational and cultural uses.

(d) Sammamish Trail - Provides for employment uses adjacent to the river in mid-rise buildings. Restaurant and entertainment uses focusing on, and orienting toward, the river trail are encouraged.

(e) Leary - Provides for office employment, entertainment, residential, and retail uses developed as part of a master plan which preserves significant trees in the design area, and creates a village environment complimentary to, and with pedestrian walking

connections to, the adjacent Old Town and Mixed Use Design Areas.

(f) River Bend - Encourages and provides for an eventual transition to a mid-rise, mixed-use office district and/or new auto mall. Existing non-conforming light industrial type uses are encouraged to upgrade through continued property investment.

(g) North Point, Carter, Foothill and East Hill Design Areas - Provides for office, service, retail and residential uses side-by-side and within close proximity to each other, within a neighborhood context and scale.

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(3) Leary - Provides for office employment, entertainment, residential, and retail uses developed as part of a master plan which preserves significant trees in the design area, and creates a village environment complimentary to, and with pedestrian walking connections to, the adjacent Old Town and Mixed Use Design Areas.

(4) River Bend - Provides for an eventual transition to mid-rise office uses, but also provides opportunities for auto related service uses oriented towards the greater downtown area, and retail uses serving the home improvement market, such as carpet stores, cabinet shops, home lighting specialists, and furnishings. Existing non-conforming light industrial type uses are encouraged to upgrade through continued property investment.

(d) Residential Districts:

North Point and Carter Design Areas - Provides for quieter, moderate density residential environments in locations close to jobs, transit, retail, entertainment, and personal services.

(e) Mixed Use/Residential Districts:

Foothill and East Hill Design Areas - Provides for office, service, retail and residential uses side-by-side and within close proximity to each other, within a neighborhood context and scale.

20C.10.230(11) - HEIGHT LIMIT OVERLAY AREA

The purpose of this section is to establish special building height limits in areas within City Center, to provide transitions in building heights within Design Area boundaries that border lower height Design Areas. These overlays apply to the Town Square and Anderson Park Design Areas.

(A) Building Height Restrictions. Building height restrictions within the Height Limit Overlay (HL) areas shall be as indicated on the Height Limit Overlay Map. The designation (HL) followed by a number indicates the maximum number of floors, and the maximum allowable building height in feet. For example HL 4/48' indicates that four stories or up to 48 feet would be allowed.

(B) Permitted Uses. Any use permitted in the underlying design area shall be permitted in the building overlay area.

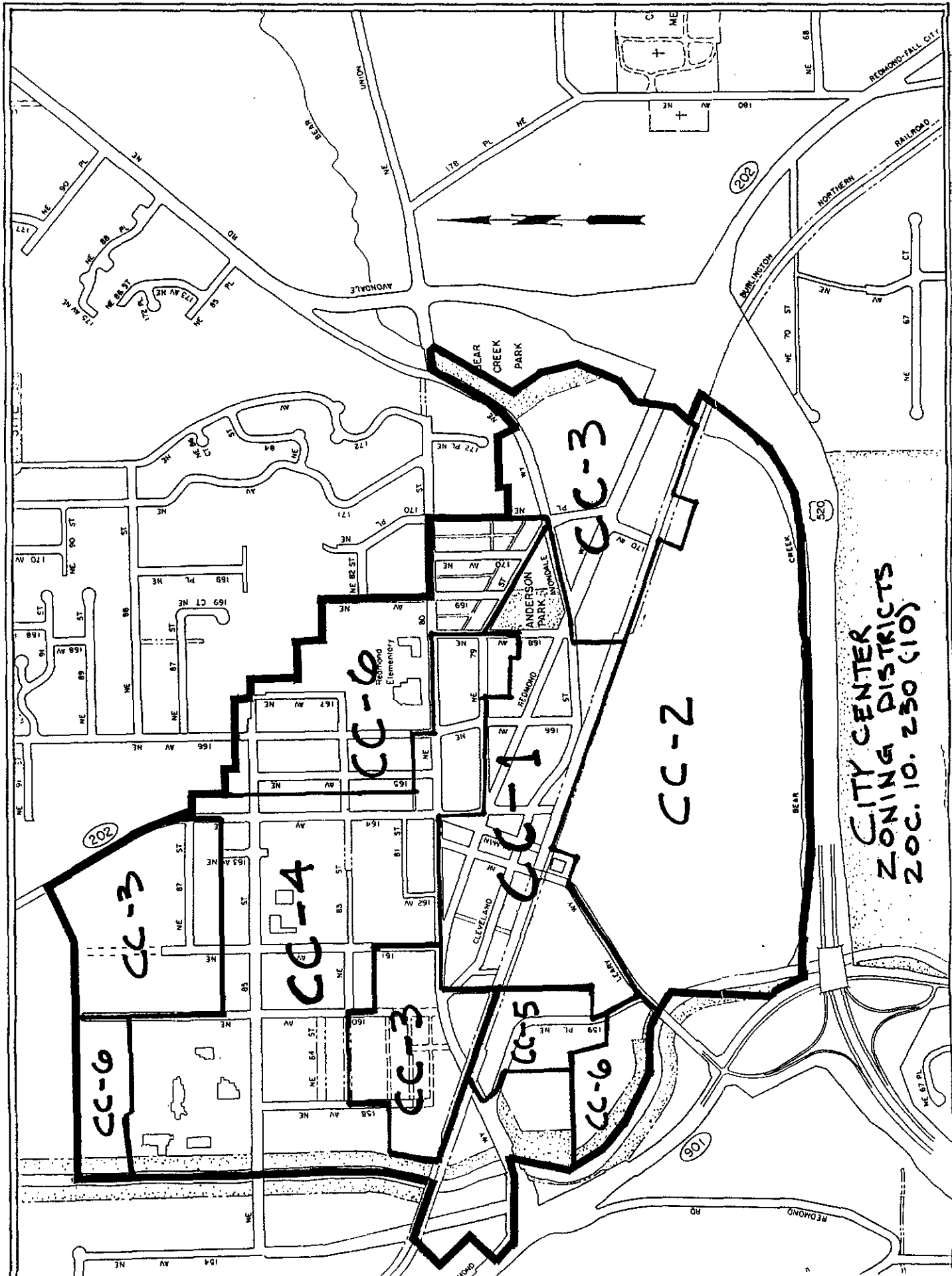
(C) General Provisions. Development shall not exceed the building height restrictions indicated on the design areas map as described in Section 20C.10.230(11)(A). For all other development standards, the standards of the underlying design area shall apply.

20C.10.230(15) Definition of Uses - For purposes of administering the City Center Permitted Land Use and Site Requirements eChart, the following definitions are noted. All new development shall meet applicable design standards and guidelines. *

(a) General Service Uses - General services are professional, commercial and public activities conducted in offices, and storefronts, without outdoor storage needs, including but not limited to lodging, personal services, business and financial services, minor repair services, entertainment and recreation services, theatres, health services, social services, legal services, educational services and cultural services. Customer and tenant parking structures and surface lots are considered accessory to the primary use. Services excluded from this definition are such as industrial laundries and dry cleaning, major automotive repair services where the primary business is work such as body repair, engine overhaul, transmission repair and junk yards, testing laboratories, publishing, warehousing/storage and similar uses which are considered industrial uses, and are excluded from this definition.

(b) General Retail Uses: General retail uses are establishments engaged in selling merchandise to the general public for personal or household consumption; processing of products does not occur or is only a minor part of use (for example; restaurants, butcher shops, breweries with taverns, art studios, etc.). This category also includes food stores, service stations, apparel; furniture and home furnishings; home improvement goods including carpeting, lighting, cabinets, plumbing fixtures; large and small durable goods for family and office use such as appliances, office furniture and supplies; eating and drinking places; and general merchandise. This category does not include uses with outdoor storage and display. Customer and tenant parking structures and surface lots are considered accessory to the primary use. *

(c) Public Facilities and Utilities: Public facilities include governmental administrative offices, libraries, parks, police and fire stations, educational institutions, cultural facilities, community or senior centers, parking structures, local utility supply lines or substations. Utilities are



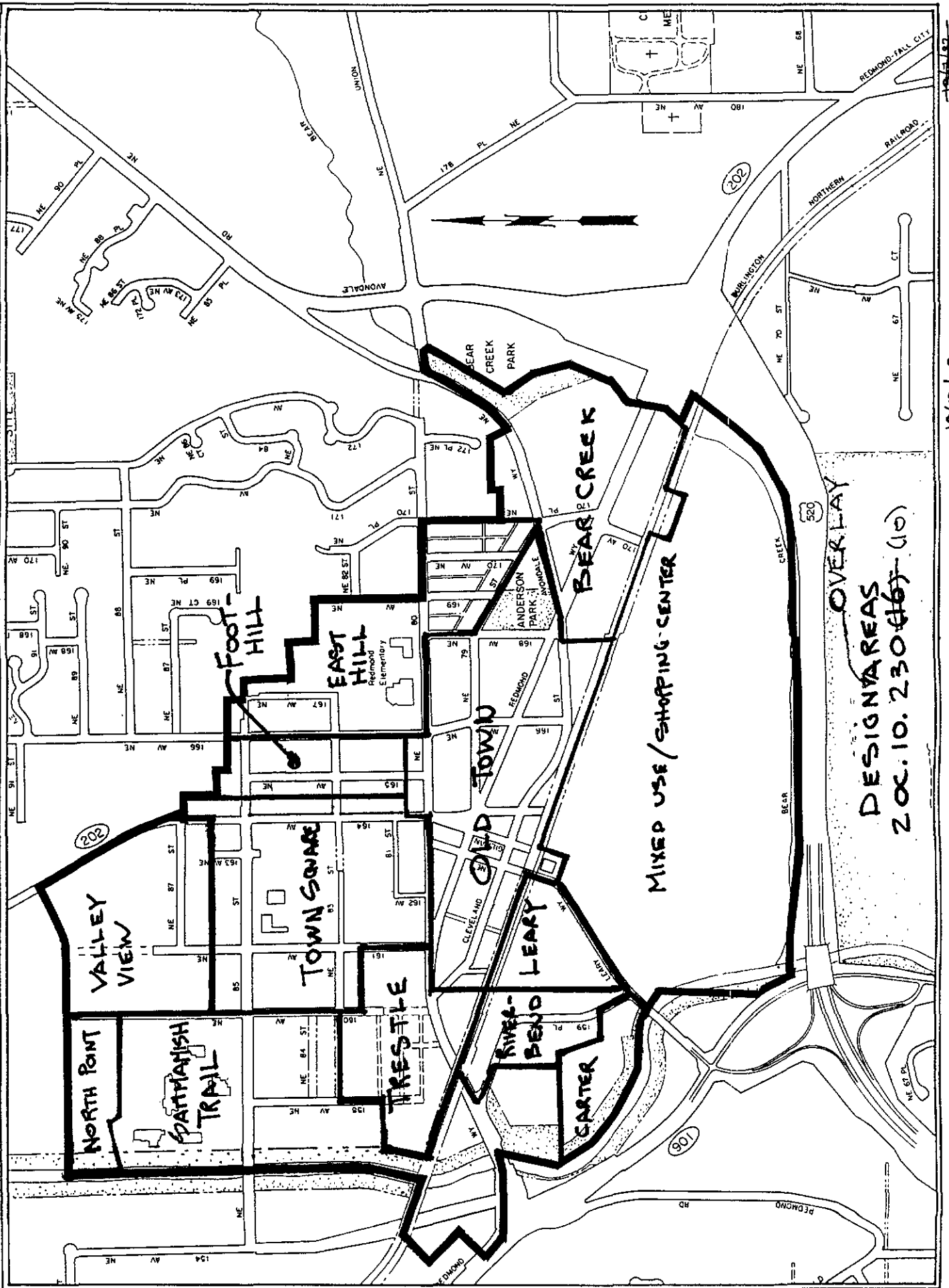
PROPOSED 20C.10.230(05) CITY CENTER PERMITTED LAND USES CHART

CITY CENTER ZONING DISTRICTS	CC-1	CC-2	CC-3	CC-4	CC-5	CC-6	8
PERMITTED LAND USES							
RETAIL				1a	1		1b
- General (except those below)	P	P	P	P	P		
- Automobiles/Boats with outdoor storage of vehicles					P		
- Gasoline			12		P		
- Vending Carts/Kiosks	G	G	G	G	G		
SERVICES							
- General (except those below)	P	P	P	P	P	P	
- Family Day Care (1-6 children)	P	P	P	P	P	P	
- Mini Day-Care (7-12 children)							
- Day Care Center (13+ children)	G	G	G	G	G	S	
- Minor Auto Repair		P	12		P		
- Repair: Electrical appliances motors, and furniture. Machine shops.			12		P		
- Repair and Rental: Electronic equipment, stereo, video, and watches.	P	P	12	P	P		
Residential:							
- Multi-Family	P	P	9	P	P	P	
- Senior Housing	P	P	9	p	P	P	
Public facilities and Utilities	P	P		P	P	P	11
Banks With Drive-Through Facilities				G	G		
All Other Drive-Through Facilities			12	P	P	P	

NOTE: P= Permitted; G= General Development Permit Required; S= Special Development Permit Required

- 1 - No single-story retail permitted, except Bulk Retail. In multi-story buildings, retail is permitted on the ground floor, but is not required. Restaurants, taverns and delicatessens are allowed on any floor. Nonconforming single-story uses may expand per Section 20F.10.060.
- 1a - Same as 1 above, except, Single-story sit-down restaurant and entertainment uses not less than 5,000 square feet GFA, and single-story master planned festival retail development adjoining and orienting to the river are permitted. No surface parking shall be within 150 feet of the river's top of bank.

- 1b - Retail uses allowed in Foot Hill Design area per footnote 1 above. See also Design Areas Map , 20C.10.230(16).
- 2 - Drive-through facilities shall have a minimum of 3,000 square feet GFA of contiguous floor space, shall be limited to one lane, only, and shall provide a minimum queuing distance of 120 feet which is not within the public right-of-way or on-site circulation aisles.
- 3 - Adult entertainment is governed by Section 20C.20.015 of the Community Development Guide.
- 4 - Governed by Section 20C.20.235(70)(c) Senior Citizen Housing Development - Density Bonus.
- 5 - Prior to approval of any development permits, a conceptual master plan which shows, at minimum, the building footprints, streets, and open space of the proposed development shall be approved by the City Council.
- 6 - Master Site Plan approval required through General Development Permit process. See Section 20F.20.030.
- 7 - On-site hazardous waste treatment and storage facilities permitted with a GDP provided that State siting criteria are met as set forth in RCW 70.105. A hearing may be required. If the associated permitted use requires a SDP, consideration of the treatment and storage facility shall be subject to same SDP process and conditions.
- 8 - Maximum density per lot dependent upon size and width of lot, per the City Center Site Requirements Chart .
- 9 - Not permitted on ground floor.
- 10 - For uses on public right-of-ways, Right-of-Way Use Permits are also required.
- 11 - Public facilities that serve only the immediate neighborhood or which by necessity must pass through the area are allowed with a General Development Permit. City wide facilities are not allowed.
- 12 - Gas Stations, minor auto repair, minor repair and machine shops , and drive-through facilities are prohibited west of 158th Ave. NE, extended. Surface parking is prohibited within 150 feet of the river's top-of-bank.
- 13 - See Section 20C.20.025, Automobile and Boat Sales.
- 14 - See Section 20C.20.027, Banks With Drive-Through Facilities. Banks without drive-through facilities are regulated as General Services.



OVERLAY
DESIGN AREAS
ZOC. 10. 230 (10)

10/12/93

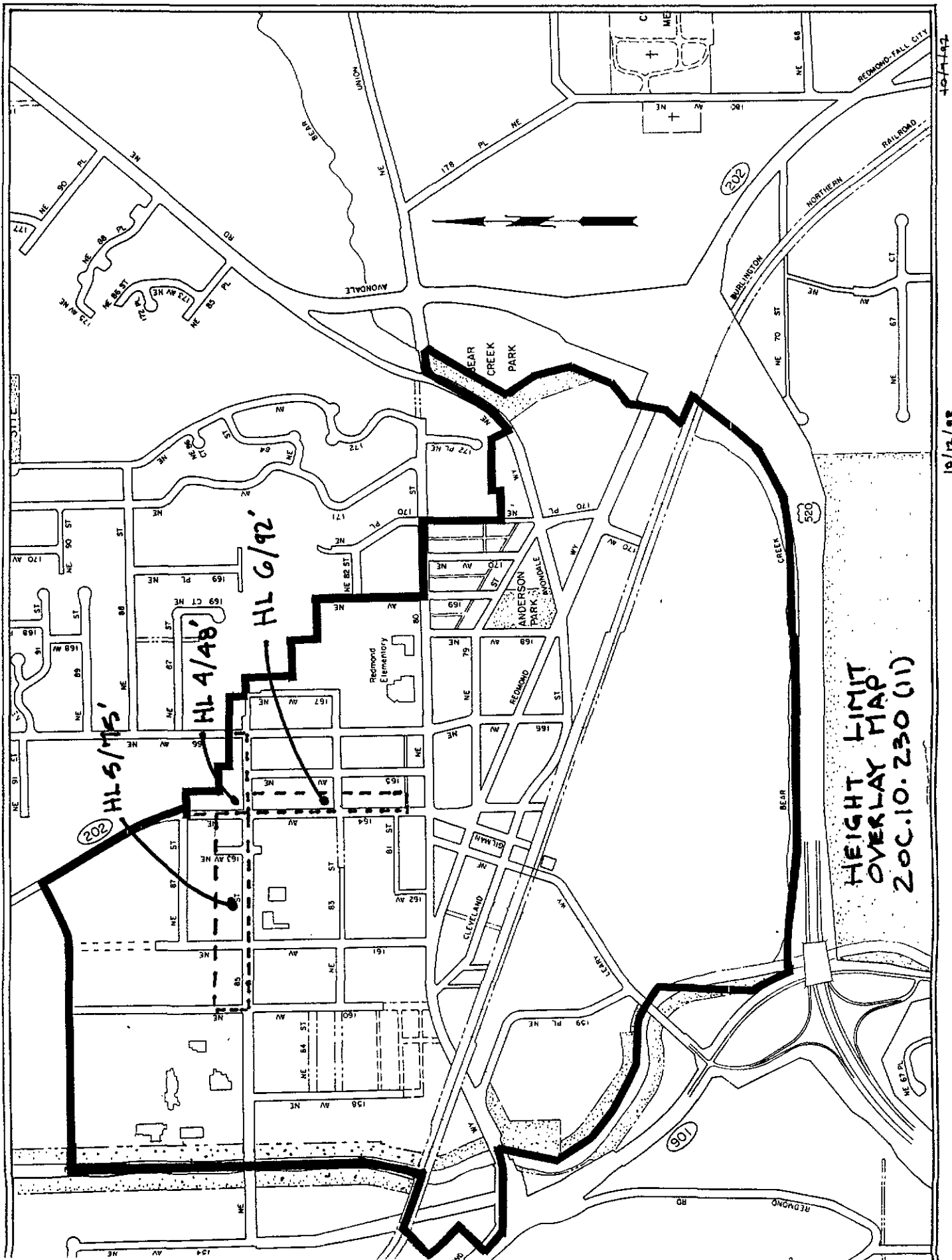
DESIGN AREA	Old Town	Mixed Use Center	Valley View Bear Creek Trestle	Town Square	Summamish Trail	Leary	River Bend	Foot Hill	North Point East Hill Carter
DESIGN CRITERIA	See Section 20C.10.050(25)	See Section 20C.30.050(30)	See Section 20C.30.050(10)	See Section 20C.30.050(15)	See Section 20C.30.050(15)	See Section 20C.30.050(35)	See Section 20C.30.050(40)	See Section 20C.30.050(50)	See Section 20C.30.050(40)
SITE STANDARDS									
RESIDENTIAL DENSITIES									
- Maximum Number of Dwelling Units Per Gross Acre	FAR 1.75 See Table 20C.10.230(16)(A)	55 du/ac See Table 20C.10.230(16)(A)	30 du/ac See Table 20C.10.230(16)(A)	FAR 2.25 See Table 20C.10.230(16)(A)	FAR 1.75 See Table 20C.10.230(16)(A)	FAR 1.75 See Table 20C.10.230(16)(A)	FAR 1.75 See Table 20C.10.230(16)(A)	FAR 2.0 See Table 20C.10.230(16)(A)	55 du/ac See Table 20C.10.230(16)(A)
- Permitted Location	2nd floor or above	2nd floor or above	2nd floor or above	2nd floor or above	2nd floor or above	2nd floor or above	2nd floor or above	No Special Restrictions	No Special Restrictions
MINIMUM YARD SETBACKS									
- Front Street	2, 2a 14 feet	3	1a, 2a 20 feet	2, 2a 14 feet	2, 2a 14 feet	2b 20 feet	2a 20 feet	2a 28 feet	2a 28 feet
- Side Street	2 14 feet	3	1a 20 feet	2 14 feet	2 14 feet	2c 20 feet	2c 20 feet	2c 28 feet	2c 28 feet
- Side Yard	0 feet	3	0 feet	0 feet	0 feet	2c 5 feet min.	2c 5 feet min.	2c 5 feet min.	2c 5 feet min.
- Side Yard - Residential uses above ground floor commercial	5 feet min.	5 feet min.	5 feet min.	5 feet min.	5 feet min.	5 feet min.	5 feet min.	5 feet min.	5 feet min.
- Rear Yard	0 feet	3	0 feet	0 feet	0 feet	2c 5 feet min.	2c 5 feet min.	2c 5 feet min.	2c 5 feet min.
- Rear Yard - Residential uses above ground floor commercial	10 feet	3	10 feet	10 feet	10 feet	2c 10 feet	2c 10 feet	2c 10 feet	2c 10 feet
- Yard Adjoining Residential District	5 feet	3	5 feet	5 feet	5 feet	2c 5 feet	2c 5 feet	2c 5 feet	2c 5 feet
- Between Buildings On-Site	N/A	3	N/A	N/A	N/A	N/A	N/A	N/A	N/A
- Parking	5	3	5	5	5	5	5	5a	5
MAXIMUM BUILDING HEIGHT (Stories/Feet)	5/75'	3-8 stories	2/30' 3/45'	6/92' 8/120'	5/75'	5/75'	5/75'	6/90'	4/48'
LOT COVERAGE	11a 100%	11, 11b	11a 80%	11a 100%	11a 100%	11a 100%	11a 100%	11a 80%	11a 80%
PARKING SPACES REQUIRED	12 See Section 20C.20.150	See Section 20C.20.150	13 See Section 20C.20.150	See Section 20C.20.150	12, 13 See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150

NOTES:

- 1 - Measured from face of curb. See, also, Section 20C.10.230 (25) City Center Pedestrian System. See also Section 20C.20.220, Sight Clearance at Intersections.
- 1a - Thirty (30) feet, if designated per Section 20C.10.230(25) City Center Pedestrian System.
- 1b - Measured from property line. In case of alley, measured after dedication, if necessary.
- 2 - On streets fronting Type V Pedestrian walkways per Section 20C.10.230(25) 14 feet is the average required. Building fronts shall be developed to the sidewalk and or plaza edges.
- 2a - Front setback for residential use above ground floor commercial shall be regulated by Section 20C.10.230(25) City Center Pedestrian System.
- 2b - Where a grove of trees (6 or more significant trees within a quarter-acre area) lie along a street front, the setback shall be 100 feet.
Otherwise the set back shall be per Section 20C.10.230(25) City Center Pedestrian System requirements.
- 2c - Where a grove of trees (6 or more significant trees within a quarter-acre area) lie along the easterly design area boundary, the setback shall be 30 feet.
Otherwise the set back shall be zero (0) feet.
- 3 - Subject to Master Plan and Site Plan Review Approval.
- 4 - Within 100 feet of adjoining Residential Districts, bay doors other than for loading may not open directly toward (without a building in between) the residential district.
- 5 - Governed by Section 20C.20.150(15)(o) Parking Garages.
- 5a - Same as 5 above, except standards apply regardless of Pedestrian System Type, and sub-paragraph (iv) does not apply.
- 6 - Six (6) floors is maximum height without bonus. Bonus to eight floors granted for provision of 20% on-site usable open space in the form of plazas/arcades accessible to public during extended business hours.
- 7 - Height limit overlay districts may apply; see Section 20C.10.230(11) Height Limit Overlay Zones.
- 8 - Regional Shopping Center - 3 stories (60 feet), Hotel on Town Square - 6 stories (75 feet) or 8 stories (100 feet) with meeting conference and banquet facilities on Town Square - 4 stories (50 feet) all other permitted uses - 3 stories (40 feet) or 4 stories (50 feet) with parking structure.
- 9 - Both stories and height standard must not be exceeded, and height shall be measured to the top of mechanical equipment enclosures.
- 10 - The maximum height may be increased to 3 stories and 45 feet when a grocery store is a major tenant on the project site.
- 11 - Governed by Section 20B.85.130(105)(a) Mixed Use Shopping Center Design Area Goal and Policy.
- 11a - For developments with residences above ground floor commercial/office, Lot Coverage shall be governed by this table.
- 11b - For residential development without ground floor commercial/office, Lot Coverage shall be governed by Section 20C.10.230(17)(A).
- 11c - Lot coverage percentage equals: The total site area measured to the PROPERTY LINE, less Pedestrian Systems measured to the CURB LINE, on-site sidewalks, landscaping, and plazas; DIVIDED by the site area measured to the CURB LINE.
- 12 - A density bonus for contribution to In-Lieu Parking Fund is available per Section 20C.20.150(20)(b).
- 13 - Surface parking is prohibited within 150 feet of the river's top-of-bank.

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HEIGHT LIMIT
OVERLAY MAP
20C.10.230 (11)

10/12/93

Residential Densities For City Center Design Districts

Districts	Minimum Site Area (Sq. Ft.)	Minimum Site Width (Ft.)	Permitted Dwelling Units Per Net Acre	Maximum (1) Allowable Lot Coverage
ALL DESIGN AREAS ALLOWING RESIDENTIAL USES, EXCEPT CONVENIENCE COMMERCIAL	2,400-3,199	less than 30	1 unit per lot	75%
	2,400-3,199	30-43	2 units per lot *	75%
	3,200-5,999	44-49	26 units per acre	75%
	6,000-11,999	50-119	30 units per acre	N/A
	12,000-17,999	120-179	45 units per acre	N/A
	18,000 or more	180-or more	55 units per acre (2)	N/A
CONVENIENCE COMMERCIAL	2,400-3,199	less than 30	1 unit per lot	
	2,400-3,199	30-43	2 units per lot *	
	3,200-5,999	44-49	26 units per acre	
	6,000 or more	50 or more	30 units per acre	

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In order to ensure that appropriate bulk and height relationships exist between residential lot widths and building intensity, the following table shall be used to determine the permitted residential density of a site. The density allowed on the site shall increase with the increase in site area and site width. The table is read from left to right, within the rows. For example, a site that is 12,000 square feet in area and 120 feet in width will be allowed up to 45 dwellings per acre. If the site's area and width are in different rows, i.e., 12,000 square feet and 100 feet wide, the lower density shall apply (30 dwelling units per acre).

The lot coverage standards for residential uses without ground floor commercial uses are in the right hand column. These lot coverage standards do not apply to residential developments with ground floor commercial uses.

Exceptions:

If the site's width is in a row that allows a higher density, as opposed to the site area, the higher density shall be allowed if the site's area is also at least 85% of the highest range in the column above. Example: Site width = 120 feet and the site area is at least 10,199 square feet (85% of 11,999 square feet), then 45 dwelling units per acre shall be allowed.

If the site's area is in a row that allows the higher density, but the width limits it to a lower density, the higher density shall be allowed if the site width is at least 85% of the lowest range of the widths in the same row. Example: Site area = 12,000 square feet and the site width is at least 102 feet (85% of 120 feet), then 45 dwelling units per acre shall be allowed.

(1) Lot coverage standards apply to North Point, East Hill, and Riverside Residential design areas. The lot coverage standard applies to the River Bend design area when in conjunction with a residential project without ground floor commercial/office use. All other design areas shall comply with the City Center Site Requirements Table 20C.10.230(16).

(2) In the Sammamish Trail, Old Town, Leary, and River Bend Design Areas, the maximum density allowed shall be governed by a Floor Area Ratio of 1.75, exclusive of bonuses. In the Foothill Design Area, the maximum density allowed shall be governed by a Floor Area Ratio of 2.00, exclusive of bonuses. In the Town Square Design Area, the maximum density allowed shall be governed by a Floor Area Ratio of 2.25, exclusive of bonuses.

* With alley access, only. Otherwise one unit per lot.

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gas, water, sewer, electricity, telephone and cable television.

(d) Vending Carts/Kiosks: Includes itinerant vendors with mobile carts and temporary structures operating on private property or public right-of-ways and plazas. Does not include drive-through facilities, except where otherwise permitted.

20C.10.230(16) CITY CENTER SITE REQUIREMENTS

(A) General: Except as otherwise provided in this Section, all applicable provisions of the Community Development Guide and other applicable codes and ordinances apply to development in the City Center Design Areas.

(B) Site Requirements: The City Center Site Requirements Chart, 20C.10.230(16), sets forth other dimensional and site requirements for Design Areas within City Center. Each structure, development or activity in the Design area must comply with these requirements.

(C) Planned Unit Developments (PUD) in City Center: The purpose of this section is to promote creativity in site layout and design, and to allow flexibility in the application of standards for commercial, office, retail, and mixed-use/ residential development within the City Center Neighborhood, and to achieve the creation of sites and uses that may benefit the public by the application of special design policies and standards not otherwise possible under conventional development regulations and standards. General Development Permit procedures defined in Section 20F.20.170 General Development Permit; PUD - Scope of Review 20C.20.235(20); and PUD - Scope of Approval 20C.20.235(25) shall apply in the establishment of a PUD in City Center.

(i) Qualifying Standards - In order to qualify for the Planned Unit Development designation within the City Center Neighborhood, a property must meet the following criteria:

(a) The property must contain not less than forty thousand square feet; and

(b) The property must be bound by two or more public streets.

(c) The proposed project design must demonstrate all of the following:

1. *Superiority in achieving the goals and policies for the design area in which the site is located;*

2. *Superiority in design in terms of architecture, building materials, site design, landscaping and open space;*

3. *The project provides benefit to the City Center Neighborhood in terms of desired use, activity, and design.*

(ii) PUD - Flexibility of Design Standards in City Center - Requirements of Article 20C.00.000, "Land Use Regulations," that may be modified by application of this subsection are defined specifically as follows:

Parking Lot Location - Requirements for the location of on-site parking lots may be modified within the PUD (except for parking within residential yard areas) to provide for greater joint-use and quasi-public parking opportunities and uses which are highly desirable in the subject design area.

- *Mid-Block Pedestrian Walkways and Vehicular Lanes - Requirements for mid-block Pedestrian and Vehicular lanes per Section 20C.10.230(25) City Center Pedestrian System may be modified to allow variations in locations and minimum widths for these items to provide superiority in site design and function which benefits both the property owner and public.*

- *Building Separation, Privacy, and Lot Coverage - Requirements for building location, privacy and lot coverage may be modified within the PUD to provide superiority in site design: ie., greater amounts of privacy, maintenance of views, preservation of vegetation, provision of usable open space, adequate light, air, and security.*

20C.10.230(17) RESIDENTIAL DEVELOPMENT IN CITY CENTER DESIGN AREAS - All residential development in City Center Design Areas shall comply with the density and development standards indicated in the City Center Site Requirements Chart, except where otherwise specified. Residential uses shall be permitted in City Center Design Areas as indicated in the City Center Land Use Chart. Residential developments within the Design Areas shall also comply with the following criteria:

(A) *Lot Coverage.* For residential developments without ground floor commercial/office uses, the maximum lot coverage permitted for principal and accessory structures shall not exceed the lot coverage standards on Table 20C.10.230(16)(A). The following structures or portions of structures shall be exempted from the measurement of lot coverage:

i) *Ramps or other access for the disabled or elderly meeting Washington State Rules and Regulations for Barrier-Free Design;*

ii) *Required Pedestrian walkways/vehicular lanes per City Center Pedestrian System. The area devoted to the required Pedestrian System between the building and curb line/property line shall be counted as open space.*

iii) *An underground structure (below natural grade), or under ground portion of a structure, on any part of the entire lot that is landscaped on the surface;*

(B) *Yard Requirements.* Yards shall be provided for the purpose of creating light, air, pedestrian and vehicular circulation, emergency access and general aesthetic improvements. All lots shall have one front yard and one rear yard, with the exception of through lots (lots with two fronts), which may have two front yards and no rear yard. Where a residential use is above ground floor commercial/office, the side and rear yard setbacks shall apply only to the residential use.

i) *Measurement.* All yard areas shall be measured from the property lines, except as noted in the City Center Site Requirements Chart. All yards shall be measured after dedication for required rights-of-way.

ii) *Permitted Structures in Required Yard Areas.* The following features are permitted within front, rear and sideyard setback areas provided that they project no closer than 5 feet to a property line and do not project more than 5 feet into the setback area. The projections may be further restricted through the Site Plan Review Process. Permitted projections include building extremities such as: balconies, patios, porches, architectural protrusions, subterranean garages and structures (below natural grade), bay

windows, and chimneys. Where the required building setback is at the pedestrian system sidewalk edge, structures on the ground floor may project into the required Pedestrian System per Section 20C.10.230(25)d. *

ii) *Front Yards.* Front yard depths shall conform to the standards specified in the City Center Site Requirements Chart 20C.10.230(16).

iv) *Side Yards.* Side Yards shall be determined by structure depth and height, according to Table 20C.10.230(17)(B)(iv). The depth of the building shall be measured perpendicular to the front property line.

Table 20C.10.230(17)(B)(iv)
Residential Side Setbacks

Structure Depth in Feet	Height of Side Facade at Highest Point in Feet		
	0-30'	31-40'	41' +
	Minimum Side Setback in Feet		
65' or less	5'	6'	7'
66' to 80'	6'	6'	8'
81' to 100'	8'	9'	11'
101' to 120'	11'	12'	14'
121' to 140'	14'	15'	17'
141' to 160'	17'	18'	20'
161' or more	19'	21'	23'

v) *Rear Yards.* Rear Yard setbacks shall be provided for all residential uses per the City Center Site Requirements Chart.

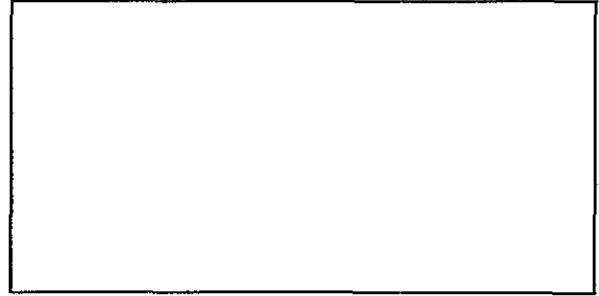
vi) *Distance Between Buildings.*

a) *Principal Buildings.* Two or more detached principal use buildings on the same lot shall have a minimum separation of 15 feet.

b) *Accessory Structures.* A detached accessory structure shall be setback a minimum of five feet from any structure to which it is not attached. This restriction shall not apply to air conditioners, heaters, vents,

pumps, solar collectors or similar equipment.

c) *Roof Eaves.* Roof eaves of principal and accessory use structures shall have a minimum separation of five feet. This restriction shall not apply to patio covers and similar structures with open lattice, grill work or uncovered roof beams.



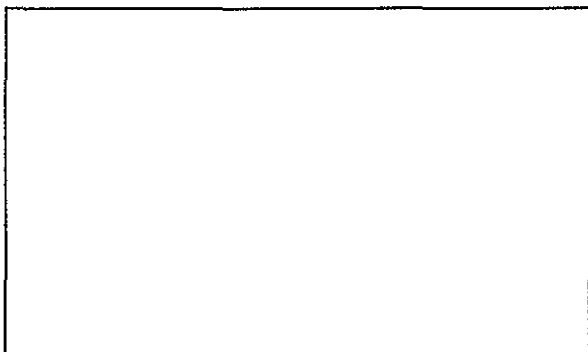
Area of walkway counted as open space.

(C) *Usable Open Space.* Two hundred (200) square feet of usable open space shall be provided with each unit, with at least 50 percent developed as common open space; and private open meeting the criteria below. Indoor recreational space may be substituted for common usable open space. For example, a four unit development requires 800 square feet of open space. At least 400 square feet must be in common open space, and each unit must have the minimum required private open space per unit, with all open space adding up to at least 800 square feet.

*

i) *Open Space Size and Dimensions.* Table 20C.10.230(i) specifies the minimum open space size and dimensions for both common and private usable open space areas.

ii) *Combining Usable Open Space and Pedestrian Access.* Parking areas, driveways and pedestrian access other than pedestrian access required by Washington State Rules and Regulations for Barrier-Free Design shall not be counted as usable open space, except; if the total width of the common usable open space is eighteen (18) feet or wider, any pedestrian path or walkway traversing through the open space may be considered usable open space. See examples below.

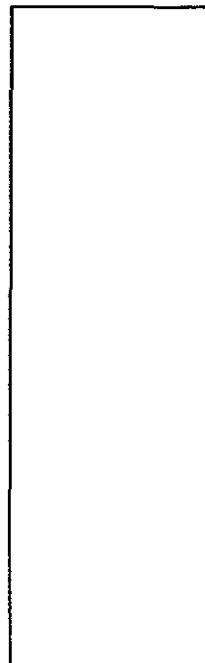


Area of walkway not counted as open space

iii) *Screening of Open Space.* Private usable open space shall be screened as specified in Table 20C.10.230(C)(iii).

iv) *On-Site Recreational Amenities.* All developments shall provide:

a) *Irrigated planters with seating between eighteen inches and twenty-four inches in height. See figure below*



b) *Projects with twenty-one (21) or more units shall provide at least one on-site amenity such as: a recreation room of three hundred (300) or more square feet, furnished with recreational facilities; a swimming pool; or other recreational amenities such as play ground equipment. The facilities shall be appropriate for*

the target housing market segment, as judged through Site Plan Review.

(D) Privacy Standards. The intent of this section is to ensure that new residential development provides adequate privacy and comfort for the tenants and their adjacent neighbors.

i) Applicability. Privacy standards shall apply to all residential occupancy areas, as defined by the Uniform Building Code.

ii) Standards. The Privacy Standards of Table 20C.10.230(D)(ii) apply to the shortest horizontal distance between the specified window and the specified property line, or to the window or wall of another unit in the same project. Each unit in a project must meet these standards in each room.

iii) Definitions.

a) The measurements indicated in Table 20C.10.230(D)(ii) are "line-of-sight" measurements from window to window.

b) The following are considered "blank walls":

- 1. Garden walls four feet or more in height.*
- 2. Etched glass, frosted glass, stained glass or similar translucent but non-transparent materials; and*
- 3. Windows with a lower sill not less than five feet-six inches above the finished floor level.*

c) The primary room means a living room, dining room or family room. Where an open floor area plan combines the living and dining rooms, the living/dining room shall be considered one room.

d) All rooms not defined above as a primary room shall be considered a secondary room.

e) In the case where windows in one room are of equal size, either window may be selected by the builder as the largest window.

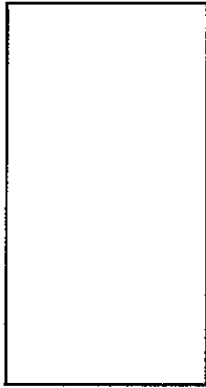
iv) Public Corridors. Public circulation corridors may be located within window-to-window or window-to-wall spacing distances. However, such corridors shall also have a minimum privacy spacing distance from primary and secondary windows as established in Table 20C.10.230(D)(ii)

(E) Minimum Courtyard Dimensions. The courtyard provision is intended to create attractive and comfortable on-site open spaces for residents so they may enjoy outdoor living and recreational activities. Where courtyards are part of a development, courtyard designs shall conform to the standards as specified. Exceptions to the minimum dimension requirement may be granted through the Site Plan Review process only if the Design Review Board, Technical Committee, or Code Administrator finds that the alternative design, improves the overall quality of the development and meets the intent of the courtyard provision. Standards are as follows:

i) The minimum dimension (width and depth) of any courtyard shall be no less than 1.5 times the tallest building or segment of building enclosing the court yard (a ratio of 1:1.5), but shall not be required to exceed fifty-five (55) feet;

ii) The height of the building wall shall be measured from the courtyard elevation to the roof eaves of the enclosing building(s);

iii) The courtyard dimension is a measurement of the usable open space between two building walls, or to a property line. If balconies or corridors project into a courtyard, the dimension shall be measured from the edge of the projecting balconies and, or, corridors, see figure below;



b) *Size.* The entry shall be at least eight (8) feet wide.

c) *Walkway.* A decorative paved walkway, separated from and not crossing driveways shall be provided between the entry and the City Center Pedestrian System.

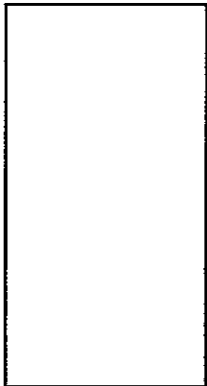
d) *The entry of ground floor units and ground floor foyers along street fronts shall be between two (2) and four (4) feet above the grade of the adjoining Pedestrian System in order to provide visual interest in the yard abutting the street. This requirement shall not apply to residential projects with ground floor commercial uses. (See figure below)*



iv) *Special incentive is provided to encourage courtyards which are visibly open to the street. If fifty (50) percent of the courtyard width is open to the street, the area that is contiguous with the courtyard, in the setback area, can be counted as usable open space (see Fig. 20C.10.230(E); and*



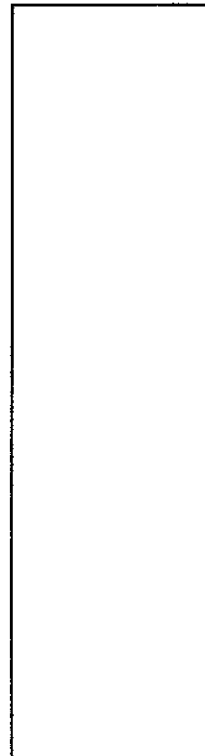
v) *If the enclosing walls terrace upward and back with succeeding stories, the courtyard dimension may be reduced but shall not be less than 1.5 times the height of the ceiling of the lowest enclosing floor (see figure below)*



(F) *Access to Buildings.* In order to ensure that orientation of the primary entrance is toward the street and adds interest to the building design, the following pedestrian access requirements shall apply.

i) *Front Entry.* Every multifamily building shall have a covered front entry way developed in accordance with the following standards:

a) *Orientation.* The primary entry shall face the front property line.



(G) *Required Facade Modulation.* In order to provide architectural interest in larger residential developments and to maintain proportional residential masses, the following standards shall apply:

i) *Front and Side Street Facades.* Facades facing streets shall be modulated approximately every forty (40) feet, depending on unit separation locations.

TABLE 20C.10.230(C)(i) RESIDENTIAL USABLE OPEN SPACE SIZE AND DIMENSIONS

Type of Usable Open Space	Minimum Length	Minimum Width	Minimum Height	Minimum Area in Square Feet
1. Common*	12'	12'	As specified in UBC for habitable overhead height	200
2. Private				
- Patio	8'	8'	Same as above	80
- Balcony (or adjoining elevated circulation corridor)	5'	5'	Same as above	50

**Front yards may not be counted as common open space, except per 20C.10.230(E)(iv).*

TABLE 20C.10.230(C)(iii) SCREENING OF OPEN SPACE

The Area from Which Open Space Must Be Screened	Screen Height
1. Common open space	4'0"
2. Corridors	4'0"
3. Streets or other public right-of-way	3'0"
4. Units on same level	4'0"
5. Units across interior side yard	4'0"

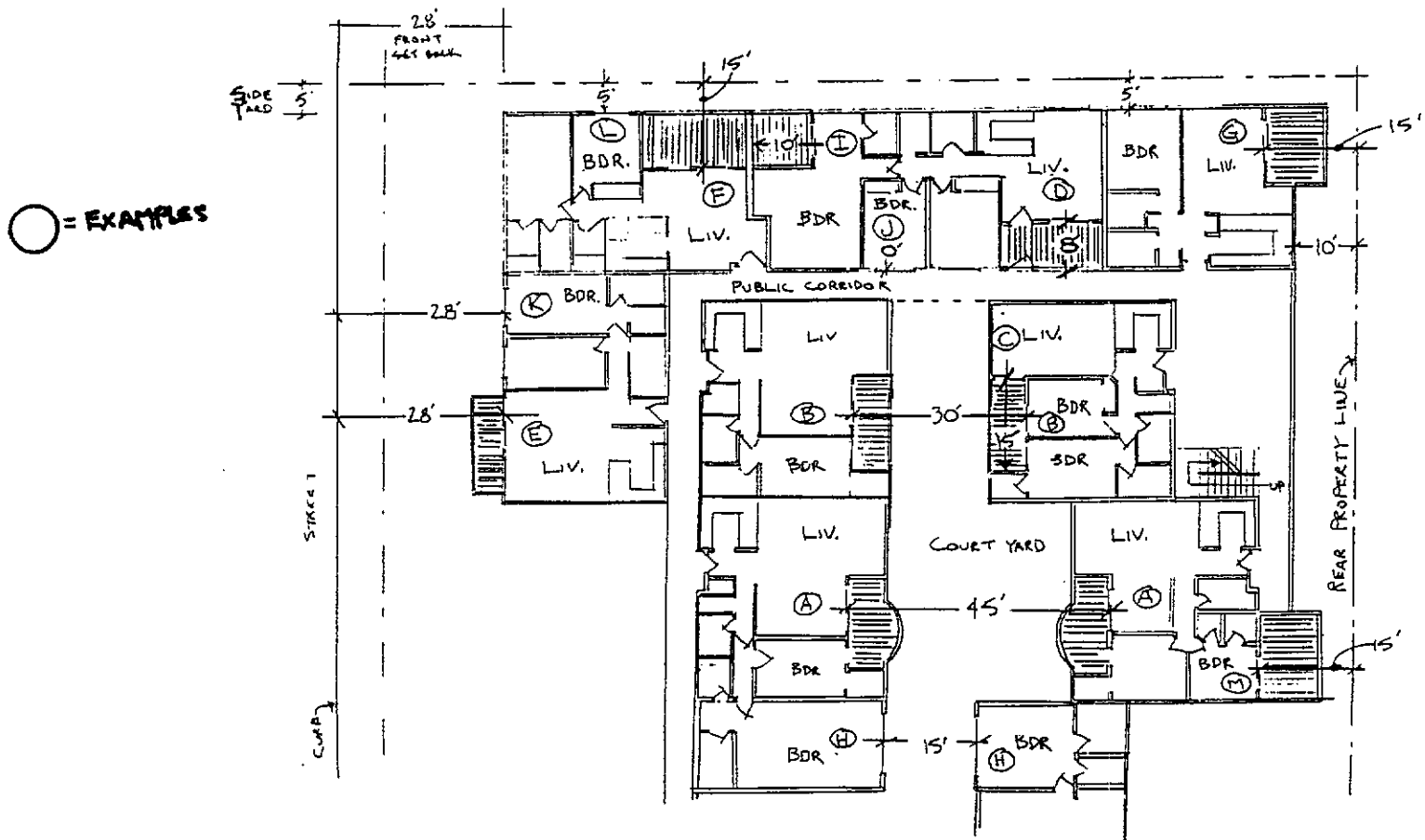
TABLE 20C.10.230(D)(ii) PRIVACY STANDARDS

	On-Site				At Property Line			
	Primary Room Largest Window	Secondary Room Largest Window	Blank Wall	Public Corridor	Front	Side	Interior	Side Street
Primary Room The largest window (Living Room, Dining Room)	45' (A)	30' (B)	15' (C)	8' (D)	Front Yard Setback (E)	20' (F)	15' 20' (G)	Front Street Setback
Secondary Room The largest window (All other rooms)	30' (B)	15' (H)	10' (I)	0' (J)	Front Yard Setback (K)	Side Yard Setback (L)	15' 20' (M)	Side Street Setback
Blank Wall	15' (C)	10' (I)	5'	0'	Front Yard Setback	Side Yard Setback	Rear Yard Setback	Side Street Setback

The Privacy Standards apply to the shortest horizontal distance between the specified window and the specified property line, or to the window or wall of another unit in the same project. Each unit in a project must meet these standards in each primary/secondary room. Measurements shall be "line-of-sight", measured from the center of the subject windows.

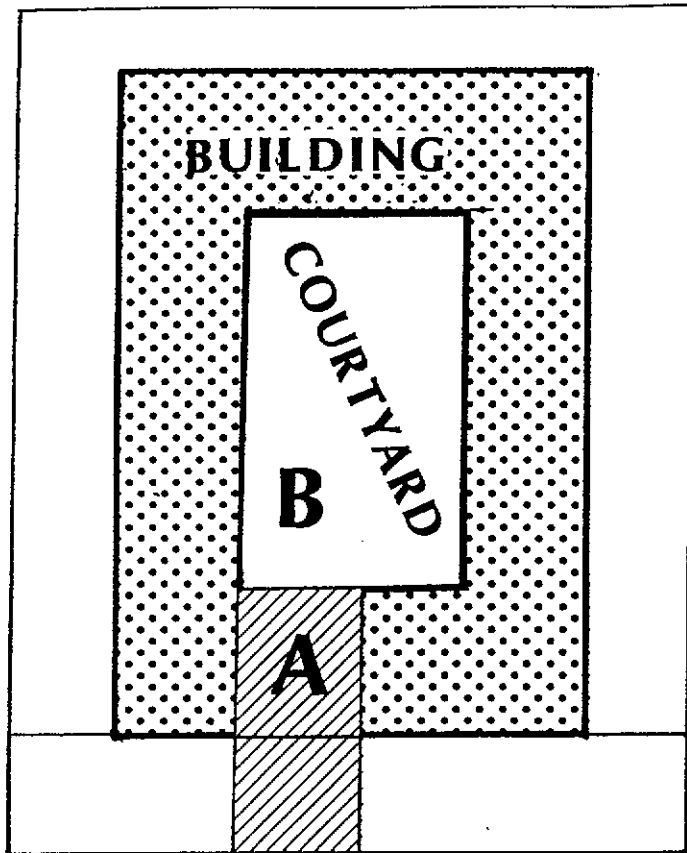
The primary room means a living room, dining room or family room. Where an open floor area plan combines the living and dining rooms, the living/dining room shall be considered one room.

All rooms not defined above as a primary room shall be considered a secondary room.



EXAMPLE #1

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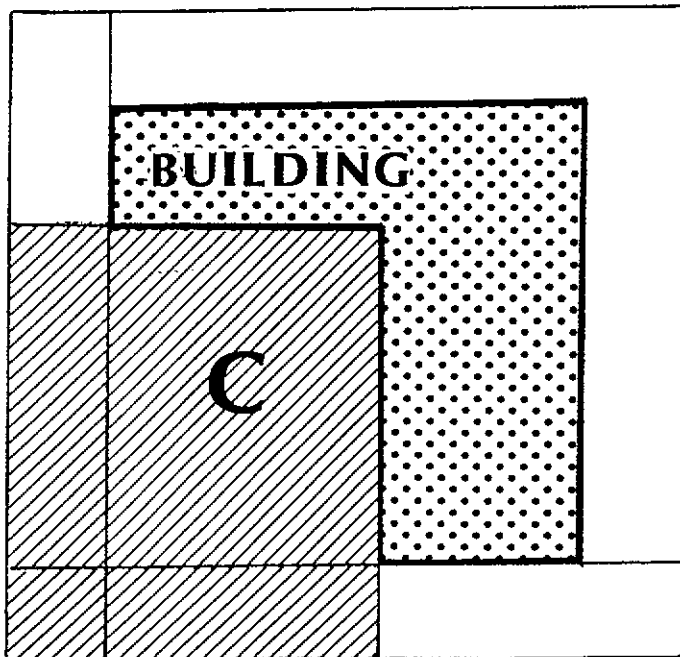
If the width of "A" is equal to at least half the width of "B" and "B" meets the minimum courtyard dimension standards, then the entire shaded area of "A" can be counted as usable open space.

REQUIRED SETBACK

FRONT PROPERTY LINE

STREET

EXAMPLE #2



When a courtyard is open to the street, the shaded area, "C", including the shaded portions of the front yard and side yard setback areas can be counted as usable open space. "C" must meet the minimum courtyard standards.

REQUIRED SETBACK

FRONT PROPERTY LINE

STREET

SIDE SETBACK

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FIGURE 20 C. 230 (E) (IV)

ii) *Facades facing interior property lines and interior portions of the lot, that are visible from the street, shall be modulated approximately every forty (40) feet, depending on unit separation location.*

iii) *Modulation Standards.*

a) *Minimum depth of modulation shall be four (4) feet.*

b) *When balconies are part of the modulation and have a minimum depth of six (6) feet and a minimum area of sixty (60) feet, the minimum depth of modulation shall be two (2) feet. See Figure 20C.10.230(G).*

*Figure 20C.10.230(G)
Reduction in Modulation
Depth for Balconies*

c) *The minimum width of modulation shall be five (5) feet.*

d) *The maximum width of modulation shall be thirty (30) feet.*

e) *The maximum width of a building shall be one hundred and twenty (120) feet.*

f) *The maximum depth of a building shall be 65% of the lot depth.*

(H) *Parking and Access.*

i) *Parking Quantity. Parking shall be required per Section 20C.20.150.*

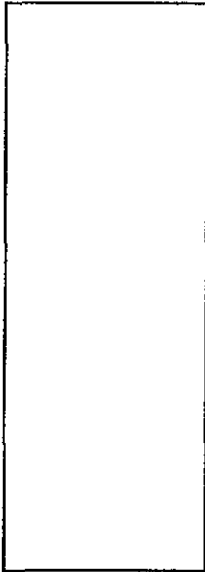
ii) *Access to Parking. Access to parking shall be provided via alley, where one exists or is determined to be feasible and desirable to mitigate parking access impacts by the Technical Committee through Site Plan Review.*

a) *Driveways and drive aisles should not running along interior property lines if possible. ~~are prohibited when at grade, except when direct alley access is required through rear or side yards abutting an alley.~~*

iii) *Location of Parking.*

a) *Required parking shall be on-site.*

b) *At grade parking is prohibited within required yard areas. Except developments with ground floor commercial uses and enclosed parking garages in and adjoining non-residential design areas shall be required to maintain a zero (0) foot setback in the front one-half of the lot. Open parking shall provide the minimum perimeter planting required in Section 20C.20.090 Landscaping and Natural Screening. (See figure below)*



design through such elements as parapet walls, false roofs or equipment rooms. Wood generally shall not be used. Louvered designs are acceptable if consistent with building design style.

ii) *Utility Meters.* All utility meters shall be fully screened from view from a public right-of-way. If enclosed in cabinets visible from public rights-of-way exterior surfaces shall be finished with material compatible and complimentary to the architecture of the building.

iii) *Parking.* Parking shall be screened from direct street view by the front facade of a structure, garage doors, fence or wall between five (5) and six (6) feet in height. When a fence or wall provides screening along a street, there shall also be a Type I or II landscape buffer in the yard abutting the street, depending on light and glare from within the parking area.

iv) *Trash Receptacles.* All garbage receptacles and recycling bins not located within parking garages shall be enclosed by a freestanding enclosure that is architecturally compatible with the building.

(J) *Landscaping.* All required yards and common usable open space areas shall be attractively landscaped. Additional landscaping requirements shall apply as follows:

(I) *Screening.*

i) All mechanical equipment, including air conditioners, heaters, vents and similar equipment, rooftop and grounded mounted, shall be fully screened from public view both at grade and from higher buildings.

a) The screening materials shall be of material requiring minimal maintenance.

b) For ground mounted equipment, landscaping may be used if a solid screen is provided at time of planting.

c) For roof top equipment all screening devices shall be well integrated into the architectural

i) *General.* The requirements specified in Section 20C.20.090 Landscaping and Natural Screening, 20C.30.020(40) Planting Design, and Appendix N - Landscape Standards, shall apply.

ii) All new development shall provide the following landscaping quantities in addition to the general the requirements. Additional material may be required through Site Plan Review.

a) One tree for each one hundred and twenty-five (125) square feet of required yard area, including front yards, side yards, rear yards and common usable open space; and

b) Three (3) shrubs of not less than five-gallon size shall be provided for each tree planted.

(K) *Density Bonus.* Density bonuses shall be granted to residential developments as follows:

i) *Semi- and Full-Subterranean Parking.* In free standing residential developments, a density bonus of 10% shall be granted to developments that provide semi- or full subterranean parking where the finish floor height of the first floor is not more than four (4) feet above the street curb.

ii) *Affordable Housing.* Density bonuses shall be granted per Section 20C.20.016, Affordable Housing.

iii) *Senior Housing.* Density bonuses shall be granted per Section 20C.20.235(70)(c) Senior Citizen Housing Development - Density Bonus.

20C.10.230(25) CITY CENTER LINKAGE PEDESTRIAN SYSTEM - The City Center Linkage Pedestrian System is a network of landscaped pedestrian walkways, vehicular lanes, and small greenbelts. It is established to provide safe pedestrian routes removed from traffic, enhance the appearance of buildings and their settings, provide a unified design element to offset varying architectural styles, and to soften the appearance of parking lots and service storage areas. Planting is intended to provide street trees and other vegetation appropriate for an urban setting.

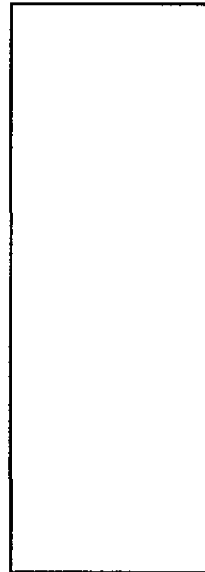
Where landscaping is required, Massing and aggregating of plantings to achieve a strong, positive statement is encouraged. Use of seasonal color and ease of maintenance are plant characteristics that should also be considered.

(a) Installation of Pedestrian Linkage System - The various components of the Linkage Pedestrian System shall be provided as noted on the map entitled, "City Center Linkage Pedestrian System" which is incorporated as a part of this section. As property is developed or redeveloped, corresponding portions of the system shall be installed or otherwise provided for by the property owner/developer. The mid-block segments shown on the map represent desired connections between blocks. In order to provide flexibility, the actual alignment shall be determined through the Site Plan Review Process.

(b) Linkage Pedestrian System Description

Type I - A 30-foot landscaped walkway which includes a 10-foot parkway planter for street trees, a 5-8 foot sidewalk and 15-22 feet of planting, when located on a street front.

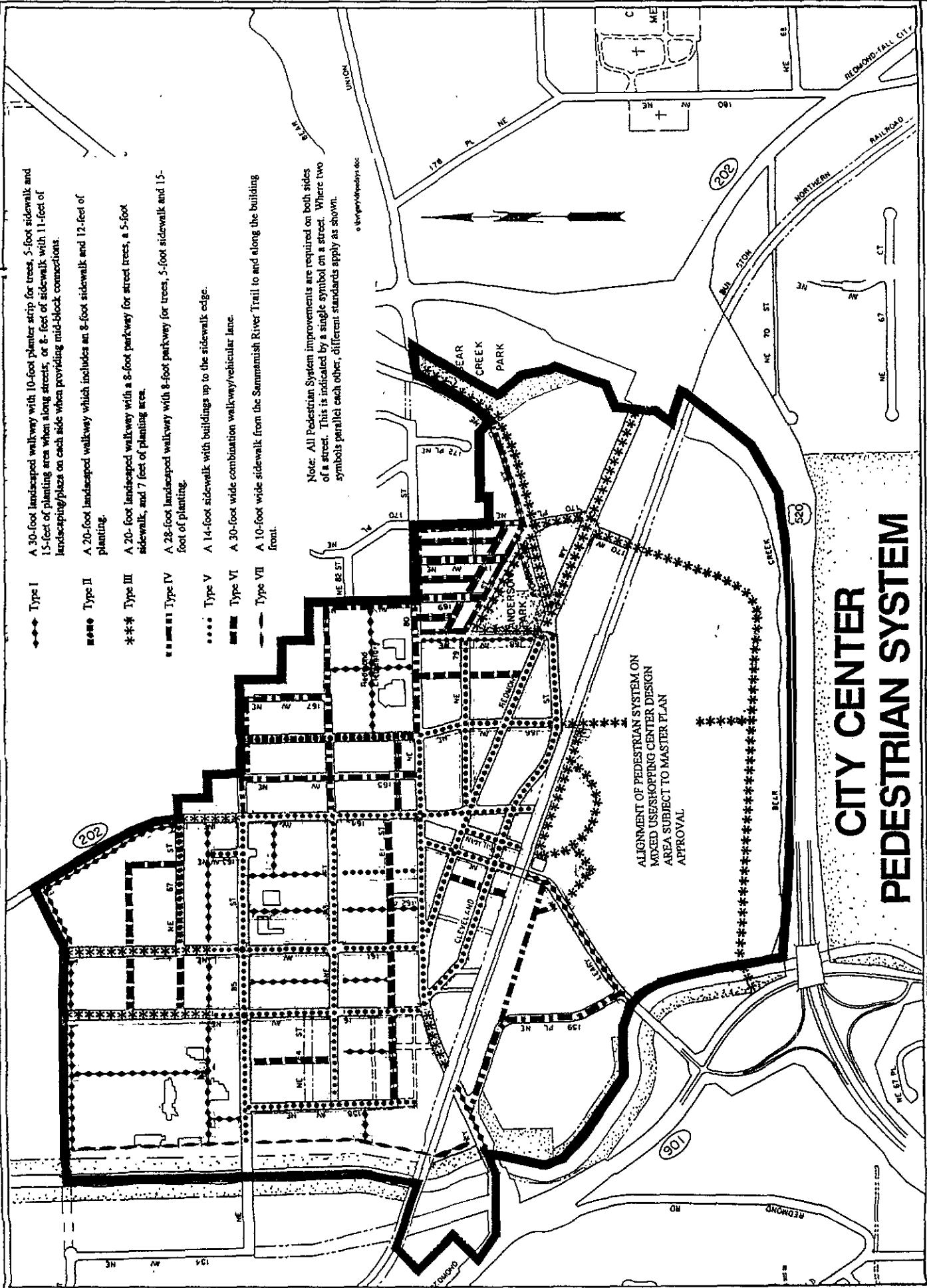
Internal block sections typically include an 8-foot wide sidewalk and 11-feet of landscaping on both sides. Where a mid-block connection is desired through the interior of a property, the landscaping buffer may be omitted through the Site Plan Review Process, provided that the walkway be a minimum of 12 feet in width and be safe and appealing for pedestrian use. Where store fronts open directly onto pedestrian ways, unit pavers or decorative pavement finishes are encouraged, as well as landscaping in planters and tree wells. Internal block sections may pass through buildings when providing access to pedestrian oriented uses within a ground floor arcade. Type I is most typically used for internal block linkages and entry arterials to City Center. Average width is 30 feet with a minimum width of 15 feet. Type I may be substituted by Type VI segments where found appropriate by the Technical Committee.



Type II - A 20-foot landscaped walkway which includes an 8-10-foot sidewalk, and 12-10 feet of planting and street trees in tree wells along the street.

- ◆◆◆ Type I
A 30-foot landscaped walkway with 10-foot planter strip for trees, 5-foot sidewalk and 15-foot of planting area when along streets, or 8-foot of sidewalk with 11-foot of landscaping/plaza on each side when providing mid-block connections.
- Type II
A 20-foot landscaped walkway which includes an 8-foot sidewalk and 12-foot of planting.
- *-*-* Type III
A 20-foot landscaped walkway with a 8-foot parkway for street trees, a 5-foot sidewalk, and 7 feet of planting area.
- Type IV
A 28-foot landscaped walkway with 8-foot parkway for trees, 5-foot sidewalk and 15-foot of planting.
- Type V
A 14-foot sidewalk with buildings up to the sidewalk edge.
- Type VI
A 30-foot wide combination walkway/vehicular lane.
- Type VII
A 10-foot wide sidewalk from the Sammamish River Trail to and along the building front.

Note: All Pedestrian System improvements are required on both sides of a street. This is indicated by a single symbol on a street. Where two symbols parallel each other, different standards apply as shown.

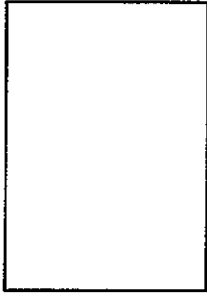


CITY CENTER PEDESTRIAN SYSTEM

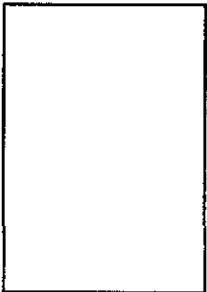
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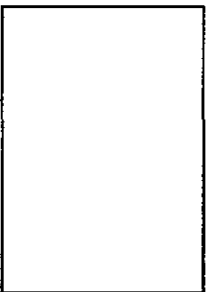
~~EXCEPTION There shall be a 20-foot landscaped walkway which includes a 10-foot sidewalk and 10 feet of planting on 164th Avenue N.E. south of N.E. 85th Street and on the Redmond Way/Cleveland Street Couplet. Type II is most typically used for streets and some internal block linkages used on a few collector streets. Average width is 20 feet with a minimum width of 10 feet.~~



Type III - A 20-foot landscaped walkway with a 8-foot parkway planter for street trees, a 5-foot sidewalk, and 7-foot planting area for site landscaping.



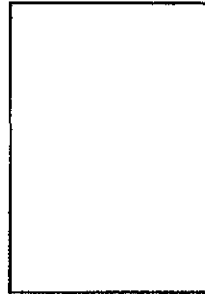
Type IV - A 28-foot landscaped walkway with a 8-foot parkway for street trees, a 5-foot side walk, and 15-feet of landscaped yard area. Type IV is most typically used in residential areas.



Type V - A 14-foot sidewalk with first floor of buildings up to the edge, and street trees in tree wells next to the street. Sidewalks may be wider than 14 feet, but shall not be

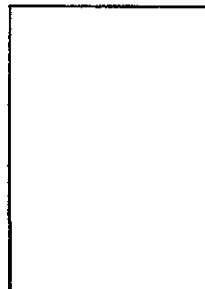


less than 8 feet in width. Pedestrian plazas, court yards, and arcades opening to the streets are encouraged.

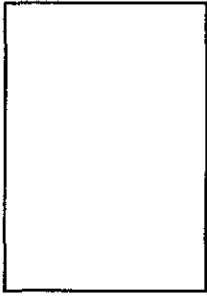


** Except Gilman Street which shall become a brick lined pedestrian/vehicular plaza, and Brown Street street which includes an 8.5 foot wide sidewalk.*

Type VI - A combination walkway/vehicular lane which incorporates landscaping, decorative lighting and seating areas. If the lane does not have a separated raised walkway six (6) feet in width, the surface must be paved with unit pavers to denote that it is a pedestrian area. The width of the lane shall provide for a minimum of one lane of vehicular travel (one way) and one parking lane. Most used to reduce the size of large blocks, provide vehicular access to rear of lots, minimize need for curb cuts on street fronts, and provide mid-block connections for pedestrians. Type VI may be substituted for Type I internal sections if the Technical Committee finds that a vehicular lane is not necessary to provide for the completion of an already started segment or circulation system.



Type VII - A 10-foot wide sidewalk from the Sammamish River Trail to and around buildings fronting the river.



SKETCH

(c) Easements/Dedications - Where a Linkage Pedestrian System walkway exists or is required outside of a public right-of-way, an easement ~~for~~ or the dedication to the City of Redmond may be required to provide continuity of the walk-way to adjoining property. In case of dedication, residential density shall be calculated based on pre-dedication lot area.

(d) Permitted Encroachments - Encroachments into Pedestrian systems are permitted as follows:

i) Buildings, marquees, awnings, and roof projections may extend over the pedestrian system when they enhance pedestrian activities and when:

- The encroachment is integrated into the linkage pedestrian system by providing a covered walkway plaza or it otherwise complements pedestrian activities;
- The required average and minimum width of the linkage pedestrian system is maintained. ~~; a ten-foot building setback is maintained from interior block property lines; except that no setback is required when adjoining properties are under common ownership or a written agreement is obtained from the adjacent property owner.~~

SKETCH

Buildings, marquees and roof projections may extend over Pedestrian systems when they enhance pedestrian activities.

Buildings may encroach into a Pedestrian system when compensation is provided at another location.

ii) *Patio walls no higher than four (4) feet for ground floor residential units may encroach into the pedestrian system a depth no greater than five (5) feet.*

iii) *Decorative walls or fences no higher than three (3) feet above the sidewalk, that are not for patios, may be permitted through Site Plan Review.*

(e) Width Measured from Curb - Where a Linkage Pedestrian System adjoins a public street, the system's width shall be measured from the edge of the existing or proposed curb. The widths specified shall be ~~required~~ averages. The minimum widths shall be 10 feet for streets requiring parkways (5 foot parkway and 5 foot sidewalk) unless otherwise specified.

(f) Construction Standards - Construction standards for sidewalks are identified in Appendix H, CITY CENTER LINKAGE PEDESTRIAN SYSTEM CONSTRUCTION SPECIFICATIONS.

(g) Driveway Crossings - Driveways crossing the Linkage Pedestrian System should be minimized and joint use of driveways encouraged to separate vehicles and pedestrians. Areas in driveways will not be calculated as part of the area required to be landscaped in the Linkage Pedestrian System.

(h) Access to Buildings - Pedestrian access from primary building to Linkage Pedestrian System ~~shall~~ *should* not be interrupted by vehicular circulation, parking or other elements which discourage pedestrian use.

(i) Interior Block Pedestrian Linkage System - Interruptions of mid-block pedestrian linkage systems by vehicular circulation or parking shall be minimized.

(j) Variations Exceeding Standards - Variations in the *pedestrian linkage* system which exceed the standards may be approved by the Technical Committee. The variation, in the judgment of the Technical Committee, must not create a *linkage pedestrian* system out of character and harmony with the surrounding *pedestrian linkage* systems.

(k) Variations not meeting standards - Variations in the *pedestrian linkage* system which do not meet minimum standards may be approved by the Technical Committee. Variations may be allowed after consideration of factors, including, but not limited to:

- existing right-of-way available to meet standards
- existing buildings encroaching in linkage area
- pedestrian and vehicular volumes anticipated
- existing vegetation
- disruption of system continuity
- accessibility to buildings

~~(l) Old Town - The area identified on City Center Linkage System Map as "Old Town" requires the sidewalk component only.~~

~~(m) Walkway Separation - On heavily traveled roadways without on-street parking or bike lanes to protect pedestrians, planting areas should be used to separate the walkways from the curb.~~

20C.10.230(30) CITY CENTER STREET TREE PLAN - Street trees within the City Center Neighborhood shall be provided as noted on the map entitled "City Center Street Tree Plan" which is incorporated as Exhibit "B" in Appendix N. As property is developed or redeveloped, trees shall be installed or otherwise provided for by the property owner/developer. For streets which do not list tree types or spacing requirements, refer to City of Redmond Street Tree Plan.

(A) Location - Trees shall be spaced on average as noted on the City Center Street Tree Plan. Trees shall be planted in parkways where they exist or are required per Section 20C.10.230(25). Where sidewalks are required to be contiguous with street

curbs, trees shall be planted in irrigated tree wells, with City approved root barriers, next to the street. Street trees may be grouped in larger planters near the curb, if found more appropriate through Site Plan Review. Street trees disallowed due to inadequate parkway width, street furniture, driveways or utilities shall be planted in the abutting yard area.

(B) *Street trees shall be planted according to guidelines outlined in Appendix N, Landscape Standards.*

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DOWNTOWN PLAN 11/19/9320C.20

ADD THE FOLLOWING

20C.20.025 AUTOMOBILE AND BOAT SALES

All automobile and boat sales uses, both new and used, within the River Bend Design Area of the City Center Neighborhood shall be subject to the following conditions and criteria:

* (a) The vehicle display area shall be limited to that area on site remaining after compliance with all parking and landscape requirements. Outdoor display and storage areas is prohibited between buildings and streets.

(b). On-site parking shall be provided at a ratio of one space per 5,000 sq. ft. of outdoor vehicle display area, plus one space per 1,000 sq. ft. of gross floor area, and three stalls per service bay.

(c) In order to minimize impacts to adjacent residential uses, outdoor vehicle display and storage areas shall be screened on property lines adjoining residentially zoned areas by a Type I solid landscape buffer, in planters not less than 15 feet in width. (See Appendix N, Landscape Standards).

(d) Outdoor storage lot lighting shall be fully shielded to prevent light intrusion onto adjoining and adjacent properties.

(e) No repair shall be allowed outdoors.

(f) No bay doors shall orient directly toward (without buildings between) the nearest residential district or public open space.

(h). There shall be at least one car washing area which drains to the sewer through a METRO approved vault oil separator.

(i). Driveway access to principal arterial streets is discouraged; access shall be via minor arterial or collector street whenever possible.

(j) Prohibition of all illegal signs including, but not limited to: animated signs, off-premise and portable signs, signs within the right-of-way, and streamers and pennants. All signs and display advertising on-site shall comply with the Redmond Community Development Guide, Section 20C.20.230 Signs and Street Graphics.

ADD THE FOLLOWING

20C.20.027 BANKS WITH DRIVE-THROUGH FACILITIES

Banks with drive-through facilities that are in districts requiring General Development Permits for such use shall be subject to review as outlined in Section 20F.20.170, General Development Permit, and conditions as outlined below. The administrative staff, Hearing Examiner, or Council may impose additional conditions that are deemed necessary in granting the permit to maintain consistency with a safe and attractive pedestrian environment.

(a) Drive-through facilities shall be allowed only as part of a multi-story building.

(b) Exit driveways serving drive-through lanes shall not exceed 11 feet in width within 20 feet of the street curb.

(c) A minimum of 120 feet of vehicle queuing space shall be provided which is not within the public right-of-way, on-site vehicular circulation aisles, or between the building and street.

(d) The drive-through lanes shall be screened by Type II landscape plantings or equivalent.

(e) Buildings shall be attractively designed, incorporating rich and traditional building materials including brick, masonry, tile, or marble. Roofs shall be well articulated and street front facades shall be pedestrian oriented and scaled.

AMEND SECTION 20C.20.090, LANDSCAPING AND NATURAL SCREENING AS FOLLOWS:

Part of Sec. 20C.20.090(30)(b) Landscape Area Requirements

Table 1. Vehicle Use Areas

Interior Landscaping	0 - 6000 sq. ft.	6000 - 30,000 sq. ft.	30,000 sq. ft. +
Area required per 100 feet of vehicle use area	0	5 sq. ft.	7 sq. ft.
Maximum contiguous landscape area (except perimeter)	350 sq. ft.	50 sq. ft.	1,500 sq. ft.
Minimum area of landscaping	64 sq. ft.	64 sq. ft.	100 sq. ft.
Perimeter Landscaping	Average Width from Property Line Street Frontage Interior Lot Line		
Parking Spaces:			
0 - 100	5'	5'	
100 - 499	10'	5'	
500 - 1000	15' (10')*	10'	
1000+	20' (10')*	10'	
<i>*PLANTER WIDTH MAY BE REDUCED WITH PROVISION OF 4 FOOT HIGH FENCE OR HEDGE BETWEEN PARKING LOT AND STREETSIDE PLANTER, SUBJECT TO REVIEW AND APPROVAL BY THE DESIGN REVIEW BOARD.</i>			
Other Requirements 1. Parked vehicles may overhang landscaped areas up to 2.5 feet when wheelstops are provided. 2. Trees shall be planted in vehicle use landscape areas at a minimum of one per 150' of landscape area <i>four parking stalls. See illustrations below.</i>			

(b) Landscape Area Requirements -

- Multi-family residential:
R-8, R-12 - 50%
R-20, R-30 - 50%
of the site shall be landscaped.
- Business (CO, CB, NB, CG): 25% of the site shall be landscaped.
- Business Park (BP): 22% for sites less than one acre in size; 20% for sites one acre or larger in size.
- Industrial (LI, HI): 20% of the site for lots less than one acre in size 18% for sites one acre or larger in size.
- City Center ~~Linkage~~ Pedestrian System: ~~Linkage~~ Pedestrian System landscaping requirements (Sec. 20C.10.200(15)230(25) are the landscape area requirements in the City Center.

AMEND SECTION 20C.20.150, SPACES FOR SPECIFIC USES TABLE AS FOLLOWS:

SPACES FOR SPECIFIC LAND USES	
LAND USE	MINIMUM PARKING REQUIRED SPACES
Assembly, including auditoriums, theaters and banquet rooms	10/1000 sq.ft. of gfa for assembly or 1/5 fixed seats
Bowling	5/lane
Hotel, motel, excluding restaurant and assembly	1/rental room
Multi-family housing designed for senior citizens	.5 to 2/du as found adequate by the Code Administrator
Multi-family housing in business districts	See requirement for Urban Residence and Multiple Residence Districts
Multi-family housing in City Center	See requirements for Residential uses in City Center
Nursing home	1/4 patient beds
Restaurant: Sit down Take out	9/1000 sq.ft.gfa* 10/1000 sq.ft.gfa*
	<i>*In City Center Sammamish Trail, and Old Town Design Areas use Design Area parking requirements per Table 20C.20.150(10)</i>
Retirement Residence	A minimum of 0.50 to 1.0 parking spaces are required per suite, as determined adequate by the Code Administrator
Schools, hospitals, institutions, public facilities and similar uses	The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc.
Notes: du - Dwelling Unit gfa - Gross Floor Area	

AMEND TABLE 20C.20.140(10) AS FOLLOWS

A Part of Subsection 20C.20.140(10) Requirements for Outdoor Storage				
ZONE	PERMITTED STORAGE	REQUIREMENTS	AREA	SCREENING
City Center	Public Street Furniture Sidewalk Restaurants Seasonal Items Special Public Event <i>Auto & Boat Sales Display *</i>	<i>Per 20C.20.025*</i>		<i>Per 20C.20.025*</i> *
CB	Bulk Storage of more than 3 days with approval by Technical Committee except vehicle storage in CC	Maximum height and width of four (4) feet	As Defined in 20C.20.140(35)	May be required by Technical Committee
	Non-Bulk Storage	Non-Bulk must be stored less than 24 hours		(non-bulk exempt)
GC	Public Street Furniture Sidewalk Restaurants Seasonal Items Special Public Events			
	Bulk Storage of more than 3 days with approval by Technical Committee including vehicle storage	Maximum Height ten (10) feet		Required as specified in 20C.20.140(20) & from streets & parks
	Non-Bulk Storage	Non-Bulk must be stored less than 24 hours		(non-bulk exempt)
A, BP, LI, and HI	All types	Maximum Height twenty (20) feet		Required as in 20C.20.140(20)
NB and R	Recreational vehicles as provided in Section 20C.20.150(25) Garden Materials & equipment			

AMEND REQUIRED OFF-STREET PARKING TABLE 20C.20.150(10) AS FOLLOWS:

REQUIRED OFF-STREET PARKING

ZONING DISTRICT		Number of Parking spaces on-site	
		Minimum Required	Maximum Allowed
General Agriculture Ranch Estates Residential Estates Suburban Estates Suburban Residence	G A RE R1 R-2-R-3 R-4-R-5-R-6	2/du	NS
Urban Residence Multiple Residence	R-8-R-12 R-20-R-30	1.2/Studio du 1.5/1 Bedroom du# 1.8/2 Bedroom du# 2.0/3+ Bedroom du#	NS
Professional Office	PO	3.5/1000 sq. ft. gfa	4.5/1000 sq. ft. gfa
Neighborhood Business	NB	4/1000 sq. ft. gfa	5/1000 sq. ft. gfa
Commercial Office	CO	3.5/1000 sq. ft. gfa	4.5/1000 sq. ft. gfa
Community Business	CB	4/1000 sq. ft. gfa	5/1000 sq. ft. gfa
General Commerce	GC	*	
City Center	CC		
<i>Residential Uses (All)</i>		<i>1.0/DU****</i>	<i>NS</i>
<i>Bear Creek, Valley View, and Trestle</i>		3.5/1000 sq. ft. gfa	5.5
<i>Old Town</i> 1,2		2.0/1000 sq. ft. gfa	2.0/1000 sq. ft. gfa***
<i>Leary</i> 1,2		2.0/1000 sq. ft. gfa	3.5/1000 sq. ft. gfa
<i>Foot Hill</i> 1,2		2.0/1000 sq. ft. gfa	3.5/1000 sq. ft. gfa
<i>East Hill</i> 1,2		2.0/1000 sq. ft. gfa	3.5/1000 sq. ft. gfa
<i>Sammamish Trail</i> 1,2		2.0/1000 sq. ft. gfa	3.5/1000 sq. ft. gfa
<i>Town Square</i> 1,2		2.0/1000 sq. ft. gfa	3.5/1000 sq. ft. gfa
<i>River Bend</i> 1,2		2.0/1000 sq. ft. gfa	3.5/1000 sq. ft. gfa
<i>Mixed Use Shopping Center</i>		3.5/1000 sq. ft. gfa	5.0/1000 sq. ft. gfa
Business Park	BP	2.0/1000 sq. ft. gfa	3/1000 sq. ft. gfa**
Light Industry	LI	2.0/1000 sq. ft. gfa	3/1000 sq. ft. gfa**
Heavy Industry	HI	2.0/1000 sq. ft. gfa	3/1000 sq. ft. gfa**
Planned Unit Developments	PD	2.0/1000 sq. ft. gfa	3/1000 sq. ft. gfa**

Notes: du - Dwelling Unit
gfa - Gross Floor Area
NS - No Specification

- * - The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc.
- ** - The Technical Committee may consider parking at a ratio as low as 1.5/100 if a covenant is recorded with the property which limits the uses to warehouse uses and/or limits the number of employees permitted in a building or project. Parking at ratios greater than 3.0/1000 (not to exceed 3.5/1000) is generally not permitted unless the employer/building owner can document that single occupancy vehicle trips can be reduced better through the employer/building owner's parking/traffic mitigation program than they would be reduced through limiting parking stalls to 3.0/1000.

*** - Bonus available for in-lieu parking fund per Section 20C.20.150(20)(b).

**** - Plus 1 guest space per 4 units for projects with 6 units or more.

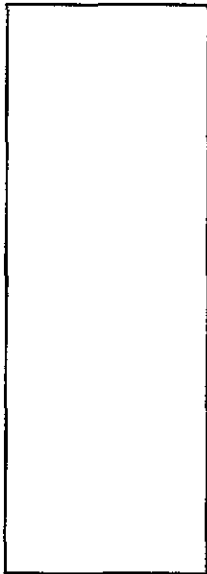
- Bedroom shall include all rooms that can be used as permanent sleeping quarters.

1 - The maximum number of parking stalls allowed may be increased to 5.0 stalls per 1000 sq. ft. GFA for the retail components of mixed-use developments.

2 - Developments may be allowed up to 5 stalls per 1,000 sq. ft. GFA provided: The excess parking is within a parking structure, or off-site within 300 feet of the use, or on the same site but in a location conducive to redevelopment or conversion to a commercial parking lot; and a portion of the parking is made available to the general public. The parking lot may be operated as a commercial lot. This provision shall sunset February 1, 1999, unless extended by ordinance.

AMEND SECTION 20C.20.150(15)(d) and (j) AS FOLLOWS:

(d) Vehicle Circulation Between Adjoining Property Required - Parking lots shall be designed to provide for off-street vehicle circulation to adjoining property and parking areas where physically feasible, except that driveways and parking aisles may not cross ~~Type I and II interior Pedestrian~~ walkways within ~~100~~ 75 feet of a street front in the City Center; see Section 20C.10.230(25), "City Center Linkage Pedestrian System." The Technical Committee may, at their discretion, modify the minimum separation between a vehicular crossing and the street through the Site Plan Review process. See figure below.



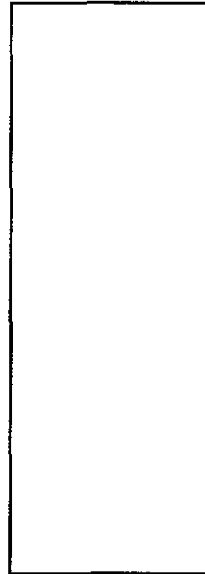
(j) Pedestrian Access from Parking to Major Walkway City Center Pedestrian System - Convenient, marked pedestrian access shall be provided from parking areas to ~~Type I and II interior~~ and street front Pedestrian walkways, and the Sammamish River Trail where appropriate, as determined by the Technical Committee.

ADD 20C.20.150(15)(o) AS FOLLOWS:

(o) Parking Area Location and Design criteria In the City Center Neighborhood - Parking areas within the City Center Neighborhood shall meet the following criteria regarding the City Center Pedestrian System 20C.20.230(25):

i) General - On Type V Pedestrian walkways per the City Center Pedestrian System Map 20C.10.230(25), parking lots shall not be located between the street and the building. Parking lots and ground floor

parking garages shall be separated from streets by building areas (excluding vehicle access points) which are developed as, and made available for, pedestrian-oriented businesses. See figure below.



ii) Surface Parking Lots On One Type V Pedestrian Walkway - If the subject property abuts a Type V Pedestrian walkway, the maximum width of the parking lot parallel to and within 20 feet of the Type V Pedestrian walkway may not exceed the lesser of 61 feet or 75% of the lot frontage. See Figure 20C.20.150(15) (o) (ii). Except, public and quasi-public parking lots may occupy 100 percent of the lot frontage, excluding perimeter landscaping.

Figure 20C.20.150(15) (o) (ii)

iii) *Surface Parking Lots On Two Type V Pedestrian Walkways* - If the subject property abuts two or more Type V Pedestrian walkways, the following regulations apply:

a) *The parking lot may be within 20 feet of only one of the Type V Pedestrian walkways as determined by the Design Review Board through Site Plan Review. See Figure 20C.20.150(15) (o) (iii). Except, public and quasi-public parking lots may be within 20 feet of two or more Pedestrian walkways as determined by the Design Review Board.*

Figure 20C.20.150(15) (o) (iii)

b) *The maximum width of the parking lot within 20 feet of a Type V Pedestrian walkway may not exceed the lesser of 61 feet or 75% of the site frontage, if possible. Except, public and quasi-public parking lots may occupy 100 percent of the lot frontage, excluding perimeter landscaping.*

iv) *Surface Parking Lots On Streets With Other Than Type V Pedestrian Walkways* - Surface parking lots on streets with other than Type V Pedestrian walkways may occupy no more than 45 percent of the lot width, up to a maximum of 130 feet. Except, public and quasi-public parking lots may occupy 100 percent of the lot frontage, excluding perimeter landscaping.

AMEND SECTION 20C.20.150(20)(b) IN-LIEU PARKING FEES AS FOLLOWS:

(b) *In-lieu Parking Fees - Fund Created - Comprehensive Parking Plan* - An in-lieu parking fee may be submitted to the City for each *required* parking space which ~~cannot be~~ *is not* provided on-site. The in-lieu parking fee shall be determined annually by the Technical Committee based on current land and construction costs.

There is hereby created a special fund within the Office of the Treasurer-Comptroller into which in-lieu fees shall be deposited to be used only for the construction of public parking facilities. Priorities for construction of parking facilities shall be identified in a comprehensive parking plan and capital improvements program approved by the City Council. The plan shall take into consideration the amount of available on-street parking within an area, the need for concentration of public facilities to prevent proliferation of private parking lots alternating with buildings, the visual and traffic impacts of parking facilities and the degree to which the parking facilities will encourage pedestrian circulation.

(i) *Floor Area Bonus* - A floor area bonus shall be granted within the Old Town Design Area for contribution to the fund. The bonus shall be granted at a ratio of three square feet of bonus floor area for each square foot of structured parking paid for by the in-lieu fee, up to the maximum allowable floor area within the permitted building envelope. The bonus floor area shall be exempt from parking requirements. For example: A 1,000 square foot building requiring 2 parking spaces would pay for the construction of two structured parking stalls, at 325 square feet per stall. The bonus floor area granted would be three times the two stalls (3 x 650 square feet), or 1,950 square feet of additional floor area.

AMEND SECTION 20C.20.150(20)(d) AS FOLLOWS TO CORRECT REFERENCE ERROR:

(d) *Parking in Building Setback Areas* - In all residential zoning districts and *design areas*, parking other than in driveways is not permitted in front setback areas. In all other zoning districts, parking is permitted in all setback areas subject to the requirements of the district and the following paragraph (f)(e) of this subsection "Parking Restricted in Shoreline areas."

AMEND SECTION 20C.20.235(70)(c) AS
FOLLOWS:

Senior Citizen Housing Developments - Density Bonus - Senior Citizen housing developments (which may also include some units allocated for disabled persons as defined in Title 24 Code of Federal Regulations Part 8:12.2(a)), as the same now exists or may hereafter be amended, may exceed the maximum density requirements in the City Center subject to the following:

1. ~~The applicant must obtain Special Development Permit approval pursuant to Section 20F.20.160;~~

2 1. The age limit for occupants of the designated senior citizen units must be 62 years of age or older;

3 2. The project must be sponsored, supported, proposed or underwritten by a government agency or private developer whose intention is to provide housing for senior citizens of ~~low to moderate income~~ and commits the property to such use;

4 3. If monthly rents of the residential units being proposed are within the limits of fair market rents as established by the King County Housing Authority, no senior citizen 62 years of age or older with a valid section 8 certificate as provided for in the Housing and Community Development Act of 1974, may be denied occupancy solely on the basis of section 8 certificate rental request;

5 4. The Final Approval Order shall be recorded as a covenant appearing on the deed to the property;

6 5 No conversion of occupancy to non-senior citizens shall be allowed without first obtaining a Special Development Permit pursuant to Section 20F.20.160;

7 6. The bonus density increase *for the provision of market rent units* shall in no case exceed ~~50 units per acre~~ 15% of the maximum permitted density for the site in the City Center. ~~The actual density bonus shall be determined based upon but not limited to the following factors:~~

n ~~Demonstration by the applicant of the demand for housing specifically for senior citizens of low to moderate income;~~

n ~~Demonstration by the applicant of how the proposed project will benefit the housing demand for senior citizens of low to moderate income;~~

n ~~Reasonable demonstration that the project will remain as a senior citizen housing facility for a minimum specified time period;~~

n ~~The proposed size of the units and number of bedrooms;~~

n ~~Proximity to services such as mass transit and commercial areas;~~

n ~~Impacts of the development and impacts upon the development including but not limited to: traffic generation, noise levels, building bulk, design and orientation and adjacent land uses;~~

n ~~Ability of the site to adequately accommodate the proposed density.~~

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DOWNTOWN PLAN 11/19/93

20C.30 DESIGN CRITERIA

20C.30.010 PURPOSE

Section 20C.30 DESIGN CRITERIA is established for the following purposes:

30.010(05) To promote development and construction consistent with goals, policies, plans and regulations of the Community Development Guide.

30.010(10) To supplement land use regulations which encourage and promote public health, safety and welfare of the citizens of the City.

30.010(15) To provide guidance to urban design decisions that will promote development of high environmental and visual quality throughout the City.

30.010(20) To assist applicants in the preparation of development applications.

30.010(25) To assist decision-making by the Technical Committee, Design Review Board, Hearing Examiner, Planning Commission and City Council in the review of development applications.

30.010(30) To assist preservation of natural landforms, vegetation, water features, and scenic views and vistas which form the strong sense of natural amenity existing throughout the City.

30.010(35) To recognize major environmental, economic and social opportunities and problems concerning community development.

30.010(40) To provide greater clarity and to enhance the site plan review process of the Community Development Guide.

AMEND AS FOLLOWS

20C.30.020 URBAN DESIGN CRITERIA

30.020(05) Purpose/Scope - The purpose of this section is to establish design criteria for urban design, natural features, ~~pedestrian-linkage~~/circulation, site design, building design, planting design, and signs and street graphics to guide preparation and review of all development applications. Additional design criteria noted in sections following are supplementary to the criteria of this section. The manner in which these design criteria are evaluated and implemented

by development applications determines in large part the urban design of the Redmond community. It is intended these design criteria will assist development applications in adhering to the desired form of urban design in Redmond as expressed by goals, policies, plans and regulations of the Community Development Guide.

30.020(10) Design Concept - Development design should be based upon a well-defined concept. The design concept should evaluate how a development relates to criteria for urban design, natural features, ~~pedestrian linkage~~/circulation, site, building and planting design, and signs and street graphics. Additionally, the orientation of development design towards a user/client group should be clearly identified to direct consideration of the design approach chosen and to assist analysis of alternative solutions should issues of design arise.

The design concept may require balancing of criteria of the user/client group with design criteria of this section to result in a development which is harmonious when viewed by its parts, and when viewed in context with natural features and the character of development of the area in which the project is proposed to be located.

30.020(15) General Urban Design - Urban design criteria involve consideration of how all elements of development design relate to a desired pattern of development as expressed by the Community Development Guide. The focus of paragraph (15), GENERAL URBAN DESIGN, is upon site design, which concerns placement of development elements within a site and their relationship to adjacent development. No one style of design is encouraged or discouraged by this set of criteria, as it is recognized that the styles of design, building construction materials and methods will continue to evolve over time.

(a) Function

1. Coordinate proposed development with current site planning and development efforts in adjoining areas to take advantage of opportunities to mutually improve development design.
2. Properly link proposed development to walkway, trail, street drainage and utility systems in the surrounding area. Assure efficient continuation of such systems to adjacent properties.

3. Provide intensively landscaped buffers where commercial or industrial uses abut residential or recreational uses as needed to minimize visual impact and other potential nuisances. Provide walls or berms when landscape materials are inadequate to minimize impacts.
4. Provide lower profile buildings with four-sided architectural treatment and with a mass and scale harmonious with surrounding development where structures adjoin a buffer.
5. Screen parking and service areas, rooftop equipment, garbage dumpsters, and outdoor storage to minimize negative visual impacts of these site elements.
6. Design screening with consideration of views from adjoining hillsides and from other areas of high public visibility, with special consideration for views from SR 520, Redmond Way, other major arterials, Marymoor Park and the Sammamish River Trail.
7. Design and select landscaping and structural materials of sufficient size, quantity and height to effectively screen and to be compatible with associated buildings.
8. Use mechanical penthouse structures when appropriate to the scale and visibility of rooftop mechanical equipment to accomplish required screening. Multi-tenant buildings should be designed with consideration for coordinating the location of rooftop mechanical equipment so as to minimize required screening. Use of parapet walls of sufficient height to satisfy screening requirements is encouraged for multi-tenant buildings.

(b) Design

1. Design a proposed development in context with its surroundings. Provide visual linkages between existing building, site and landscaping design so as to create a cohesive overall effect.
2. Strengthen and enhance positive qualities of existing natural and built environments through design of proposed development. Respect the scale and character of nearby

development of merit to contribute to an overall impression of unity.

3. Develop visual linkages among adjacent development to create a cohesive visual effect through consideration of the following design elements:

A. Site Design

1. building setback
2. massing of structures
3. location of pedestrian/vehicular facilities
4. spacing from adjoining buildings

B. Planting Design

1. composition of plant materials
2. type and quantity of plant materials
3. street trees

C. Building Design

1. scale
2. massing
3. proportion of windows, doorways and other features
4. roof silhouette
5. facade proportions and directionality
6. location of entryway
7. surface material, finish, color and texture surrounding development
8. style of architecture

D. Signs

1. number, size and location of signs
2. type of illumination
3. Avoid monotony of design in multiple building projects by creating visual interest and pleasing form through variation in detail, form and siting of building, landscaping, parking and access elements within a site.

30.020(20) Natural Features

(a) Function

1. Control potential erosion, slope stability and drainage problems by conforming grading, buildings and other impervious surfaces to existing topography of the site and natural drainage systems, and by minimizing grading, filling and the use of retaining walls.

2. Incorporate the storm drainage system into the natural drainage pattern wherever possible to minimize impacts to water quality, to facilitate ground water recharge, and to retain existing vegetation when feasible.

(b) Design

3. Preserve and enhance natural features to the advantage of development by creating a desirable environment for tenants, employees, or residents. Design the building and site in a form which provides visual access and, where appropriate to the sensitivity of natural features, physical access to attractive natural features of the site.
4. Provide for compatibility of buildings and site design with significant natural features by not crowding natural features to be preserved, and by providing an informal development edge in character with adjoining areas of natural features being preserved.

30.020(25) Linkage Pedestrian/Circulation

(a) Function

1. Enhance site access and traffic flow by linking walkways, bikeways, driveways and parking areas to adjoining development and to public walkways and transit systems.
2. Provide highly visible and direct *bicycle and* pedestrian and vehicular entrances.
3. Provide for access needs of the handicapped.
4. Encourage pedestrian use by:
 - A. reducing walkway distances and providing short cuts;
 - B. eliminating pedestrian barriers and obstacles;
 - C. leveling walkway routes;
 - D. assuring continuity of walkways;
 - E. providing protection from wind and rain, especially at main building entrances and over public walkways;
 - F. separating walkways from driveways to reduce perceived fears about safety;
 - G. increasing character and visual diversity of walkways;
 - H. providing pedestrian amenities such as benches, planters, perching walls, plazas,

drinking fountains, water features and waste receptacles;

- I. providing *safe* walkways *through* in parking areas *which* leading to destination points; ~~which have curbs or other barriers to prevent vehicle intrusion;~~
 - J. providing sufficient walkway widths to allow easy flow of pedestrian traffic;
 - K. providing adequate lighting where nighttime use is occurring or is to be encouraged.
 - L. providing convenient and direct pedestrian access from public walkways to primary building entrances.
5. Improve the safety and appearance of walkways when crossing parking and driveway areas by varying paving materials, texture and color of walkways.
 6. Assist distribution of varying volumes of pedestrian traffic to its destination by providing a hierarchy of walkway widths.

(b) Design

1. Make use of pleasing views and existing site features in determining walkway and driveway alignment.
2. Enhance the pleasure of pedestrians by providing variety and interest in walkway design.
3. Use planters, rails, retaining walls and other raised surfaces for seating. Any height between 12 and 24 inches will work, with 16 inches being best. A width of a minimum of 6 inches, up to 24 inches, should be provided.
4. Strive to locate seating where sun exposure is available from late morning to late afternoon, where views can be taken advantage of, and near to activity centers of a site such as at building entrances and at the intersection of walkways.
5. Cluster some seating so groups of people can use the seating while talking together.
6. Consider the use of movable seating where appropriate to encourage social interaction.

30.020(30) Site Design

(a) Function

1. Orient buildings to the street in a manner which provides easily identifiable and accessible pedestrian entry ways.
 2. Locate parking where possible behind buildings and away from areas of high public visibility.
 3. Integrate parking area design with landscape design in a way that reduces the visual impact of impervious surfaces and provides screening of parking from public view. Parking areas should provide for landscaping next to buildings and alongside walkways.
 4. Minimize pavement areas necessary for vehicular use, and avoid the use of parking aisles with parking located only along one side.
 5. Provide transit facilities where appropriate to the intensity of use.
 6. Locate services and outdoor storage areas, utility vaults and waste receptacles away from areas of high visual exposure, and integrate required screening with site, landscaping and building design. Locate waste receptacles in areas convenient for on-site use and accessible for collection.
 7. Locate utilities with consideration of utilizing existing easements and rights-of-way, providing access for City and private maintenance, avoidance of significant vegetation and other natural features being preserved.
 8. Design grades of walkways, driveways and parking areas to be generally less than 5%; grades of up to 10% are acceptable for short distances.
2. Incorporation of special design features and site amenities to highlight a project in the community is encouraged. Such features and amenities could include plazas, open space areas, employee lunch and recreational areas, architectural focal points and accent lighting.
 3. Highlight building, walkway and driveway entrances to a project through landscape design.
 4. Locate landscape buffers along building margins where vehicle use areas abut.
 5. Cluster interior parking lot landscaping when possible to conserve significant portions of existing tree cover as an amenity to the site. Disperse interior parking lot landscaping throughout a parking lot when no significant existing vegetation exists.

30.020(35) Building Design

(a) Function

1. Encourage high-quality building design that is functional, providing maximum efficiency for occupants while presenting a human scale and an attractive appearance.
2. Base building design on a unified design concept when construction will be in phases.
3. Use building materials and building techniques of durability and high quality.

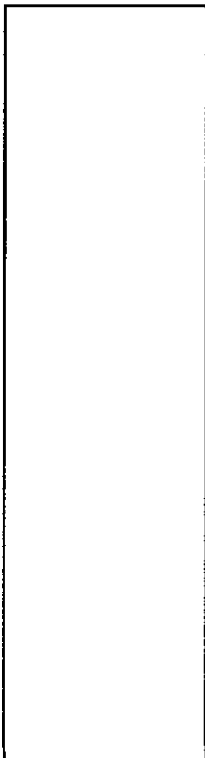
(b) Design

1. Vary building facades, rooflines, surface textures, and architectural design throughout the site to create design interest in multiple-building residential projects.
2. Use a basic building form and shape which is attractive and well-proportioned, with details of the design well-integrated and compatible with building form.
3. Provide design interest in balance with maintaining simplicity of building design; building form should be free of confusing, cluttering or unnecessarily complex elements.

(b) Design

1. Site buildings to create an interesting and attractive appearance at the streetscape and along building setbacks with significant visibility. Focus on varying the spaces among groups of buildings, especially for residential projects, to avoid creating a walled corridor of long, unbroken rows of buildings along a setback, or a monotonous pattern of buildings across a site.

4. Use ~~the same~~ complimentary building materials on all building elevations exposed to public view.
5. Interrupt the sequence of repetitive building elements, especially doorways and windows, at recurring intervals to provide variety and to avoid total repetition, which usually is monotonous.
6. Avoid the use of false or ostentatious building elevations and large, blank walls.
7. Coordinate differing surface textures to be compatible with one another.
8. Minimize the potential for glare created by windows or smooth, shiny materials.
9. Select muted colors for use on large building surfaces.
10. Use accent colors in a way to enhance or highlight building design, and not in a manner which creates clutter or otherwise detracts from building design.
11. Provide tripartite building articulation (building base, middle, and top) to provide pedestrian scale and architectural interest. See Figures below.



12. Strong vertical and horizontal reveals; offsets, and three-dimensional detail between surface planes should be incorporated into building design to create shadow lines and break up flat surfaces. Provide a clear and strong division between floors on multi-story buildings. *

13. A unified cornice/beltcourse treatment should be provided on buildings fronting Redmond Way, between the rail road bridge and Anderson Park, to create a visual connection between them and buildings in the Old Town Design Area. The cornice/beltcourse treatment should be applied between the third and fourth floor, generally, or along the roof line of a three story flat roofed building.

14. Blank walls at street level should not be greater than 30 feet in length for office uses and 15 feet in length for retail uses.

15. Roofs should provide a variety of vertical dimensions. Multi-planed and intersecting rooflines are encouraged. Flat roofed designs shall include architecture/details such as cornices, decorative facings, and arches to provide interest to the roof line. *

30.020(40) Planting Design

Planting design is an integral part of the overall site and community design and should complement the architecture, other site elements and the visual appearance of the neighborhood. The landscape plan should be based on a well-defined concept addressing criteria for function, design, horticulture, maintenance and irrigation. The concept should be consistent with the architectural character and overall site design and should be harmonious with and enhance the visual appearance and natural amenities of the area in which the project is located.

(a) Function

In the overall plant composition, the following functional criteria should be incorporated as appropriate to the individual project and the area in which it is located. Some of these functions may be accomplished through architectural or other site elements such as walls, berms, arbors, fences and plazas, either alone or in combination with planting. These other site elements should be integrated effectively

with the planting and shown on the landscape plans.

1. **Environmental Determinants:** Provide planting to reduce erosion, control storm-water runoff; encourage soil infiltration; maintain or improve air quality; reduce perception of noise; provide wildlife habitat; maintain solar access; conserve energy; reduce heat, glare and reflection; restore areas disturbed by construction; enhance microclimates for people and plants, and to protect and enhance watercourses.
2. **Retention of Existing Vegetation:** Preserve as much desirable vegetation as possible, with preference given to plant groupings.
3. **View Preservation and Enhancement:** Retain and improve desirable views such as mountains, valleys, lakes, watercourses, skylines, pastures and woodlands, parks and open space, and quality urban environments.
4. **Usable Open Space:** Provide space on site for active and/or passive recreational purposes.
5. **Buffer:** Provide separation between incompatible adjacent land uses or activities, and reduce the impact of wind and perception of noise.
6. **Transition:** Provide plantings that effectively accommodate a change in design between adjacent sites, within a site and from native vegetation areas. Also, design planting to create an effective change from public to private space and from the vertical to horizontal plane at building edges.
7. **Mitigation of Adverse Visual Impacts:** Provide planting to soften the visual impact of less desirable development and/or structures such as large blank walls and large areas of pavement.
8. **Screening:** Provide a physical or visual barrier to partially conceal or totally block unsightly elements such as parking areas, service areas, mechanical equipment, loading docks and outdoor storage areas.
9. **Visibility:** Maintain sight lines where appropriate such as at street intersections, driveway entrances and building entrances.

10. **Definition:** Use planting to highlight significant site features and to define site use areas and circulation corridors without interfering with the use of such areas.

(b) Design

The planting design should be a composition of plant materials that creates an appropriate visual character such as stylized, formal, informal or natural. The design should include a suitable combination of trees, shrubs, groundcover plants, vines, lawns and herbaceous material, *including native and northwest adapted plants*. The number, size and arrangement should be carefully selected to balance color, texture, form, line, proportion and scale in both the horizontal and vertical plane. Plants should be selected and arranged according to the following design criteria:

1. **Unity:** Arrange plants in a harmonious and orderly composition creating an overall unified and balanced design which is the sum of the parts, visually, and provides a sense of order, balance and harmony.
2. **Variety:** Select a diversity of plants providing interest, accent and contrast.
3. **Harmony:** Select plants and arrange them in a manner that is visually pleasing, compatible and sympathetic to the site and its environs.
4. **Order:** Provide planting that has a logical sequence and a rational arrangement.
5. **Consistency:** Develop a planting design compatible with and conforming to the overall project design concept.
6. **Appropriateness:** Develop a planting design concept that is suitable, proper, and fitting with the architectural character, land use and environment. Select plants with an awareness of their growth requirements, tolerances, ultimate size, preferences for soil and climate and negative impacts. (See also Section C - Horticulture).
7. **Density:** Provide adequate plant quantity, size and spacing to fulfill the functional and design objectives within the stipulated time.

(c) Horticulture

Plant material should be selected and installed according to the best industry standards to assure optimum growing conditions. A superior environment for plant growth (including existing vegetation, *native and northwest adapted plants*) should be provided to achieve the original design concept. Selection and installation considerations should include but not be limited to such items as soil conditions, exposure, water conditions, temperature, resistance to pests and diseases, ultimate growth habit and ease of future maintenance.

(d) Maintenance

Regularly scheduled maintenance according to standard horticultural practice should be considered to insure that the planting will achieve the intent, purpose and function of the original design concept within the stipulated time.

(e) Irrigation

Water appropriate to the horticultural requirements of different plant species and lawn areas should be provided. Irrigation systems should be designed to industry standards and provide complete coverage with the least amount of wasted water.

30.020(45) Signs and Street Graphics

Signs:

(a) General

1. Signs should be used primarily for the purpose of identification, conveying recognition of a particular enterprise, group of enterprises, or franchise.
2. A sign should complement or enhance the visual character of the land use zone in which it is located.
3. The scale of a sign should be in proportion with the building or site to which it pertains.
4. The shape of a sign should strive for simplicity with all elements constituting an integrated design.
5. A sign must be legible.

6. Signs should be harmonious with building design and surrounding structures, and be appropriate to the type of activity to which they pertain. Design elements, such as the size, shape, materials, lighting, color, lettering style, and the number and arrangement of signs should present a professional ~~and quality~~ appearance *and quality of permanence*.
7. The number of signs should be minimized in order to avoid visual clutter.

(b) Sign Message

1. The sign message if oriented towards automobile traffic should reflect the speed of traffic and the distance at which the sign is seen. Presentation of more than 15 items of information if predominantly for advertising purposes is discouraged.
2. Use of easily recognized symbols is encouraged.
3. The use of third party advertising signs is discouraged unless the product or service advertised reflects a franchise or dealership identity.
4. Signs may be used to advertise products or services except in areas zoned for residential, industrial and business park uses.
5. The advertising content of signs should be simplified so as to not detract from the identification purpose and the legibility of signs.
6. Signs should provide for good aesthetic presentation of the sign message through careful consideration of color combinations; illumination; sign placement; letter height, proportion and spacing, and by avoiding use of small and/or excessive lettering.

(c) Color

1. Colors should be used harmoniously and with restraint. Bright and brilliant colors should be avoided except for use as accent color. No specific color or combination of colors are specifically prohibited.
2. Sign colors should promote visibility without being garish, and consideration should be

given to the contrast between sign letters and their background.

3. The effectiveness of a sign should not be compromised by use of too many colors.
4. Sign colors of national or regional advertising programs should not be altered.

(d) Illumination

1. Surface brightness or intensity of lighting should not be beyond that necessary for visibility and proper color rendition.
2. Illumination should be harmonious with sign design. ~~If external flood lighting is used, it~~ *Illumination should be energy efficient and should be arranged so the light source is shielded from view.*
3. Signs should not produce a harsh uncomfortably bright light either through the level of illumination or a combination of illumination levels and design of the sign face.
4. Signs shall not be overly bright so as to overshadow signs in the immediate vicinity, cause glare, or create an island of light.

(e) Freestanding Signs

1. Freestanding signs shall be of a style, material and design compatible with the associated building.
2. Berming should not be utilized to exceed the maximum allowable height of signs.
3. The base or support elements of freestanding signs should be integrated with the surrounding environment, and landscaping may be required to buffer such signs.
4. Freestanding signs should be sited so as to integrate with the location of street trees and other site landscaping, and to avoid obscuring the view of adjacent freestanding signs.

(f) Wall Signs

1. The size and location of wall signs shall be reviewed in terms of their relationship to the building entry, height of sign fascia or size of wall where the sign is to be installed and the

relationship to other signs on a building, as well as visibility from the street, sidewalk or parking lot.

2. On multi-tenant buildings wall signs should be evaluated for compatibility with the building fascia and neighboring signs in terms of size, color, lighting materials, sign style and quality.
3. The depth of wall signs on multi-tenant buildings should be consistent.

(g) Sign Programs

1. Sign programs should be adhered to unless special circumstances can be substantiated for an individual tenant to deviate from the standard requirements.

20C.30.030 LAND USE DESIGN CRITERIA

30.030(05) Open Space

(a) General

1. Outstanding natural site features should be selected upon initial site design, and the open space designed to include and enhance these features.
2. Conservation open space is of greatest importance for locating open space within a site with amenity and recreation open space following in degree of importance.
3. Open space should be located to buffer proposed development from neighboring developments especially where there is a significant contrast in type or intensity of land use.
4. Open space should be located to buffer proposed development from environmental nuisances, such as busy streets.
5. Open space shall be integrated with housing location and circulation system design.
6. Open space should be organized into a general system of aggregated spaces rather than constituting a series of isolated leftover areas.
7. Environmental conditions, such as solar access, view and privacy should determine

the siting of open space and buildings, with roads to be located after open space and buildings are sited.

8. Storm drainage systems should be designed to complement existing land forms rather than appear as rigid engineered shapes.
9. Views ~~from~~ *through* a development should be preserved, opened up or designed to become part of the *surrounding* open space focus.
10. View corridors shall be provided to preserve visual access ~~of the public~~ to open space amenities *by the public*.

(b) Conservation Open Space

1. Sites with unique resource areas such as steep slopes, ravines, watercourses and wetlands, should save the natural area for common use and enjoyment of the light, air, amenity and views, and cluster units advantageously around it.
2. Conservation open space should be preserved as nearly practical in its native state, with minor disruptions permitted to provide for public access.

(c) Amenity Open Space

1. Amenity open space shall be provided to complement conservation open space, or where a site has no conservation open space, amenity space shall be provided as an element of focus within a site.
2. Amenity open space shall not constitute a series of thin meaningless landscape strips, but rather should be aggregated to provide a focus for building clusters.

(d) Recreation Open Space

1. Urban residence and multiple residence developments shall provide recreation open space commensurate with the size of development.
2. Recreation open space for active resident use shall be enhanced through provision of level areas for specific outdoor activities and by tying open spaces together through a linear

network of pedestrian and bicycle pathways, essentially free of auto intrusion.

3. Open space should be designed to provide readily accessible places for informal recreation to occur.
4. Linear features of the landscape such as ravines, stream and river valleys, bluff lines and utility rights-of-way should be considered for the location of trails to connect major open space areas.

(e) Designated Open Space

Where a proposed development abuts or includes designated areas as open space by Section 20B.50.060, "Parks, Recreation, and Open Space Plan", required open space shall be provided by using the following criteria:

1. Locate required open space next to designated open space.
2. Locate the required open space so that it buffers the proposed development from neighboring developments.
3. Place the required open space in such a way as to implement and enhance the walkway and trail plans and regulations.
4. Locate all required open space to provide access to public recreation areas.
5. Locate open space to increase access to waterfront access and recreation areas.

(f) Open Space Abutting Park or Recreation Facility

Where a proposed development abuts a park or recreation facility, it shall provide a link to the park or recreation facility such as a sidewalk, pathway, greenway, or break in the continuity of buildings. The development shall not block access to the park or recreation facility from adjacent areas.

30.030(10) Residential Design

(a) Site and Building Design

1. Provide an appearance of openness by clustering, separation of building groups, use of well-designed open space as provided for in 20C.20.130, and landscaping.

2. Create livability through the provision of recreation facilities, attractive common areas, and accessibility to buildings from parking areas and public walkways.
3. Assure internal privacy between dwelling units, and external privacy for adjacent dwelling units. Create a sense of privacy and separation from adjacent units through careful location of building entrances, windows, fences, walls and landscaping.
4. Orient buildings to enhance views from within the site by taking advantage of topography, building location and style.
5. Promote variety and irregularity in site and building design. Buildings in groups should be related by common materials and roof shapes, but contrast should be provided throughout a site by the use of varied materials, architectural detailing, building orientation and house type.
6. Design the perimeter of a project to enhance adjacent uses and not create nuisances such as "walled corridor" of buildings, heavily travelled streets, light and glare. Perimeter buildings should be similar in scale and bulk to buildings on adjacent sites.
7. Provide a system of off-street walkways which tie residential areas to recreational areas, transit, public walkways, and schools.
8. Parking areas shall be alleviated by landscaping and not designed in long, monotonous rows. The size of parking areas should be minimized and each area related to the group of buildings served.
9. Plan each phase of the proposed development as it is to be completed, containing the required parking spaces, recreation, landscape and utility areas necessary for creating an attractive and livable environment within that phase.
10. Consider solar orientation and climate in siting buildings.
11. Encourage a variety of compatible housing types, including attached dwellings where permitted by zoning. Design attached structures to minimize bulk and the

appearance of density, and create a scale that is harmonious with single-family units.

12. *Provide ample articulated window treatments in facades visible from streets and public spaces for architectural interest and human scale. Windows should be articulated with mullions, recesses, etc., as well as applying complimentary articulation around door ways and balconies.* *
13. *Building facades visible from public streets and public spaces, shall provide a minimum of 40% facade modulation. The minimum depth of modulation shall be one (1) foot and the minimum width shall be five (5) feet.* *

30.030(15) Street Design

(a) Street Design

1. Provide well-designed streets which consider terrain, intersection configurations, and connections to streets or adjacent sites or streets.
2. Minimize steep gradients, and cuts and fills in slope areas.
3. Promote safety through adequate sight distance, separation of vehicles from pedestrians, limited driveways on busy streets, and avoidance of difficult turning patterns.
4. Allow safe, efficient access for emergency vehicles.
5. Discourage through traffic and long cul-de-sacs, while assuring adequate circulation between neighborhoods.
6. Minimize disturbance of water courses by avoiding filling wherever possible.
7. Locate access street where joint use with other properties may occur provided impacts can be minimized.

(b) Street Capacity

1. Provide streets adequate to carry traffic planned for functional classification (residential feeder, arterial, etc.) but avoid overbuilding of street capacity where not

required in order to reduce impervious surfaces.

2. Provide streets adequate to carry traffic generated by the development and to avoid adverse impacts on surrounding neighborhoods.

20C.30.040 ZONING DISTRICT DESIGN CRITERIA

Reserved.

20C.30.050 CITY CENTER DESIGN DISTRICT DESIGN CRITERIA

20C.30.050(05) Purpose/Scope - The purpose of this section is to create guidelines for the development of the City Center Design District that is attractive in appearance, functionally integrated and promotes pedestrian and recreational activities.

Each Design Area is related to the other through circulation, building scale and open space relationships. The design criteria for the Design Areas are intended to identify general objectives and specific guidelines which will implement the objectives.

20C.30.050(10) Valley View, Bear Creek and Trestle Convenience Commercial Design Areas

(a) Design Objective

1. Create a lower-scale suburban commercial area oriented to safe automobile access.
2. Provide visibility and access from automobiles while maintaining adequate landscaping and screening.
3. Develop features such as signs and artwork that identify the entrance to the City Center and provide a unifying visual theme for the area.

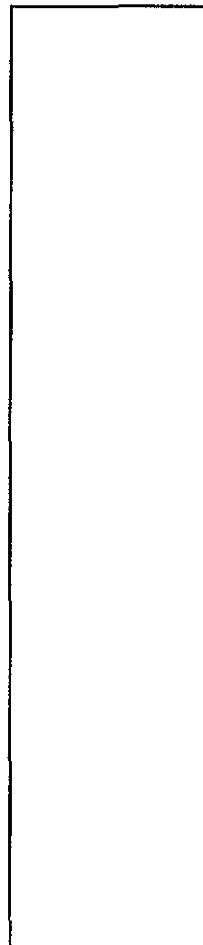
(b) Building Orientation and Access

1. Buildings are not required to be sited on the ~~linkage~~ pedestrian system, but should provide safe and accessible pedestrian connections to it.
2. Service areas and rear entrances should not be oriented to public areas.

3. Orient buildings toward the streets and locate parking on the side or rear whenever possible.

(c) Building and Site Design

1. Develop sign programs to coordinate and consolidate signs in order to increase visibility from automobiles.
2. Minimize elements in signs such as too brilliant colors or other features that would distract from the entry feature.
3. Landscaping on streets should be simplified to allow adequate visibility from automobiles to businesses.
4. Parking lots should be landscaped to screen parking and provide visual relief from large asphalt surfaces.



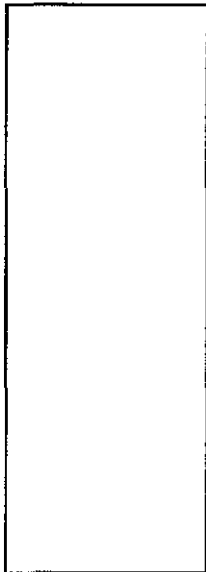
5. Pedestrian access from parking areas should be safe and clearly defined.

6. Landscaped medians should be provided where access and traffic movements allow.
7. Conflict between pedestrians and automobiles should be avoided by minimizing vehicle crossings of pedestrian access ways.

20C.30.050(15) Office Sammamish Trail and Town Square Design Areas

(a) Design Objectives

1. These ~~Office Design Areas~~ *is are* intended to provide areas of greater structural density adjacent to the retail core to promote high levels of economic and pedestrian activity. These areas will provide transition from the auto-oriented convenience areas to the pedestrian core.
2. Density and building mass are increased in the ~~Urban Office Town Square~~ Design Area nearest the ~~Center~~ of the City Center District to encourage development of office-type structures of larger scale and size. Structured parking is ~~required~~ *encouraged* as heights increase over four stories to reduce the impact of on-site parking and allow greater open space at the ground level. Retail uses are also ~~required~~ *encouraged* at ground floor levels to *further* encourage pedestrian activity.



3. Heights and density are moderated in peripheral areas of the district to promote a more suburban, ~~campus-like~~ setting adjacent

to residential and convenience shopping areas.

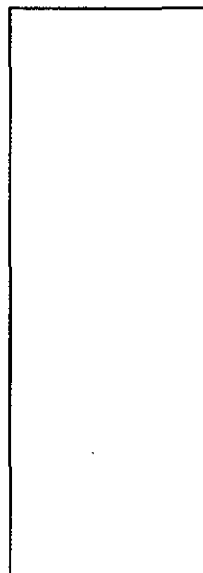
4. ~~Leary Way~~ ~~Leary Way~~ between the Sammamish River and the Burlington Northern railroad tracks should remain as a "green gateway" to the City of Redmond. Setbacks from Leary Way should be adequate to preserve enough trees on the west side of the road to preserve the "green gateway" image.

~~The setback from the property line on this portion of Leary Way shall be at least 50 feet. No parking shall be allowed within the setback. No clearing, grading permit or site plan approval shall be granted until the approval authority determines that the existing trees will be preserved to the maximum extent possible within the setback.~~

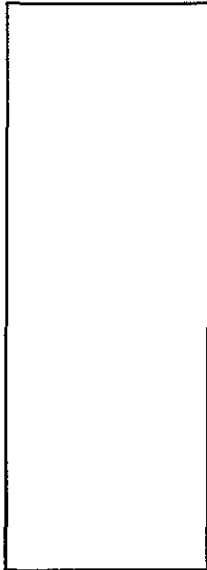
~~Signage for traffic control, directional information, and business identification should be kept at a minimum in both size and number.~~

(b) Building Orientation and Access

1. Buildings should ~~about~~ ^{have} the linkage pedestrian system on at least one side. Buildings ~~fronting on 164th Avenue N.E. or Redmond Way~~ *Type V Pedestrian System segments per the Section 20C.10.230(25) should* ~~shall~~ *about* the street front sidewalk and orient the primary entrance, or entrances, toward the street. ~~orient to the linkage system on these streets.~~



2. Vehicle access should be designed to minimize interaction of vehicles with pedestrians particularly in relation to the ~~linkage~~ pedestrian system.
3. Parking structures should not front on the *internal block connections of the pedestrian linkage system.*
4. Driveway openings to arterials should be minimized.
5. *When ground floor commercial spaces abut mid block pedestrian connections, entrances to the commercial spaces should be oriented toward the mid block connections as well as toward the streets. The area along side the required mid block sidewalk should be developed as a usable plaza with a combination of hardscapes, planters and seating areas.*



(c) Building and Site Design

1. Buildings should be sited in a manner compatible with adjacent buildings and the streetscape. Consideration should be given to size and height relationships with adjacent buildings.
2. Open space and landscaping should be coordinated and linked wherever possible, particularly in relation to public areas and the ~~linkage~~ pedestrian system.

3. Landscaping should be designed with consideration of shade and sun; plazas and outdoor areas should be oriented to sunlight.
4. Outdoor and ground floor areas should be designed to encourage outdoor activities such as vendors, art displays, seating areas, outdoor cafes, abutting retail activities and other features of interest to pedestrians.
5. Taller buildings should have adequate separation to maintain a sense of openness, adequate light, and views.
6. Buildings should be constructed of materials that minimize light reflection and glare.
7. Facades should be divided into increments through the use of architectural features such as bay windows, offsets, recesses and other devices which break or minimize scale.
8. Rooftops should incorporate features which soften rectilinear forms and effectively screen mechanical equipment from view.
9. Buildings should be designed to provide for weather and wind protection at the ground level. *Buildings fronting on Type V Pedestrian System segments should provide pedestrian weather protection by way of awnings, overhangs, a minimum of 48 inches in depth. The elements should be complimentary to the building's design and design of contiguous weather protection elements on adjoining buildings. Materials and design should engender qualities of permanence and appeal.*
10. Allow for ~~Encourage~~ varieties of shapes, angles and reliefs in the upper stories of structures over four stories. *

~~to the commercial spaces should be oriented toward the mid block connections as well as toward the streets. The area along side the required mid block sidewalk should be developed as a usable plaza with a combination of hardscapes, planters and seating areas.~~

(c) Building and Site Design

1. ~~Building design should be harmonious with adjacent Design Areas.~~
2. ~~Landscaping should not overpower pedestrians or block views of crossings and intersections. Linkage system planting and design should provide landscaping bulbs or islands to decrease length of crossings and encourage pedestrian movement across streets.~~
3. ~~Column and bay spacing on street facades should be provided approximately 16 to 20 feet apart to compliment the existing rhythm in adjacent Old Town and provide visual interest along the street.~~
3. ~~Outdoor space should be designed to provide amenities for pedestrians such as seating and generally integrate business activities with pedestrian activity on the street.~~
4. ~~Windows shall be provided on the street level rather than blank walls to encourage a visual and economic link between the business and passing pedestrians. A minimum of 60% of ground floor facades facing streets shall be in non-reflective, transparent glazing. Where windows can not be provided, art work in window boxes may be used with Site Plan Review approval.~~
5. ~~Facades should be divided into increments through the use of architectural features such as bay windows, offsets, recesses and other devices which break or minimize scale.~~
6. ~~Roof tops should incorporate features which soften rectilinear forms and effectively screen mechanical equipment from view.~~
7. ~~Buildings should be designed to provide for weather and wind protection at the ground level. Buildings fronting on Type V Pedestrian System segments should provide pedestrian weather protection by way of~~

~~awnings, overhangs, friezes, a minimum of 48 inches in depth. The elements should be complimentary to the building's design and design of contiguous weather protection elements on adjoining buildings. Material and design should engender qualities of permanence and appeal.~~

(d) Physical Features

1. Pedestrian/Customer Elements

- a. ~~Awnings—sunshades—canopies~~
~~Awnings or sunshades should be harmonious to the design of the building to which attached, and should be in keeping with the pedestrian character of the area. Materials should be durable, long lasting, and require low maintenance. Back lit awnings are discouraged.~~

b. Trees, plants and flowers

~~The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.~~

c. Street lighting

~~Street lighting should relate in scale to the pedestrian characters of the area. The design of the light standards and luminaires should enhance an historic theme or design area character.~~

d. Street Furniture

~~Public seating, trash receptacles and informational/directional kiosks should be of uniform design and be provided throughout Old Town.~~

e. Sidewalks

~~Paving of sidewalks and other pedestrian rights-of-way should be safe and constructed of a uniform material that is compatible with the pedestrian character. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.~~

2. Merchant Related Elements

a. Store fronts

1. Store fronts should be visually open wherever practical. Stores should use enough glass so that the activity inside the store is obvious to the passerby. In all cases merchandise should be easily visible to pedestrians.
2. Store fronts should not depart from the character of the building facade of which they are a part.
3. Store fronts should be brick, wood or stone where glass is not used. Care should be taken to avoid clashing colors on individual buildings and between adjacent buildings.

b. Signs

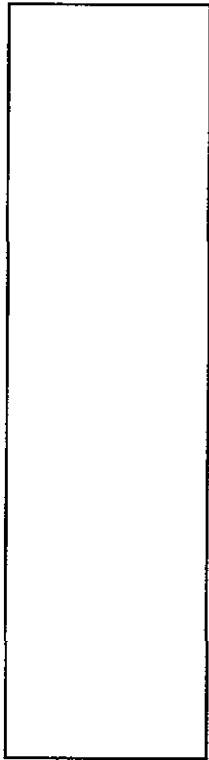
1. Signs will be evaluated according to the overall impact, size, shape, texture, lettering style, method of attachment, color, and lighting in relation to use, the building and street where the sign will be located, and the relation of the sign to other signs and other buildings in the vicinity. Adherence to or enhancement of sight lines both parallel and perpendicular to the sidewalks will be considered. The primary reference will be to the average pedestrian's eye level view, although views into or down the street from adjacent buildings should be an integral feature of any review.
2. Signs should be simple, clear and direct. Generally, single faced, flat surfaced, painted signs are preferred. Extruded aluminum or plastic signs may not be appropriate. Careful use of neon may be acceptable.
3. Signs should not hide or obscure the architectural elements of the building.
4. Exterior signs should be flat against the building, painted on it, or hung from the underside of the marquee, perpendicular to the sidewalk. Signs attached to the edge of the marquee should not extend above the marquee's upper edge.

5. Projecting, double faced or three-dimensional signs may be allowed if they are integrated into the overall streetscape and do not destroy sight lines. Symbolic three-dimensional signs such as a shaving mug, barber pole, pawn shop symbol, or other symbols illustrating the product being sold on the premises, are acceptable provided they meet other sign guidelines. When possible, signs should reflect the character and the use within the structure.
6. Sign lighting should be energy efficient.

30.050(25) Pedestrian Design Area (Old Town) Old Town

(a) Design Concept

1. "Historic Character" in the Old Town Old Town section of the Pedestrian Design Area refers to the wide variety of architectural styles which are representative of Redmond's history from the late 19th century through early 20th century, and which incorporate building materials and architectural treatments typically used during that period. Within the design concept of "historic character" one specific period or style of architecture is not dominant or thematic.
2. The design concept of "historic character" also encompasses the primary orientation of the built environment to the human or pedestrian scale. Historically, this was accomplished by the use of moderate building heights (2 - 3 stories), minimal setbacks from the pedestrian right-of-way and ground floor accessibility.



(b) General Criteria

1. Repair, renovation and new construction should retain the character of ~~Old Town~~ Old Town. This may be achieved by assuring that development meets the general requirements, and fits the special characteristics of ~~Old Town~~ *this design area*.
2. New buildings must relate in material, scale and form. Contemporary designs which are harmonious with the surrounding environment in ~~Old Town~~ Old Town in terms of scale, materials, and color may be acceptable.
3. Buildings and facades should be brick, stone or wood. In all design there should be emphasis upon the quality of detail and special form in: window treatments, columns, eaves, cornices, lighting, signing, etc. Building facades should have a greater proportion of voids (windows) than solids (blank walls) on pedestrian levels.
4. Buildings and the spaces between should relate easily and openly to the external public areas.

5. The scale of all structures in relationship to other structures and spaces is important. The scale should be two to three stories. Some variation in heights contributes to the variety and complexity of the environmental experience, and is encouraged.
6. The development of ground level view points as well as public balcony and roof spaces which take advantage of solar access and views is encouraged.

(c) Physical Features

1. Permanent Building Elements

- a. Buildings should be a backdrop to the ~~Old Town~~ Old Town activities. New or physical change will be evaluated for the effect on ~~Old Town~~ *the design area*. The integrity of a building as an individual structure or as part of a series of buildings should be respected.
- b. The main architectural elements should not be altered or disguised. Buildings as a whole should be seen as significant architectural elements.
- c. Exterior building surfaces above the first level and the primary architectural elements below the marquee such as columns, arches, rails are important elements to be considered in the building design and entry areas.
- d. The exteriors of significant historic buildings should be carefully restored and maintained. Other buildings should be related to the historic ones in terms of scale, proportion of openings, materials and color.

2. Pedestrian/Customer Elements

a. Awnings - sunshades - canopies

Awnings or sunshades should be harmonious to the design of the building to which attached, and should be in keeping with the historic character of the area. *Materials should be durable, long lasting, and require low maintenance. Back-lit awnings are discouraged.*

b. Trees, plants and flowers

The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.

c. Street lighting

Street lighting should relate in scale to the pedestrian characters of the area. The design of the light standards and luminaires should enhance an historic theme.

d. Street Furniture

Public seating, trash receptacles and informational/directional kiosks should be of uniform design and be provided throughout ~~Old Town~~ Old Town.

e. Sidewalks

Paving of sidewalks and other pedestrian rights-of-way should be safe and constructed of a uniform material that is compatible with the historic *pedestrian* character. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.

3. Merchant Related Elements

a. Store fronts

1. Store fronts should be visually open wherever practical. Stores should use enough glass so that the activity inside the store is obvious to the passerby. In all cases merchandise should be easily visible to pedestrians.
2. Store fronts should not depart from the character of the building facade of which they are a part.
3. Store fronts should be brick, wood or stone where glass is not used. Care should be taken to avoid clashing colors on individual buildings and between adjacent buildings.

b. Signs

1. Signs will be evaluated according to the overall impact, size, shape, texture, lettering style, method of

attachment, color, and lighting in relation to use, the building and street where the sign will be located, and the relation of the sign to other signs and other buildings in the vicinity.

Adherence to or enhancement of sight lines both parallel and perpendicular to the sidewalks will be considered.

The primary reference will be to the average pedestrian's eye-level view, although views into or down the street from adjacent buildings should be an integral feature of any review.

2. Signs should be simple, clear and direct. Generally, single-faced, flat-surfaced, painted signs are preferred. Extruded aluminum or plastic signs may not be appropriate. Careful use of neon may be acceptable. One of the many standard lettering styles should be used.
3. Signs should not hide or obscure the architectural elements of the building.
4. Exterior signs should be flat against the building, painted on it, or hung from the underside of the marquee, perpendicular to the sidewalk. Signs attached to the edge of the marquee should not extend above the marquee's upper edge.
5. Projecting, double-faced or three-dimensional signs may be allowed if they are integrated into the overall streetscape and do not destroy sight lines. Symbolic three-dimensional signs such as a shaving mug, barber pole, pawn shop symbol, or other symbols illustrating the product being sold on the premises, are acceptable provided they meet other sign guidelines. When possible, signs should reflect the character and the use within the structure.
6. Sign lighting should be subdued, ~~incandescent, and front-lit from the exterior rather than back-lit of the fluorescent type~~ energy efficient.

20C.30.050(30) Mixed Use/Shopping Center Design Area

(a) Urban Design

1. Gateway to Downtown - The Design Area classified Mixed Use/Shopping Center functions as a gateway to the City from SR 520. Development in this area should complement the other components of this gateway, Marymoor Park and Bear Creek, by providing attractive, interesting urban activity. Development should be consistent with the natural environment by minimizing glare, providing indirect lighting, avoiding intense signage, and providing a soft edge where the urban and natural environments meet.
2. Downtown Integration - Connection to existing roads including landscape treatment, road surface, sidewalk size and placement should respect and extend the existing grid system, townscape and character while meeting or if necessary exceeding current standards and regulations. Development in the Design Area classified Mixed Use/Shopping Center should consider existing conditions in the following subareas:
 - a. Leary Way - Leary Way between the Sammamish River and the Burlington Northern railroad tracks should remain as a "green gateway" to the City of Redmond. Setbacks from Leary Way should be adequate to preserve at least 75% of the existing stand of trees on the east.

Signage for traffic control, directional information, and business identification should be kept at a minimum in both size and number.

- b. Northern Boundary - Leary Way to 164th - Building siting should consider continuity of retailing frontage in order to integrate new development with ~~Old Town~~ Old Town retailing.
- c. Northern Boundary - 164th to 168th Ave. - This area should provide linkage capability between existing roadways north of Burlington Northern RR and new roadways south of same. New alignments should respect Gilman, 164th, 166th Avenue connections, either immediately or in the future. These new alignments should provide extension of the established visual corridors.

New connections to existing north/south roads in this area should be consistent with

the character of the existing older improvements.

- d. Bear Creek - The edge along Bear Creek should be kept as a natural area, with uses limited to passive activity and trail/pathway connections.

Signage in this area should be limited to traffic, safety and directional information, or be consistent with the public recreational use of the area.

Structures consistent with and supporting passive use of this area may be allowed, but should be kept to a minimum.

- e. Sammamish River - The edge along the Sammamish River should serve as an extension of existing activity on the Sammamish River Trail just north of this Design Area. Uses should include trail and pathway activities.

Signage should be limited to traffic, safety and directional information or be consistent with the public recreational use of the area.

Structures consistent with and supporting trail/pathway activities may be allowed, but should be kept to a minimum.

(b) Architectural Guidelines

1. Siting of Buildings - Buildings should be sited to enclose either a common space or provide enclosure to the street. Consideration should be given to the relationship between buildings and adjacent open space areas. All design should appear as an integrated part of an over-all site plan.
2. Building Form - The roofline of buildings should be modulated to avoid large areas of flat roof. As an alternative, flat roofs should be designed with interesting architectural features (such as textured materials or color) or landscaped. Consideration should be given to the appearance from nearby hillsides.

Encourage varieties of shapes, angles, and reliefs in the upper stories of structures over four stories.

The ground floor of buildings should provide pedestrian interest and activity. The use of

arcades, colonnades, or awnings to provide pedestrian protection is encouraged.

Large buildings should avoid continuous, flat facades.

Avoid the use of false fronts and large, blank walls.

3. **Building Entry** - Orient building entrances to the street in a manner which provides easily identifiable and accessible pedestrian entry ways. Highlight building entrances through landscape design. Building entries should be designed in conjunction with the landscape treatment of pedestrian ways in the parking areas.
4. **Public Art** - Encourage public art in public areas of the design area particularly in and around the regional shopping center and Town Square.

(c) Transportation Guidelines

1. **VEHICULAR**

- a. **Roadway Configurations** - If not at existing elevation, roadways should be bermed or screened in a manner to reduce visual impact of pavement area.

Alignment of all roadways shall minimize the removal of existing trees.

- b. **Parking - Surface** - Where possible, locate parking behind buildings and away from areas of high public visibility. Provide landscape screening where parking in areas visible to the public cannot be avoided.

Parking areas should include landscape areas and be designed to minimize long, straight, monotonous rows. The size of parking areas should be minimized and each area related to the group of buildings served.

Parking should be provided with a clear circulation network to guide pedestrians to the building entrances.

Visual impact of surface parking areas should be minimized from the SR 520 corridor.

- c. **Parking - Structured** - Structured parking should be designed to avoid long faceless planes. The scale of parking structures

should be modulated by interruptions of the facades, setbacks, and lowering the first level below the existing grade to reduce total height.

Facades of parking structures should include a landscape treatment in addition to architectural screening from the SR 520 corridor.

The top floor of parking structures should be provided with landscape screening, along the cornice and on the deck, either by trees or a screening trellis treatment. Provide walkways in parking floors that lead to elevators which have curbs or other barriers to protect from vehicular intrusion.

For security, pedestrian routes should be visible and avoid enclosed, hidden areas. Emergency call boxes should be available.

2. **PEDESTRIAN**

- a. **Linkages** - Link proposed development to walkway, trail, bicycle systems in the surrounding area by connecting and lining up directly to existing linkages, closing gaps and treating crossings of barriers (such as railroad, Bear Creek and driveways) with special design treatment, minimizing barriers, designing with consistent materials, widths and locations and providing safe, easy and clearly identifiable access to and along the linkages. Safe, convenient and attractive connections to Marymoor Park, Sammamish River Trail and the Bear Creek trail system should be provided.

- b. **Sidewalks** - When extending an existing sidewalk, the new walkway should meet current standards and regulations while being of a material and dimension which is compatible with while improving on the existing character.

Alignment of new sidewalks should minimize the removal of existing trees.

- c. **Arcades, Colonnades, Canopies** - In areas of high pedestrian activity, devices for protection from weather are encouraged. Consistent treatment within a single area is also encouraged in order to provide a strong identity of space.
- d. **Enclosed Malls** - The design of enclosed malls should allow pedestrians through

access during hours of mall operation while still maintaining security.

- e. Trails - Pedestrian - Pedestrian/vehicular intersections should be avoided when possible. Special design treatment should be afforded the pedestrian trail crossings of the Burlington Northern railroad tracks.
- f. Trails - Bicycle - Facilities for parking and locking bicycles should be provided and be readily accessible from bicycle trails.
- g. Trails - Equestrian - Width of the trail should be adequate for two riders side-by-side in order to avoid earth compaction and vegetation deterioration. Equestrian trails should be separate from pedestrian and bicycle trails.

(d) Landscape Guidelines

- 1. Urban Landscape Treatment - The building entry, primary vehicular entries and building perimeters should be planted with ornamental vines, groundcovers, shrubs and/or trees selected for their screening, canopy, spatial enclosure and seasonal variation characteristics.
- 2. Moving Mature Trees - Encourage the relocation of mature trees that would otherwise be lost during construction.
- 3. Site Furnishings - Benches, kiosks, signs, bollards, waste receptacles, water fountains, lighting standards, perch walls, sidewalks, pathways, trail and special water features should be designed to be compatible elements of like materials and design.

(e) Open Space Guidelines

- 1. Northwest corner - Preserve at least 75% of the existing stand of trees in the northwest corner of the site.

Minimize new grading and overland storm water treatment in this area.

Install screening between this open space area and adjacent parking areas.

Encourage passive recreation including a walking trail, bicycle trail, seating and rest areas, pedestrian lighting, and site furnish-

ings. Connections should be provided to the White House, shopping center, Marymoor Park, Sammamish River trail system and other open space areas.

- 2. White House - Open space areas at the Justice William White House should encourage active and passive recreation and should connect to other open spaces, trails and the shopping mall.
- 3. Sammamish River - Open space should be retained along the Sammamish River and enhanced by:
 - a. Providing grade separation for trails at all appropriate and feasible locations.
 - b. Making connections to other open space zones.
- 4. Bear Creek - Open space along Bear Creek should be retained and enhanced by:
 - a. Encouraging passive recreation activities and discouraging active recreation.
 - b. Limiting storm water treatment swales and recharge areas so that they do not destroy the function of preserving the natural environment and providing passive recreation areas. Storm water treatment structures should not be located in this area. All swales and recharge areas should be integrated with the natural environment.
 - c. Protecting vegetation of the riparian habitat in this zone by establishing trails and facilities outside of the floodway and limiting access to the creek to designated access points.
 - d. Providing connections to Marymoor Park, the Sammamish River, shopping mall and other open spaces.
 - e. Facilities within this area should include a pedestrian pathway, bicycle path, equestrian trail, passive water access area; seating, and site furnishings.
- 5. Public ~~e~~Open sSpace - Open space should be retained, enhanced and made available for public use through the City in this design area.

(f) Sign Kiosks

1. Sign kiosks - Sign kiosks, which provide direction to groups of tenants are encouraged as a means for discouraging numerous individual tenant signs and providing clear directions. These kiosks shall be located in high traffic areas and be consistent with the design theme of the development.
2. Symbols and Colors - International symbols and colors should be used whenever applicable.

20C.30.050(35) Leary Design Area

(a) Design Objective

1. Leary Way - Leary Way between the Sammamish River and the Burlington Northern railroad tracks should remain as a "green gateway" to the City of Redmond. Setbacks from Leary Way should be adequate to preserve ~~enough~~ significant stands of trees on the west side of the road to preserve the "green gateway" image.

The setback from the property line on this portion of Leary Way shall be at least 100 feet where there is an existing grove of significant trees (six trees within a quarter acre area). No parking shall be allowed within the setback. No clearing, grading permit or site plan approval shall be granted until the approval authority determines that the existing trees will be preserved to the maximum extent possible within the setback.

Signage on Leary Way for traffic control, directional information, and business identification should be kept at a minimum, in both size and number.

(b) Building Orientation and Access

1. Vehicle access and parking should be designed to minimize curb cuts along Leary Way and to minimize the loss of existing trees within the design area.
2. Where significant trees exist between the street and building sites, driveways from the primary streets to on-site parking areas should be designed in a tree-lined parkway manner, complimenting, preserving, and taking advantage of ~~as many trees as possible~~ remaining stands of trees.

3. Building orientation should compliment any remaining stands of trees.

(c) Building and Site Design

1. Site layout for individual parcels within the design area ~~are encouraged, but not required, to be consolidated to achieve~~ should be designed to provide reciprocal vehicular and pedestrian access to and from adjoining lots in order to achieve a unified circulation plan which minimizes curb cuts on Leary Way, and provides pedestrian connections between uses in the design area.
2. New landscaping should be designed to compliment and reinforce any remaining groves of trees within the design area. Large trees and shrubs, both evergreen and deciduous, should be used to compliment the natural character of this design area.
3. Buildings and site layout should be designed to compliment, the village character of the adjoining Old Town design area, which includes features such as narrow store fronts on pedestrian oriented streets, shops fronting on street sidewalks, and pedestrian scale architecture.
4. Pedestrian connections from buildings within the design area should be provided to the adjoining Old Town and Mixed Use Shopping Center design areas.

30.050(40) River Bend Design Area

(a) Design Objectives

1. Provide visibility and access from automobiles while also providing adequate landscaping and screening.
2. Create and maintain a lower-scale commercial and service oriented design area.

(b) Building Orientation and Access

1. Orient buildings toward the streets and away from adjacent residential uses.
2. Service areas and rear entrances shall be oriented away from adjacent residential uses, public open spaces, and primary streets.

3. *Vehicular access should be minimized on the primary arterial, and shared access is encouraged.*

(c) Building and Site Design

1. *Parking lots should be landscaped to screen parking and provide visual relief from large asphalt surfaces.*
2. *Parking lot and security lighting shall be screened by buildings and other means to avoid light intrusion onto the adjacent river and residential uses.*
3. *Buildings shall be sited to ensure maximum land use compatibility between adjoining and adjacent uses, while minimizing impacts on each other.*

20C.30.050(45) North Point and Carter Design Areas

(a) Design Objective

1. *These residential design areas are intended to provide areas of greater residential density within the downtown in order to create a better balance between jobs and housing, and to provide a variety of housing choices within close proximity to goods and services.*
2. *Provide opportunities to develop moderate density housing in small residential neighborhoods that are in close proximity to schools, shopping, parks and services.*

(b) Building Orientation and Access

1. *Buildings should be oriented toward the primary streets.*
2. *Primary entrance to buildings shall be oriented to the street. See Section 20C.10.230(F).*
3. *Yards of ground oriented units fronting on streets should be designed to indicate a separation between public and private spaces, while also providing maximum territorial view of the street front from the units' private open spaces.*
4. *Vehicular access to sites from streets should be minimized. Alley access should be utilized where possible and feasible to minimize curb*

cuts along street fronts, whereby maintaining available street front parking.

(c) Building and Site Design

1. *Buildings shall provide focus and articulation of primary entrances orienting toward streets.*
2. *Buildings and private usable open spaces shall be appropriately setback from property lines to provide ample light, air, and privacy for the on-site tenants and adjacent neighbors.*
3. *Buildings should be designed to provide maximum amounts of usable open space on site for the residents and to provide front yards complimentary to the size and scale of the developments.*
4. *Parking shall be enclosed within subterranean or semi-subterranean parking when ever possible to provide maximum amounts of usable open space on parking decks and minimize building heights.*
5. *Building facades and roof lines shall be articulated to provide visual interest and relief as well as to promote long term neighborhood and community pride.*
6. *Open exterior corridors fronting and visible from streets and public spaces shall be minimized in use and length in order to minimize their negative visual impacts on surrounding streets and properties. They should be placed on the interior of developments when ever feasible.*
7. *On-site parking, both within structures and outdoor, shall be screened from public streets.*
8. *Common usable open spaces and yards shall be adequately lighted and visible from units within the developments to ensure security and safety of the residents and their guests.*

20C.30.050(50) Foothill and East Hill Design Areas

(a) Design Objective

1. *Create areas which allow a variety of uses side-by-side that maintain a residential look and feel in terms of design and character, in order to promote and maintain a comfortable*

and attractive living environment while also allowing non-residential uses next to residential uses.

2. *Provide opportunities to create mixed use residential, office/retail, and residential only developments within a neighborhood context and scale.*

(b) Building Orientation and Access

1. *Buildings should be oriented toward the primary streets.*
2. *Primary entrance to buildings shall be oriented to the street. See Section 20C.10.230(F).*
3. *Yards of ground oriented residential units fronting on streets should be designed to indicate a separation between public and private spaces, while also providing maximum territorial view of the street front from the units' private open spaces.*
4. *Vehicular access to sites from streets should be minimized. Alley access should be utilized where possible and feasible to minimize curb cuts along street fronts, whereby maintaining available street front parking.*

(c) Building and Site Design

1. *Buildings shall provide focus and articulation of primary entrances orienting toward streets.*
2. *Buildings and private usable open spaces shall be appropriately setback from property lines to provide ample light, air, and privacy for the on-site tenants and adjacent neighbors.*
3. *Residential buildings should be designed to provide maximum amounts of usable open space on site for the residents and to provide front yards complimentary to the size and scale of the developments.*
4. *Parking shall be enclosed within subterranean or semi-subterranean parking when ever possible to provide maximum amounts of usable open space on parking decks and minimize building heights.*
5. *Building facades and roof lines shall be articulated to provide visual interest and*

relief as well as to promote long term neighborhood and community pride.

6. *Office and mixed use buildings should be designed with a residential character, in terms of building materials, roof design, fenestration, and front yard landscaping.*
7. *Open exterior corridors fronting and visible from streets and public spaces shall be minimized in use and length in order to minimize their negative visual impacts on surrounding streets and properties. They should be placed on the interior of developments whenever feasible.*
8. *On-site parking, both within structures and outdoor, shall be screened from public streets.*
9. *Common usable open spaces and yards shall be adequately lighted and visible from private spaces within the developments to ensure security and safety of the tenants, residents, and their guests.*

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20F.10.060

LEGAL NONCONFORMITIES

10.060(05) Purpose and Intent - Recognizing that land, structures, and uses of land and structures which do not conform to the Development Guide can become nuisances, can disrupt the orderly development of the City, and can create unsafe, hazardous and unhealthful conditions, the City of Redmond declares that all nonconforming structures and land uses, whether created by adoption of the Development Guide or in existence under the prior zoning ordinance, shall be terminated as soon as it is reasonable, with justice to the owner(s) and operator(s) of the nonconformances and the general welfare of the City. The city recognizes that land, structures, and uses of land and structures which do not conform to the Development Guide can become nuisances, can disrupt the orderly development of the City, and can create unsafe, hazardous, and unhealthful conditions. The City also recognizes that the eventual elimination of existing nonconforming uses and structures benefits the health, safety and welfare of the community. It is the intent of this chapter to establish regulations and procedures which ensure that the elimination of nonconforming uses and structures occurs as expeditiously and as fairly as possible and with justice to property owner(s) and business operator(s).

10.060(06) Definitions For the purpose of this chapter, the following definitions apply:

- A. "Demolish" means to remove more than fifty percent of the exterior walls of an existing building or structure, as measured by the linear length of the walls. Windows, doors and/or deteriorated wall sections are all considered part of a wall.
- B. "Rebuild" means to undertake construction within and/or on an existing building which has a valid construction permit with construction value greater than fifty percent of the replacement cost of the existing building being rebuilt. The permit value is valid for a twelve-month period beginning on the date of permit issuance.

10.060(07) Continuance of nonconforming rights- Nonconformities, as defined in Section 20A.60 Definitions, may continue to be used and maintained in accordance with the provisions of this chapter. The use and maintenance is permitted as a result of vested rights obtained through the legal establishment of the nonconforming use or structure.

10.060(10) Uses-Abandonment of nonconforming rights- Abandonment or Termination - A nonconformance which has been abandoned, discontinued or terminated for a continuous period of twelve (12) months for any reason shall not be resumed. All nonconforming rights to a use are lost if the use is changed or if the use is abandoned for twelve months or if the structure housing the use is demolished or rebuilt, except as provided in Section 20F.10.060(20) Restoration, and 20F.10.060(25) Expansion.

10.060(12) Illegal uses or structures - Illegal uses or structures have no vested rights. Illegal uses and structures shall either be brought into legal conforming status or shall be removed.

10.060(14) Special uses and structure - Nothing in this chapter shall prohibit the establishment of special regulations for specific nonconforming uses and structures regulated by other sections of the Community Development Guide. Such regulations may provide for the retirement or amortization of those specific uses and structures.

10.060(15) Nonconforming Lots of Record - Lots of record that do not conform to the dimensional requirements of the development Guide may be used as otherwise permitted by the Development Guide if they were legally created and were in conformance with the prior zoning code or were a legal nonconformance under that code.

10.060(20) Restoration- Loss or Damage to Nonconformance - A nonconformance which has been destroyed, damaged or has incurred a loss equal to or greater than fifty percent (50%) of its assessed or appraised value, whichever is greater, shall thereafter conform to the regulations of the Development Guide. A nonconformance which has been damaged or incurred a loss less than fifty percent (50%) shall have no more than twelve (12) months in which to resume activity or rebuild or the non-conformance shall be considered to be terminated and shall not be resumed. - Any building containing a nonconforming use or any nonconforming structure may be repaired and restored to its nonconforming state if the need for repairs or restoration shall be the result of fire, explosion, earthquake, imminent public hazard, replacement of underground fuel tanks, vandalism or other accidental destruction. Such restoration shall comply with the following conditions:

- A. **Level of Restoration.** The damaged use or structure may be repaired to the area and

footprint of the previous use or building. In the case of total destruction or need for underground fuel tank replacement, a new structure may be established to the same area or footprint of the previous use or building. Alternatively, the structure may be built to a more conforming area or footprint.

- B. **Additional Floor Area Added.** Floor area may be added to the existing nonconforming use per Section 20F.10.060(25).
- C. **Time Limit.** The repairs must be commenced within one year of the event causing damage to the structure, and the repairs must be diligently pursued until completed.

10.060(23) Improvements - Any nonconforming structure or any structure containing a nonconforming use may be improved provided that nonconforming landscaping and Pedestrian System Improvements are brought into conformance per Section 20C.10.060(F) Landscaping.

10.060(25) Alteration or Expansion of a Nonconformance -

- A. **General.** The alteration or expansion of a nonconformance is prohibited unless it does not enlarge or increase the degree of nonconformity, does not enlarge the area of occupancy of the non-conformance, or unless it is specifically permitted through an official action as state in Chapter 20F.20.000, "Permits and Procedures", or as stated below.
 - i. **Nonconforming Structures.** A nonconforming structure may not be expanded or altered in any way so as to increase that nonconformity. A nonconforming structure shall be brought into full compliance with the Development Guide when alteration or expansion of the structure takes place and the following takes place within any three year period: The gross floor area of the structure is increased by 100% or more; or the costs stated on all approved building permit applications for the structure equal or exceed the assessed value of the structure at the beginning of that three year period.

- ii. **Landscaping.** A nonconforming landscaping or linkage pedestrian system area shall be brought into compliance with Section 20C.20.090 and 20C.10.230(+25) based on the following criteria:

when improvements to the building or parking areas, on any lot, do not exceed \$5,000, in any three year period, no improvements are required; for improvements to the building or parking area which exceed \$5,000, on any lot, in a three year period, the percentage (by value) of the required landscaping or linkage pedestrian system, to be installed shall be established by dividing the value of the proposed improvement by the assessed value of the existing lot improvements up to 100 percent; the Design Review Board or the Technical Committee shall have the authority to specify the location and phasing sequence of the landscaping or linkage pedestrian system improvements which fall under this section.

- iii. **Nonconforming Uses In the City Center.** The City recognizes that some existing, vital businesses of value to the community in the City Center may have been rendered non-conforming as a result of land use plan changes. The intent of this section is to encourage retention of existing businesses and to allow continued investment and upgrades to the business structures in the City Center. Nonconforming uses in the City Center may expand as described below.

- B. **Conditional Uses.** Any use which was originally established in a zone district by right and has since been reclassified as a use requiring a general or special development permit in that district shall obtain a general development permit or a special development permit prior to expansion of the use of any structure related to the use.

C. **Nonconforming Residential Uses.**

- i. **General.** A nonconforming residential use may not be expanded to create additional dwelling units.

- ii. **Single-family Dwellings.** A single-family dwelling on a single lot may be expanded up to four hundred fifty square feet of floor area.
- iii. **Two-family and Multi-family Dwellings.** Two or more dwellings may be expanded up to four hundred fifty square feet of floor area per unit, provided that parking for all units is brought into conformance with existing regulations.

D. Nonconforming Commercial, Institutional, and Park.

Nonconforming commercial, institutional, and park uses in the City Center Neighborhood may expand and, or, rebuild in conjunction with restoration per Section 20F.10.060(20) to occupy a greater area of building, but shall not be expanded beyond the original parcel(s) occupied at the time the use or structure became nonconforming. Parking for the expansion shall be provided in accordance with current parking standards. Nonconforming landscaping and Pedestrian System Improvements shall be brought into conformance with current standards per Section 20F.10.060(F) Landscaping.

E. Industrial Uses.

- i. **General.** Nonconforming industrial uses in the City Center Neighborhood not deemed unsafe, hazardous, unhealthful, or obnoxious due to noise, odor, air quality or visual blight may expand to occupy a greater area of building, but shall not be expanded beyond the original parcel(s) occupied at the time the use or structure became nonconforming. Parking for the expansion shall be provided in accordance with current parking standards. Nonconforming landscaping and Pedestrian System Improvements shall be brought into conformance with current standards per Section 20F.10.060(F) Landscaping.
- ii. **Outside Uses.** Outside nonconforming equipment and machinery may be relocated or altered, provided the

equipment or machinery is not relocated any closer to the nearest residential district or street.

- iii. **Volumes.** Increased sales, production or throughput volume shall not be considered as an expansion, provided the use does not expand to occupy additional land or another building.

~~(d) Parking See Section 20C.20.150.~~

~~10.060(30) Parking-Moving a Nonconformance-~~
~~If a nonconformance is moved, it shall conform to the land use district regulations in which it is then located. - The following shall apply to uses with nonconforming parking:~~

- A. **Abandonment of Nonconforming Rights.** All nonconforming rights related to parking shall be lost if the primary structure on the lot is demolished or rebuilt. Rights shall not be lost if a building is merely vacated.

B. Expansion of Conforming Residential Use with Nonconforming Parking.

- i. **No New Units.** A conforming residential use with nonconforming parking may be expanded by up to four hundred fifty square feet, provided no new dwelling unit is created by the expansion. Any expansion beyond four hundred fifty square feet shall require the provision of one parking space for each four hundred fifty square foot addition not to exceed the total number of parking spaces required by current parking standards. However, for single-family residential uses outside of parking impacted areas, no additional parking is required on lots with driveways twenty feet or more in length.
- ii. **New Units Added.** A conforming residential use with nonconforming parking may be expanded to create additional conforming units only if the parking for all units, old and new, is brought into conformance with current parking standards.

- C. **Expansion of Nonresidential Use with Nonconforming Parking.** A conforming nonresidential use with nonconforming parking may be expanded or intensified,

provided that parking is provided for the expansion or intensification in accordance with current parking standards.

10.060(35) Change In Use Change of Nonconformance ~~A nonconformance shall not be changed to another nonconformance.~~

A. To a Conforming Use. A nonconforming use may be changed to a conforming use.

B. To Another Nonconforming Use. A nonconforming use in the City Center Neighborhood may be changed to another nonconforming use if a general development permit is granted as provided for in this title and provided:

- i. A special building inspection is conducted to ensure the building conforms or is repaired to conform to minimum building, plumbing, fire, housing, electrical and earthquake code provisions as necessary to protect public health and safety;*
- ii. The proposed change of use will provide a service or will be beneficial to the neighborhood, and will more closely conform to the zoning of the site than the existing use.*
- iii. The proposed use is compatible with the goals of the design area or zone in which it is located.*
- iv. The proposed use is compatible with the surrounding uses.*
- v. The proposed use will not pose or create greater negative impacts than uses which are permitted within the design area or zone in which it is located.*
- vi. The proposed use will not pose or create unsafe, unhealthful, or hazardous conditions upon surrounding properties.*

10.060(40) Maintenance Repairs, Maintenance and Safety of Nonconformance ~~Repair and maintenance work may be undertaken on a nonconformance to the extent that it does not exceed ten percent (10%) of the assessed value of the structure within a period of twelve (12) consecutive months. Nothing shall prevent the City from~~

~~requiring repairs on any non-conformance to protect the public health and safety. All nonconformances shall conform to all revisions, modifications and amendments to the construction, building and housing codes. - Ordinary maintenance and repair of a building containing a nonconforming use, such as painting, plumbing repair, shall be permitted as necessary to ensure the protection of general health, safety and welfare. All nonconforming uses and structures are subject total applicable property maintenance and substandard building laws.~~

10.060(45) Amortization Reserved -

10.060(50) Abatement of Public Nuisances - Regardless of any provisions in this section, any nonconformance found to be a public nuisance shall be terminated.

10.060(55) Prior Nonconformance - Any nonconformance which under the prior zoning ordinance was nonconforming and was required to terminate by a certain date shall continue to be subject to the amortization provisions of the prior zoning ordinance.

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11/19/93

REVISIONS RELATED TO CELEBRATION
DISPLAYS/BANNERS20C.20.230 SIGNS & STREET GRAPHICS

20.230(35) Permitted Temporary Signs - Temporary signs, exempt from paragraph (30) of this section unless otherwise provided, are permitted in any zoning district subject to the following requirements:

(a) Banners on Redmond Way Railroad Overpass - Sign banners may be displayed on the Burlington Northern railroad bridge Redmond Way overcrossing on a temporary basis for a period not to exceed fourteen (14) days providing the following criteria are met:

1. The banner shall not exceed 100 square feet in area;
2. The sign message must promote a governmental, charitable or civic activity sponsored by a governmental entity or private non-profit organization;
3. The sign banner shall be of a light weight, plastic, fabric or durable paper material and shall not be constructed of wood, metal or any other such heavier material which could present a hazard if the banner became detached from the overpass.
4. Permits for overpass signs shall be issued and administered by the Director of Public Works or the Director's designee. A fee in an amount sufficient to cover the cost of processing a permit application shall be charged for each permit.

(b) Construction Signs - Non-illuminated signs that identify the architects, engineers, planners, contractors or other individuals or firms involved with a construction project, or announce the character or purpose of the project may be displayed at the project site. There may be one sign on each street the project fronts upon, but no more than a total of two signs. No sign shall exceed 32 square feet in area and ten (10) feet in height. The signs shall be removed upon occupancy of the project.

(c) Celebration Displays - Unless otherwise provided, Temporary signs, banners, posters,

festoons and clusters of lights, flags, pennants and balloons and searchlights are permitted only for a period of seventeen (17) days and for a total of three (3) times per year to announce the opening of a new enterprise, celebrate business anniversaries or announce major sales. These displays must be used on the site of the enterprise and must be removed at the end of the time period. Banners must be attached securely to and flat against the building wall of the respective establishment. A permit is required to display a banner. Searchlights are permitted for the above-specified occasions only if they are used for a maximum period of seventeen (17) consecutive days and no more than three (3) times a year and the beam of the searchlight does not flash against any building or does not sweep an arch lower than 45 degrees from vertical. Large celebration displays are not allowed to project above the building in which they are associated and must be outside all site distance triangles noted in Section 20C.20.220(20).

(d) Banner Display in City Center Neighborhood - In the City Center Neighborhood only, banners may be displayed for a period of thirty (30) days and for a total of six (6) times per year to announce the opening of a new enterprise, celebrate business anniversaries or announce major sales. Each business or organization may display only one banner per period. No permit is required to display a banner in the City Center Neighborhood. The rights and privileges granted under this paragraph shall automatically terminate three years from the date of its adoption, unless otherwise provided for by the City Council.

(e) Major Land Use Action Notice - Where required by Section 20F.20.070(15)(c), public notice signs which describe proposed major land use actions and public hearing dates are permitted. The sign or signs shall comply with the standards contained in Appendix O.

(f) Political Signs - On-premises political signs located at the headquarters of a political party, candidate for public elective office, or a public issue decided by ballot are permitted. All on-premise political signs shall comply with the dimensional and locational requirements of the sign district in which located. Off-premise political signs that do not exceed four square feet in area are permitted from the closing date for filing for an election until fourteen (14) days after the general election, except that after a primary election, candidates not advancing to the general election shall remove their signs within the

time limit. Political signs shall not be placed in the public right-of-way. Permits for political signs are not required. A \$250 bond shall be posted with the City by each campaign headquarters for candidates and issues decided by ballot to ensure that signs are removed within the specified time limit after the election. Failure to remove signs will result in the forfeiture of the bond.

(fg) Real Estate Signs - Real estate signs and For Rent or Lease signs shall meet the following requirements:

Items Being <u>Advertised</u>	Max Size <u>In Sq. Ft.</u>	Max Ht <u>In Feet</u>	Max. No. <u>of Signs</u>
Single-family House or Lot	4	6	2
Commercial industrial or multiple-family lots	32	6	2
Commercial, industrial or multiple-family building or portion of building	16	6	2
Subdivisions (lots or houses)	32	6	2

Three off-premise portable open house or other directional real estate signs are permitted only during times specified by the Director of Public Works. The use of balloons, festoons, flags, pennants or any other attached display on a directional sign is prohibited.

(gh) Temporary Window Signs - Temporary window signs shall not be included in the sign area for each facade, provided that such signs do not exceed the smaller of a total of fifty (50) square feet or ten percent (10%) of the window area.

(hi) Signs on Kiosks - Temporary signs on kiosks are permitted but the signs shall not exceed four (4) square feet in area.

(ij) Temporary Uses - Temporary signs relating directly to allowed temporary uses under these regulations may be permitted for a period not to exceed the operation of the use. The signs need not

be processed through the Design Review Board and are subject to the following requirements:

- Signs must be portable in nature and placed on premise (no off-premise signs are permitted).
- No more than one sign per temporary use shall be permitted, except by virtue of having been in consistent operation prior to the existence of this ordinance, and due to the fact numerous individual operators participate in the operation, the open air crafts and farmer's market commonly known as the Saturday Market, shall be allowed two signs.
- No sign shall exceed 32 square feet total of all sign faces.
- Maximum sign height shall be eight (8) feet.
- Signs shall not be portable readerboard types, electrical or neon. Only indirect lighting is allowed.
- Signs shall be secured with an approved tie down.
- Signs shall be approved by the Planning Director with a temporary use application.

4/20/93

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20B.60 TRANSPORTATION

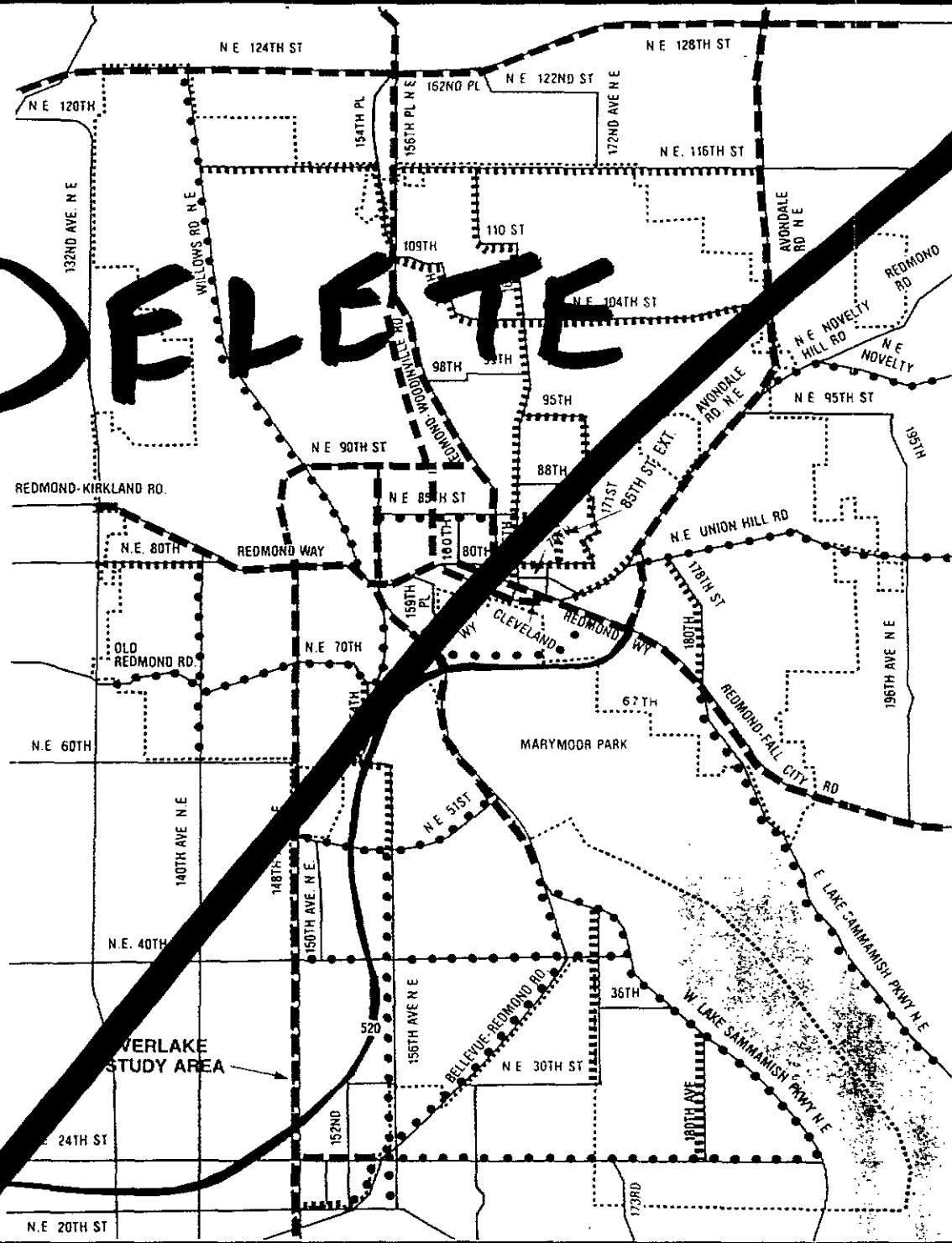
PROPOSED REVISIONS RELATING TO CITY CENTER NEIGHBORHOOD

The revisions to this section include revisions to tables and maps only.

It is proposed that:

1. The Functional Classification and Street Plan map (page 2 of 12) be replaced by the map which follows (page 3 of 12);
2. The City Center Arterial Street Plan map (page 4 of 12) be replaced by the map that follows (page 5 of 12);
3. The table titled Functional Arterial Classification Summary for Long Range Transportation Plan (page 6 of 12) be replaced by the table on the following two pages (7 and 8);
4. The Bike Way Plan map (page 9 of 12) be replaced with the map that follows it (page 10); and
5. The City Center Bike Plan map (page 11 of 12) be replaced by the map which follows it (page 12 of 12).

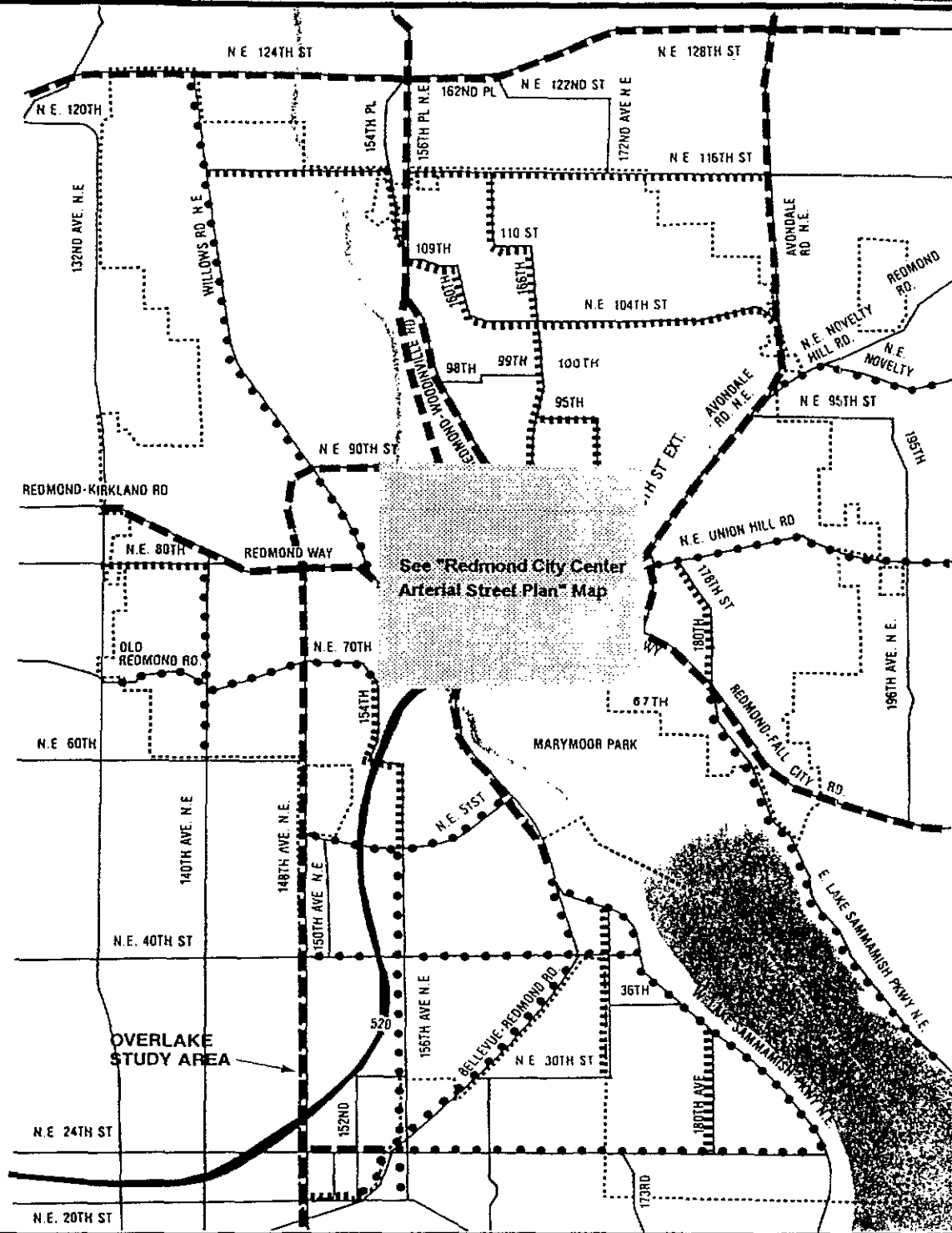
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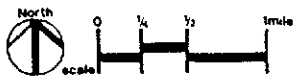
FUNCTIONAL CLASSIFICATION AND STREET PLAN



- FREEWAY
- - - PRINCIPAL
- • • MINOR ARTERIAL
- COLLECTOR

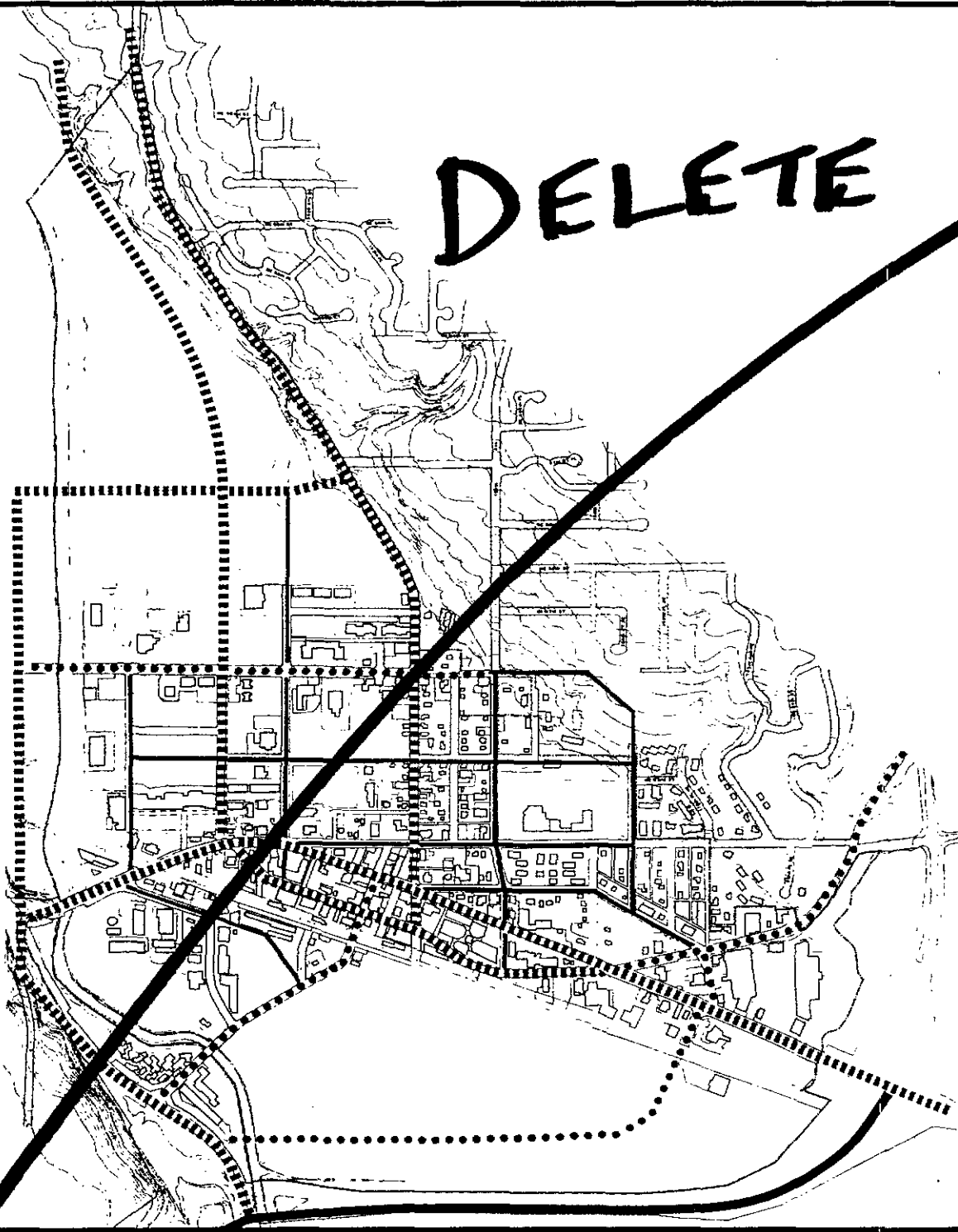


FUNCTIONAL CLASSIFICATION AND STREET PLAN



- FREEWAY
- - - PRINCIPAL
- • MINOR ARTERIAL
- COLLECTOR

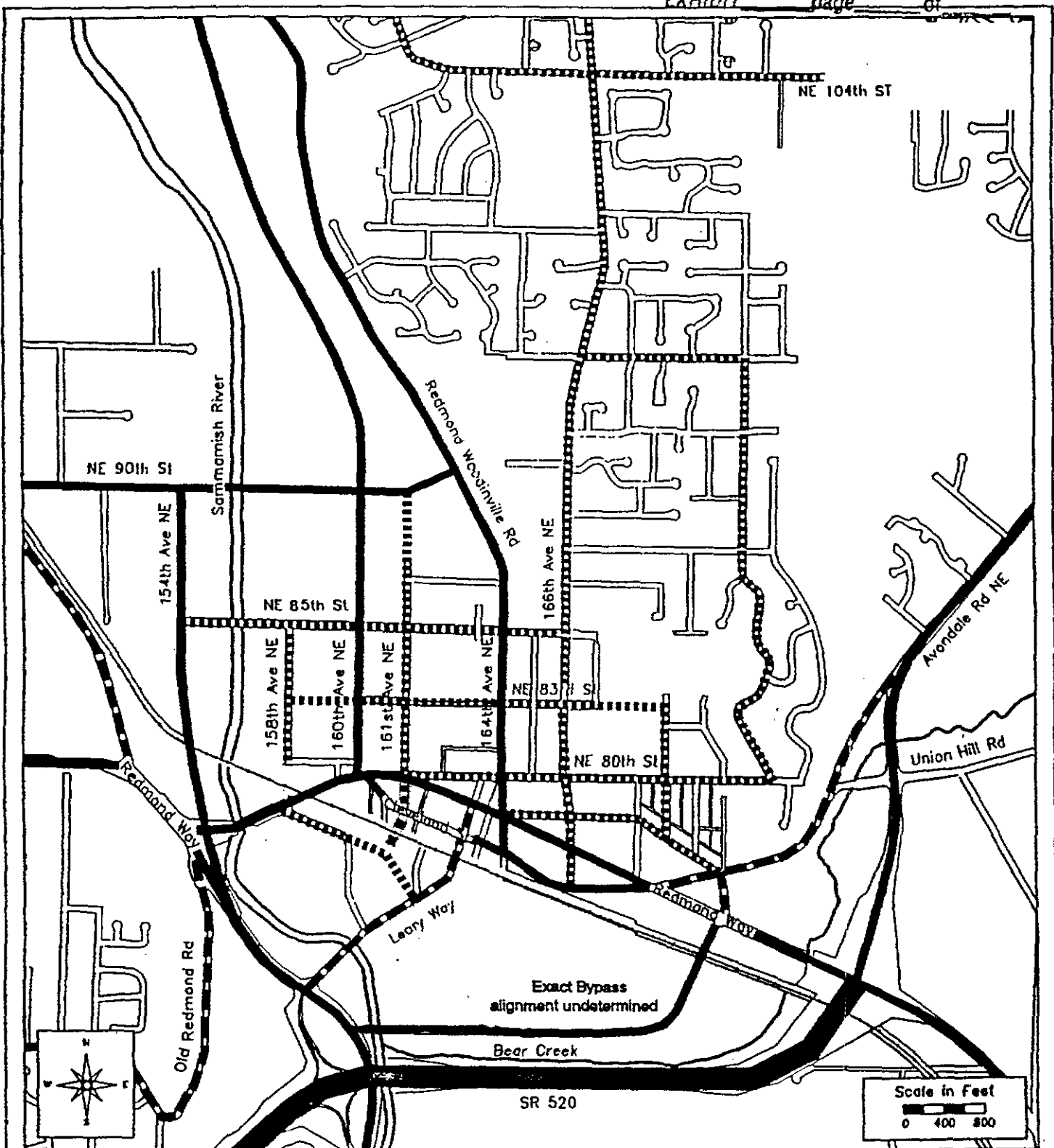
DELETE







CITY CENTER
ARTERIAL STREET PLAN



- FREEWAY
- - - - - PRINCIPAL ARTERIAL
- MINOR ARTERIAL
- COLLECTOR



Redmond City Center Arterial Street Plan

- Freeway 
Principal Arterial 
Minor Arterial 
Collector Arterial 

FUNCTIONAL ARTERIAL CLASSIFICATION SUMMARY
FOR LONG RANGE TRANSPORTATION PLAN

STREET	TRAFFIC LANES*		SIDEWALKS		BIKE LANES*	
	EXISTING	PLANNED	EXISTING	PLANNED	EXISTING	PLANNED
PRINCIPAL ARTERIALS:						
STATE ROUTE 520	2-4	4	0	0		2
REDMOND WAY (except couplet)	4/5	4/5	0-2	2	0	0
REDMOND WAY/CLEVELAND	3	2-3	2	2	0	0
STREET COUPLET						
148TH AVENUE N.E.	4	4	2	2	0	0
RED WOOD ROAD (164TH AVE. N.E.)	2-4	2	0-2	0-2	0	0-2
AVONDALE ROAD	2	4	0	2	1	2
148TH AVE. N.E. EXTENSION	0	4	0		0	0
SR 901 EXTENSION/WLSP EXTENSION	0	4/5	0	0	0	0
N.E. 24TH STREET (148TH TO 156TH)	4	4	2	2	0	0
W. LK. SAMMAMISH PKWY (BEL RED ROAD TO REDMOND WAY)	2	4	0	2	0-2	2
N.E. 124TH STREET	2	2-4	0	0-2	0	2
160TH AVE. N.E.	2	2-4	0	2	0	0
N.E. 90TH ST. (WILLOWS TO SR 202)	0	4-5	0	2	0	0
MINOR ARTERIALS:						
LEARY WAY	2	3	0-2	1-2	0	2
N.E. 85TH STREET	4		1-2	2	2	2
N.E. 24TH STREET (156TH TO W. LK. SAMMAMISH PKWY.)			2	2	1	2
W. LK. SAMMAMISH PKWY	2	2	0	0	1	1-2
156TH AVE. N.E. (N.E. 20TH to N.E. 51ST)		4	2	2	2	2
N.E. 51ST STREET	2	2-4	1-2	2	0	0-2
N.E. 40TH STREET		2-4	0-2	2	0-2	2
N.E. OLD REDMOND ROAD	2	2-4	0-2	2	0-2	1
140TH AVENUE N.E.	2	2	0	2	0-1	1
E. LK. SAMMAMISH PKWY	2	4	0	0	0	2
N.E. UNION HILL ROAD	2-4	2-4	0-2	0-2	0	0-2
NOVELTY HILL ROAD	2	2	0	0	0	1-2
BEL-RED ROAD	2-4	4	0-2	2	0	2
WILLOWS ROAD	2	2	0-2	1-2	1-2	2
132ND N.E.	2	2	0-2	1-2	0	1-2
N.E. 72ND ST.	0	4	0	1	0	0
COLLECTORS:						
158TH AVE. N.E. (N.E. 86TH ST. TO REDMOND WAY)	0-2	4/5	0-1	2	0	0
159TH AVE. N.E.	2	2	-	0-2	0	0
156TH AVE. N.E. (N.E. 51ST TO OLD RED ROAD)	2	2	0-2	2	0	1
161ST AVE. N.E. (N.E. 90TH ST. TO CLEVELAND ST.)	0-4	4/5	0-1	2	0	0
N.E. 111TH STREET	2	2	0	1	0	2
N.E. 10TH STREET	2	2	2	2	1	1
N.E. 10TH	2	2	2	2	0	2
164TH AVE. N.E.	2-4	2-4	2	2	0-2	0-2
N.E. 95TH STREET	2	2	1	2	0	1
N.E. 80TH STREET (REDMOND WAY TO 171ST AVE.)	2	2-4	0-2	2	0	2
172ND AVE. N.E.	2	2	2	2	1	1
N.E. 20TH STREET	4	4	0-1	2	0	0
180TH AVE. N.E. (SOUTH OF UNION HILL ROAD)	2	2	0	2	2	2
N.E. 80TH ST. (132ND AVE. N.E. TO 140TH AVE.)	2	2	1	2	1	1
AVONDALE WAY	4	4	2	2	0	0
171ST AVE. N.E.	2	2	1-2	1-2	1	1
N.E. 85TH STREET EXTENSION	0-2	2	1	2	0	0

* May include left turn lanes or two-way left-turn lanes except as noted by 2/3 or 4/5.

** Does not include Class I or Class III Bicycle facilities.

TABLE 3: Functional Arterial Classification Summary for the Long Range Transportation Plan (*Recommended changes in bold italics*)

STREET	TRAFFIC LANES*		SIDEWALKS		BICYCLE FACILITIES**	
	BUILT	PLANNED	BUILT	PLANNED	BUILT	PLANNED
PRINCIPAL ARTERIALS:						
AVONDALE RD. N.E.	2	4	0	2	1	2
REDMOND WAY (except couplet)	4/5	4/5	0-2	2	0	0
REDMOND WAY (couplet)	2-3	2-3	2	2	0	2
CLEVELAND ST. (couplet)	2-3	2-3	2	2	0	0
REDMOND-WOODINVILLE RD./ 164TH AVE. N.E. - north of NE 90TH ST.	2-4	2-4	0-2	1	0	1
REDMOND-WOODINVILLE RD./ 164TH AVE. N.E. - south of NE 90TH ST.	2-4	3-4	0-2	2	0	2
SR 520	2-4	4	0	0	2	2
SR 901 EXTENSION/W. LK. SAMMAMISH PKWY. N.E. EXTENSION	0	4/5	0	0	0	0
SOUTH BYPASS	0	4-5	0	2	0	2
W. LK. SAMMAMISH PKWY. N.E. (BELLVUE-REDMOND RD. TO REDMOND WAY)	2	4	0	2	0-2	2
N.E. 24TH ST. (148TH TO 156TH AVE. N.E.)	4	4	2	2	0	0
N.E. 90TH ST. (WILLOWS RD. TO SR 202)	0-2	3-4	0	2	0	2
N.E. 124TH ST.	2	2-4	0	0-2	0	2
148TH AVE. N.E.	4	4	2	2	0	0
148TH AVE. N.E. EXTENSION	0	4	0	2	0	0
160TH AVE. N.E.	2	2-4	0	2	0	0
MINOR ARTERIALS:						
BELLEVUE-REDMOND RD.	2-4	4	0-2	2	0	2
E. LK. SAMMAMISH PKWY. N.E.	2	4	0	0	0	2
LEARY WAY N.E.	2	3	0-2	1-2	0	2
N.E. UNION HILL RD.	2-4	2-4	0-2	0-2	0	0-2
NOVELTY HILL RD.	2	2	0	0	0	1-2
OLD REDMOND RD.	2	2-4	0-2	2	0-2	1
W. LK. SAMMAMISH PKWY. N.E.	2	2	0	0	1	1-2
WILLOWS RD.	2	2	0-2	1-2	1-2	2
N.E. 24TH ST. (156TH TO W. LK. SAMMAMISH PKWY. N.E.)	2	2	2	2	1	2
N.E. 40TH ST.	2-4	2-4	0-2	2	0-2	2

TABLE 3 (Continued):

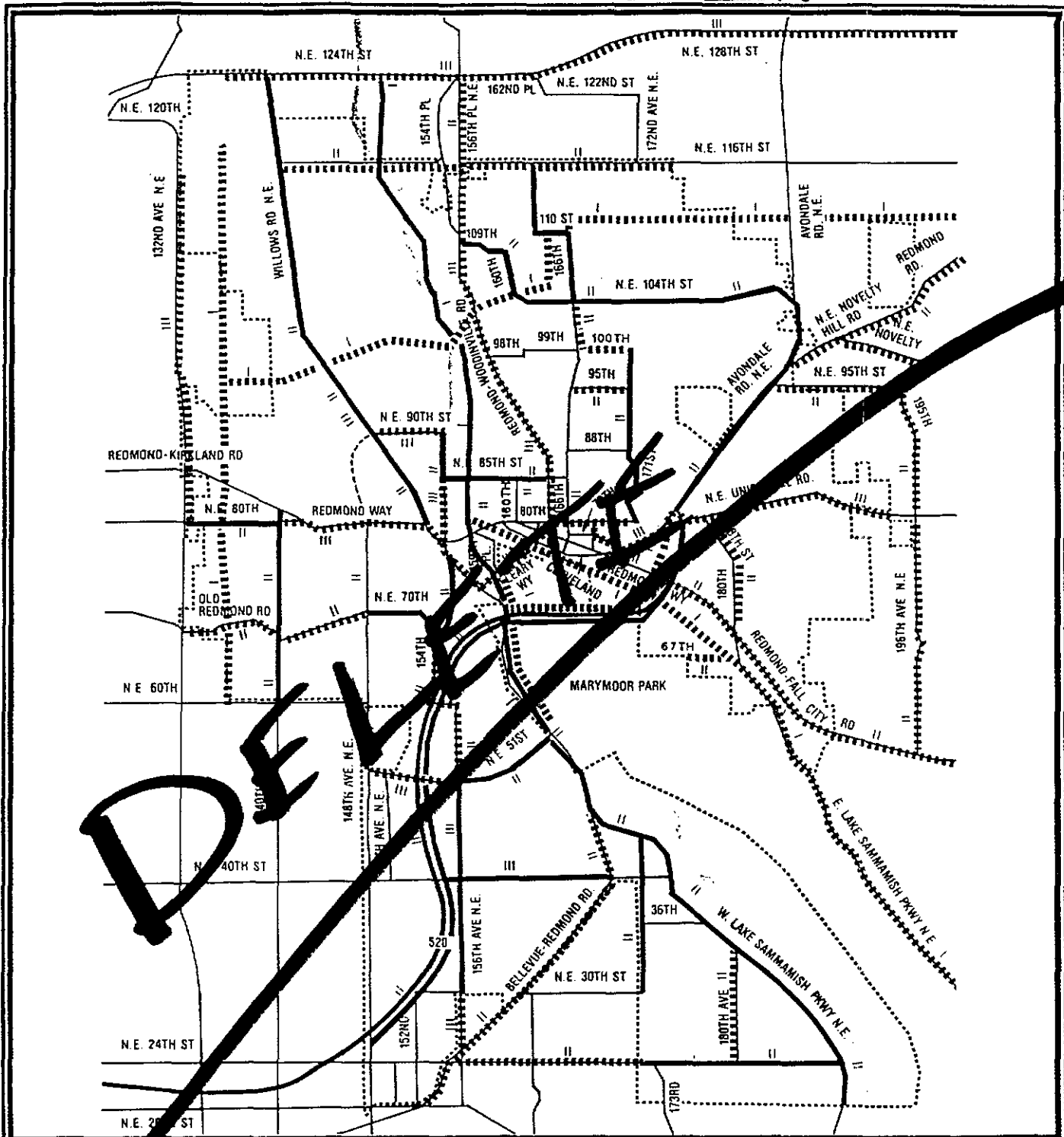
STREET	TRAFFIC LANES*		SIDEWALKS		BICYCLE FACILITIES**	
	BUILT	PLANNED	BUILT	PLANNED	BUILT	PLANNED
N.E. 51ST ST.	2-4	2-4	1-2	2	0	0-2
N.E. 72ND ST.	0	4	0	1	0	0
N.E. 85TH ST.	4	3	1-2	2	2	2
132ND AVE. N.E.	2	2	0-2	1-2	0	1-2
140TH AVE. N.E.	2	2	0	2	0-1	1
156TH AVE. N.E.	4	4	2	2	2	2
(N.E. 20TH to N.E. 51ST ST.)						
170TH PL./AVE. N.E.	2	3-4	0-2	2	0	0
COLLECTOR ARTERIALS:						
AVONDALE WAY N.E.	4	4	2	2	0	0
N.E. 20TH ST.	4	4	0-1	2	0	0
N.E. 80TH ST. (132ND TO 140TH AVE. N.E.)	2	2	1	2	1	1
N.E. 80TH ST. (REDMOND WAY TO 171ST AVE. N.E.)	2	2-4	0-2	2	0	2
N.E. 83RD ST.	0-2	2-3	0-2	2	0	2
N.E. 95TH ST.	2	2	1	2	0	1
N.E. 100TH ST.	2	2	2	2	0	2
N.E. 104TH ST.	2	2	2	2	1	1
N.E. 116TH ST.	2	2	0	1	0	2
156TH AVE. N.E. (N.E. 51ST ST. TO OLD REDMOND RD.)	2	2	0-2	2	0	1
158TH AVE. N.E. (N.E. 86TH ST. TO REDMOND WAY)	0-2	4/5	0-1	2	0	0
159TH AVE. N.E.	2	2	-	0-2	0	0
161ST AVE. N.E. (N.E. 90TH ST. TO CLEVELAND ST.)	0-4	3	0-1	2	0	2
166TH AVE. N.E.	2-4	2-4	2	2	0-2	0-2
169TH AVE. N.E.	2	2-3	0-2	2	0	2
171ST AVE. N.E.	2	2	1-2	1-2	1	1
172ND AVE. N.E.	2	2	2	2	1	1
180TH AVE. N.E. (SOUTH OF N.E. UNION HILL RD.)	2	2	0	2	2	2

* May include left turn lanes or two-way left-turn lanes except as noted by 2/3 or 4/5.

** A Class I, II or III bicycle designation is planned on or near the arterial as indicated. See the bicycle plan maps.

2/9/93

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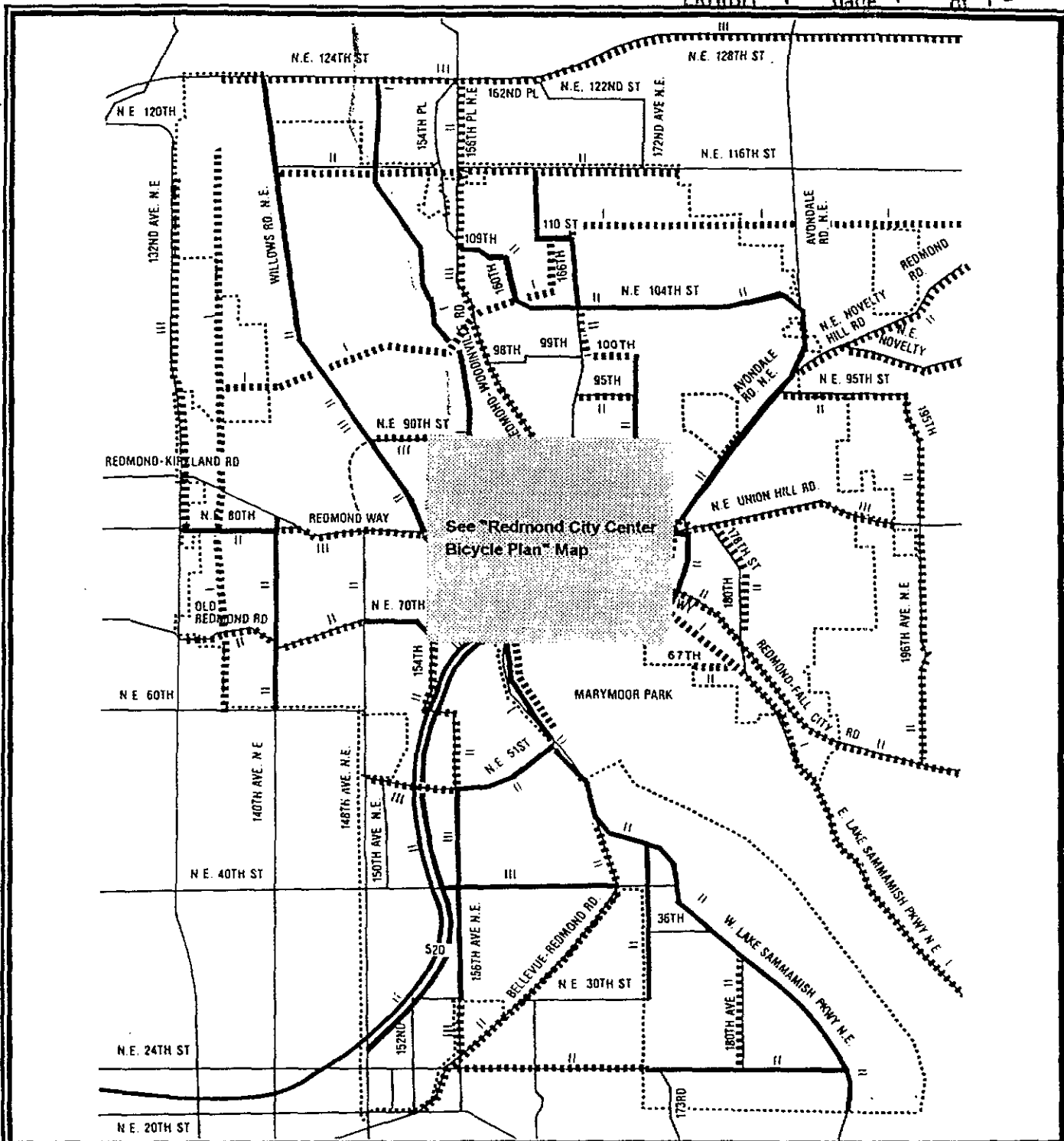


BIKE WAY PLAN



- EXISTING BIKEWAY
- PROPOSED BIKEWAY

Bikeway Classification
 Class I
 Class II
 Class III



BIKE WAY PLAN

EXISTING BIKEWAY

***** PROPOSED BIKEWAY

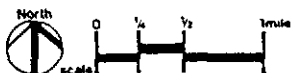
Bikeway Classification

Class I

Class II

Class III

7/88



BIKEWAY (Class I)

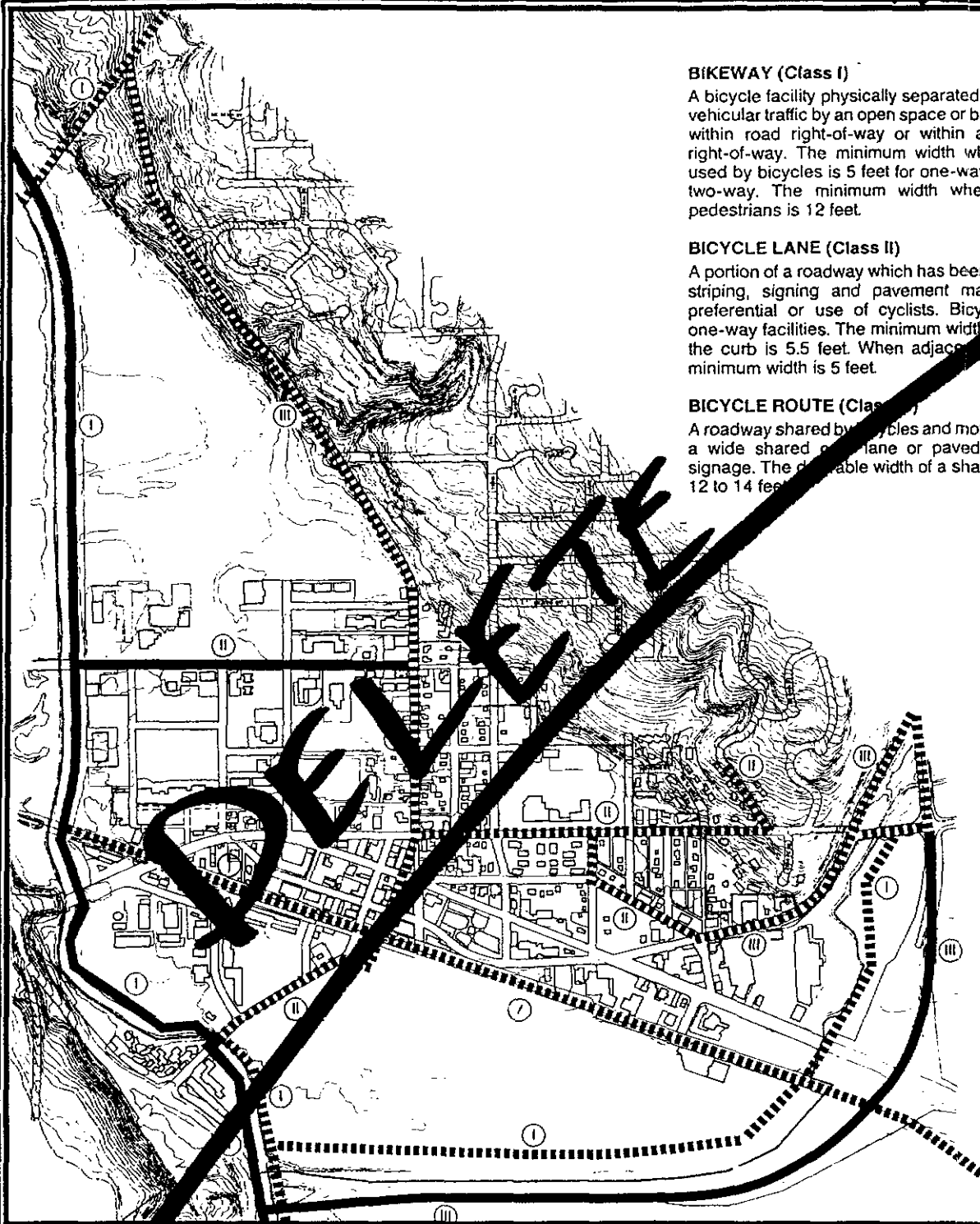
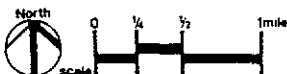
A bicycle facility physically separated from motorized vehicular traffic by an open space or barrier and either within road right-of-way or within an independent right-of-way. The minimum width when exclusively used by bicycles is 5 feet for one-way, and 8 feet for two-way. The minimum width when shared with pedestrians is 12 feet.

BICYCLE LANE (Class II)

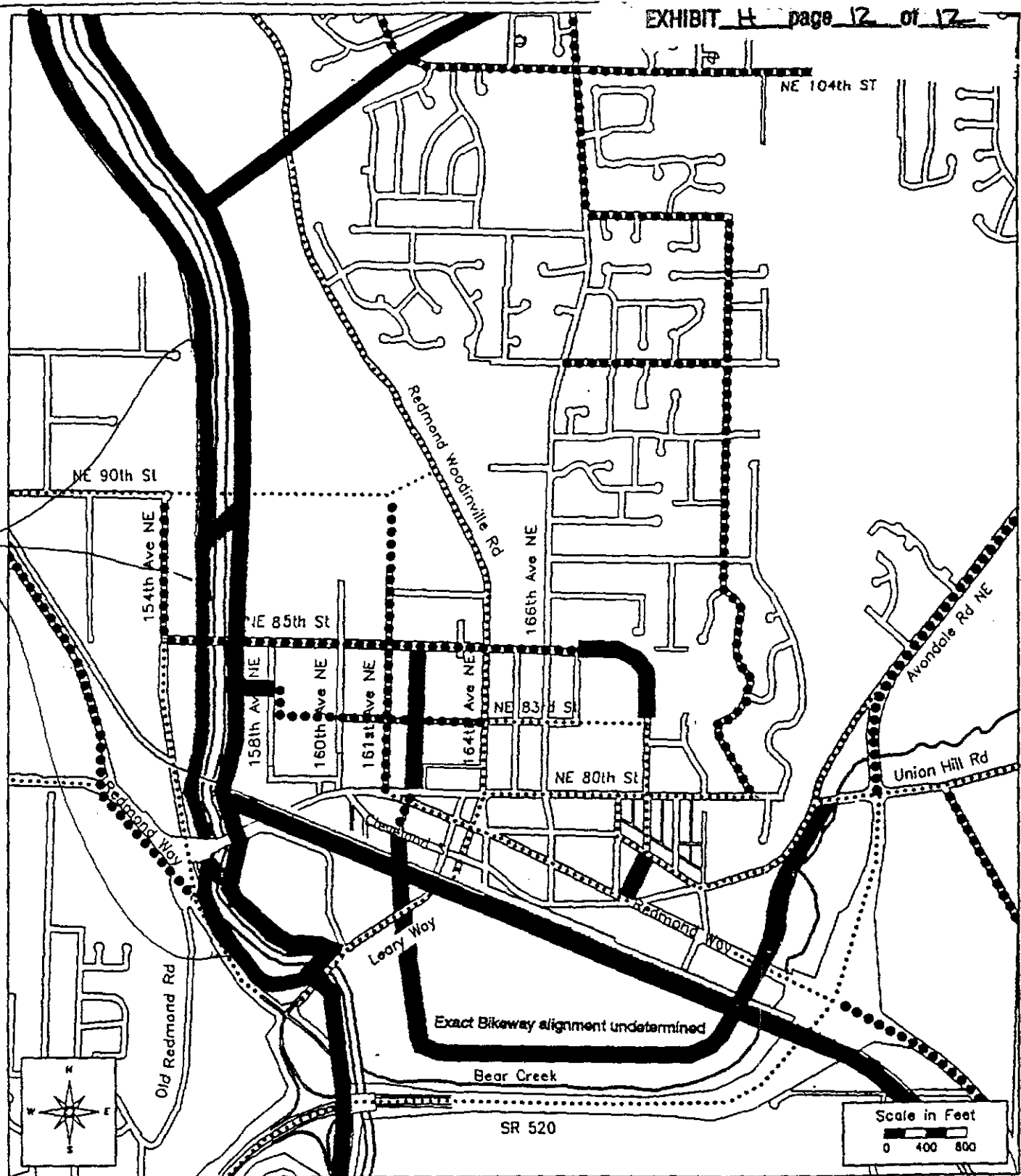
A portion of a roadway which has been designated by striping, signing and pavement markings to be preferential or use of cyclists. Bicycle lanes are one-way facilities. The minimum width when abutting the curb is 5.5 feet. When adjacent to parking, the minimum width is 5 feet.

BICYCLE ROUTE (Class III)

A roadway shared by bicycles and motor vehicles with a wide shared curb lane or paved shoulder, and signage. The desirable width of a shared curb lane is 12 to 14 feet.

**CITY CENTER BIKE PLAN**

————— EXISTING BIKEWAY
 - - - - - PROPOSED BIKEWAY

NEW
LINKS

Redmond City Center Bicycle Plan

- █ Bikeway (Class I):** A bicycle facility physically separated from motorized vehicular traffic by an open space or barrier and either within road right-of-way or within an independent right-of-way. The minimum width when exclusively used by bicycles is 5 feet for one-way travel and 8 feet for two-way travel. The minimum width when shared with pedestrians is 12 feet.
- Bicycle Lane (Class II):** A portion of a roadway which has been designated by striping, signing and pavement markings for the preferential use of cyclists. Bicycle Lanes are one-way facilities. The minimum width when abutting the curb is 5.5 feet. When adjacent to parking, the minimum width is 5 feet.
- Bicycle Route (Class III):** A roadway shared by bicycles and motor vehicles with a wide shared curb lane or paved shoulder, and signage. The desirable width of a shared curb lane is 12 to 14 feet.

20B.50 PARKS, RECREATION AND OPEN SPACE**20B.50.010 PARKS PROFILE**

The City of Redmond, King County and other public and private agencies have taken advantage of the natural condition in and around the City to provide a variety of parks and recreation facilities. These include neighborhood, community and regional parks, hiking and bicycle trails, tennis courts, and golf courses. The area's school supplements this supply with athletic fields, playgrounds, gymnasiums and multipurpose rooms.

Redmond's park, recreation and open space system has three basic functions:

- It attempts to serve existing and future demand for a variety of park types (e.g. neighborhood, regional) and a multitude of recreation activities (e.g. fishing, picnicking, baseball).
- It provides for the conservation of important environmental resources which might otherwise be lost to development (e.g. shorelines, wetlands).
- It sets aside areas which are unsuitable for development due to flooding, erosion, or other hazards (e.g. flood plains, steep slopes).

The following is an inventory summary of existing parks, recreation and open space in the Redmond Planning Area by Planning Districts. Neighborhoods are defined in Section 20B.85 which describes goal, natural features, character and design.

EXISTING PARKS, RECREATION AND OPEN SPACE**Bear Creek Neighborhood****Parks**

Arthur Johnson Community Park

7901 - 196th Avenue N.E.

Ownership - City of Redmond

Type - Neighborhood Park

Acres - 15

Development - Undeveloped

Notes - Partial development for outdoor activities, fishing along Evans Creek

Marymoor Regional Park

6046 W. Lake Sammamish Parkway

Ownership - King County

Type - Regional

Acres - 523

Development - Baseball/softball fields, MG, soccer fields, velodrome, tennis courts, trails, model airplane flying area, picnic areas, picnic shelters, picnic table, museum, children's play areas

Sportsman Park

6520 - 187th Avenue N.E.

Ownership - Private

Type - Community

Acres - 5

Development - Shooting Range

City Center Neighborhood**Parks**

Albert Anderson Park

7802 - 168th Ave. N.E.

Ownership - City of Redmond

Type - Neighborhood Park

Acres - 2.8

Development - Covered picnic area, picnic tables, children's play area, outdoor stage and plaza, historic cabins, horseshoe pit, bicycle racks, and restrooms.

Luke McRedmond Landing

15811 Redmond Way

Ownership - City of Redmond

Type - ~~Neighborhood Park~~ Resource Park

Acres - 2.5

Development - Picnic area, picnic shelter, picnic tables, canoe launch, Sammamish River Trail, fishing pier for the handicapped.

Redmond Elementary School

16600 N.E. 80th

Ownership - Lake Washington School District

Type - Neighborhood Park

Acres - 40 11.8

Development - Baseball/softball fields, soccer fields, basketball/volleyball, tennis courts

**Central
Downtown Park**

Northeast corner of NE 83rd Street
and 161st Avenue NE

Type - ~~Passive and Active Open Space~~

Acres - 1.4 existing of possible 3 to 6 acres

Development - Unimproved

Multi-use Plaza/
Urban Park

(2 to 5)

Other Open Space

Sammamish River Regional Trail

Ownership - King County

Type - Regional

Acres - ~~65~~ 18.8 within City Center

Development - Paved multi-purpose trail, benches

Municipal Campus

15670 N.E. 85th St.

Type - ~~Community Park~~ Open Space

Acres - ~~22~~ 10.5

Development - Municipal building, senior center plaza, picnic tables, open space

Bear Creek Park

Bear Creek Village

Ownership - City of Redmond

Type - ~~Neighborhood Park~~ Resource Park

Acres - 4.5

Development - Undeveloped

Education Hill NeighborhoodParks

Jonathon Hartman Park

17300 N.E. 104th St.

Ownership - City of Redmond

Type - Community park

Acres - 40

Development - Baseball/softball fields, tennis courts, basketball court, children's play area, soccer field, picnic area, picnic tables, nature trails, King County swimming pool.

Reservoir Park

16317 N.E. 95th St.

Ownership - City of Redmond

Type - Resource Park

Acres - 2

Development - 4 tennis courts, picnic area

Nike Neighborhood Park

17207 N.E. 92nd St.

Ownership - City of Redmond

Type - Neighborhood Park

Acres - 5

Development - Children's play area, basketball/pickleball court, trails, picnic tables, open space.

Meadow Park

10710 - 160th Ave. N.E.

Ownership - City of Redmond

Type - Neighborhood Park

Acres - 5

Development - Children's play area, basketball court, pickleball courts, picnic area, picnic tables, par course

Redmond Pool

17535 N.E. 104th St.

Ownership - King County

Type - Community

Acres - Located in Hartman Park

Development - Indoor Swimming Pool

Schools

Horace Mann Elementary School

17001 N.E. 104th

Ownership - Lake Washington School District

Type - Neighborhood

Acres - 10

Development - Children's play area, half court basketball, baseball/softball fields, soccer fields, basketball/volleyball gymnasium.

Redmond Junior High School

10055 - 166th N.E.

Ownership - Lake Washington School District

Type - Community

Acres - 24.3

Development - Tennis courts, baseball/softball fields, football fields, track, basketball/volleyball gymnasium.

Redmond Senior High School

17272 N.E. 104th

Ownership - Lake Washington School District

Type - Community

Acres - 40

Development - Tennis courts, track, baseball/softball fields, football fields, basketball/volleyball, gymnasium, stadium.

Rockwell Elementary School

11125 - 162nd N.E.

Ownership - Lake Washington School District

Type - Neighborhood

Acres - 13.3

Development - Baseball/softball field, basketball court, soccer field.

Other Open Space

Puget Power/City of Redmond Multi-purpose trail
 Ownership - Puget Sound Power and Light (25 yr. lease to City)
 Type - Trail and Open Space
 Acres - 90
 Development - Dirt trails

Grass Lawn NeighborhoodParks

Grass Lawn Community Park
 7031 - 148th Ave. N.E.
 Ownership - City of Redmond
 Type - Community Park
 Acres - 28.5
 Development - Lighted tennis courts, baseball/softball fields, lighted soccer field, picnic shelter, children's play areas, par course, basketball court, trails, picnic tables, bleachers.

Schools

Rose Hill Junior High School
 13505 N.E. 75th
 Ownership - Lake Washington School District
 Type - Community
 Acres - 23.4
 Development - Track, baseball/softball fields, football fields, basketball/volleyball gymnasium, tennis courts.

Parks

Strattonwood Swim Club
 7616 - 139th Pl. N.E.
 Ownership - Private
 Type - Community
 Acres - 5
 Development - Swimming pool

Overlake NeighborhoodParks

Spiritbrook Park
 6500 - 15th Street
 Ownership - City of Redmond
 Type - Neighborhood Park
 Acres - 2
 Development - Children's play area, baseball/softball field, picnic area, picnic table.

Westside Park
 5850 - 156th Ave. N.E.
 Ownership - City of Redmond
 Type - Neighborhood Park
 Acres - 6.4
 Development - Baseball/softball field, children's play area, picnic area, trail

Idylwood Beach Park
 Ownership - King County
 Type - Community Park
 Acres - 10
 Development - Picnic area, picnic tables, swimming beach, children's play area.

Schools

Benjamin Rush Elementary School
 6101 - 152nd N.E.
 Ownership - Lake Washington School District
 Type - Neighborhood
 Acres - 9.4
 Development - Baseball/softball field, soccer field, basketball/volleyball gymnasium.

Sammamish Valley NeighborhoodParks

Sixty Acres Park
 N.E. and the Sammamish River
 Ownership - King County (leased to Lake Washington Youth Soccer)
 Type - Community
 Acres - 60
 Development - Soccer fields

Viewpoint NeighborhoodParks

Viewpoint Park
 18440 N.E. 24th
 Ownership - City of Redmond
 Type - Neighborhood Park
 Acres - 5
 Development - Children's play area, basketball court, pickleball courts, picnic tables, trails, picnic area.

Viewpoint Open Space
 17541 N.E. 24th
 Ownership - City of Redmond
 Type - Resource Park
 Acres - 10
 Development - Undeveloped
 Note - Adjacent to 10 acres of Bellevue open space

Schools

Audubon Elementary School
 3045 - 180th N.E.
 Ownership - Lake Washington School District
 Type - Neighborhood
 Acres - 9.4
 Development - Baseball/softball fields, soccer field, children's play area, basketball/volleyball, multi-purpose room.

Other Open Space

Brae Burn Golf Course
 Ownership - Private
 Type - Community
 Acres - 40
 Development - 9-inch golf course, swimming pool

Tam O'Shanter Golf Course
 Ownership - Private
 Type - Community
 Acres - 40
 Development - 9-hole golf course, tennis courts, swimming pool.

Willows Neighborhood

Parks

Welcome Property
 141st Ave. N.E. and N.E. 84th
 Ownership - City of Redmond
 Type - Resource Park
 Acres - 2.5
 Development - Undeveloped

Boys Club Property
 Ownership - Private
 Type - Community
 Acres - 10
 Development - Baseball/softball fields, multi-purpose room, basketball/volleyball gymnasium.

North Union Hill Neighborhood

Parks

Farrel-McWhirter Park
 19200 N.E. 102nd Pl.
 Ownership - City of Redmond
 Type - Resource Park
 Acres - 76
 Development - Picnic shelters, horse arena, children's barnyard, orienteering course, picnic tables, nature trails.

Redmond Watershed Property
 Ownership - City of Redmond
 Type - Resource Park
 Acres - 800
 Development - Undeveloped

Other Open Space

Bear Creek Golf Course
 Ownership - Private
 Type - Community
 Acres - 90
 Development - 18 hole golf course

Union Hill Neighborhood

Parks

Kathryn Adams Lewis Park
 234th N.E. Union Hill Road
 Ownership - King County
 Type - Community
 Acres - 10
 Development - Undeveloped

30 Acres Park
 6500 - 196th N.E.
 Ownership - King County
 Type - Community
 Acres - 38.17
 Development - Undeveloped

Schools

Alcott Elementary School
 4213 - 228th N.E.
 Ownership - Lake Washington School District
 Type - Neighborhood
 Acres - 8

Development - Baseball/softball field, soccer field, basketball/volleyball court gymnasium.

Dickinson Elementary School

7300 - 208th N.E.

Ownership - Lake Washington School District

Type - Neighborhood

Acres - 8

Development - Baseball/softball fields, soccer field, basketball/volleyball court, gymnasium.

Evergreen Junior High School

6900 - 208th N.E.

Ownership - Lake Washington School District

Type - Community

Acres - 22

Development - Baseball/softball fields, football fields, track, basketball/volleyball court gymnasium, tennis court.

Sahalee Neighborhood

Parks

Mead Park

Ownership - King County

Type - Community

Acres - 18.5

Development - Undeveloped

Schools

Mead Elementary School

1725 - 216th N.E.

Ownership - Lake Washington School District

Type - Neighborhood

Acres - 9

Development - Baseball/softball field, soccer field, basketball/volleyball court gymnasium.

Other Open Space

Sahalee Golf and Country Club

Ownership - Private

Type - Community

Acres - 206

Development - 27 hole golf course

South Hollywood Neighborhood

Parks

No facilities.

20B.50.020 PROJECTED NEEDS

GUIDELINES AND STANDARDS

Each type of park provides a function in serving the residents of the community. However, these guidelines must be tempered by the type and style of residential development which will take place in the planning area and the supply of regional facilities provided by King County and others. As the eastern neighborhoods are developed, with the population densities and open space and park amenities proposed in the development plans, the need for neighborhood parks is reduced. Community parks will be more appropriate in these neighborhoods, assuming the planned development takes place.

Service Radius and Accessibility

Recreation facilities and parks serve a certain population depending on the size and type of recreation opportunity offered. To provide an accessible park system to all neighborhoods, guidelines have been established for park service radius. Three separate types of parks are identified in these guidelines: neighborhood parks, community parks, and resource parks.

1. Neighborhood parks

Uses -	Space for passive and active uses; picnicking, informal areas for outdoor sports, trails, children's play areas, court and field sports.
Service Area -	.25 to 5 mile radius
Guideline -	1.0 to 2.0 per 1000 population
Desired size -	7 to 10 acres

2. Community Parks

Uses -	Diversified active and some passive uses: baseball/softball complexes, tennis, basketball and pickleball courts. Use may be extended with lighting. Areas are provided for
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children's play equipment,
jogging and walking paths
with natural wooded areas
for outdoor enjoyment.

Service Area -	1 to 3 mile radius
Guideline -	5.0 to 8.0 acres per 1000 population
Desired size -	25 to 40 acres

3. Resource Parks

Uses -	Special use facilities, open space, variable environ- mental qualities. Provide outdoor recreational opportunities such as walking, hiking, jogging, cycling, equestrian, camping, bird watching, interpretive trails, picnic- ing and water-oriented recreation.
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Service Area -	No applied guideline
Guideline -	Variable
Desired size -	Large enough to protect natural resources and provide maximum public benefit; variable depending desired use.

Facility Guidelines

The preparation of the guidelines were derived from a review of the following information:

- Guidelines from surrounding communities
- The actual number of facilities in Redmond per 1000 population
- Participation rates for programmed activities
- Accepted national facility guidelines

The guidelines for recreation facilities utilized by the cities of Bellevue, Kirkland and Issaquah are influencing in that they are serving similar populations with similar lifestyle.

Also, their supply or lack of supply of facilities can impact the use of Redmond's recreation facilities due to the transient nature of some users, especially adult sports teams.

The actual number of facilities provided by Redmond per 1000 population also helps in this analysis. This number may not be desirable, but it does indicate what the residents are accustomed to using. This, coupled with the participation rated for programmed activities, provides a picture of the current situation.

Utilizing national standards as a check, a professional judgment is made concerning the appropriate guidelines for the community.

This list of facilities focuses on the major elements of a park and recreation system. Many other types of facilities are essential to a quality system to meet the needs of the public.

20B.50.030 MISSION

PHILOSOPHY:

"Preserve Redmond's natural environment by minimizing the alteration of natural land features..."

"Maintain the community's natural character through the extensive use of landscaping."

The Mission is to establish a reasoned process for acquiring and maintaining parks, recreation facilities, and open space which retains the balance of green space consistent with the city's foundations. This balance will evolve to follow the urban development that will continue during the planning period. The following mission statement, goals, policies, and guidelines have been adopted by the City Council to direct the planning and implementation decisions which will see the parks and recreation system into the next century.

MISSION STATEMENT

The City of Redmond Parks and Recreation Department is committed to

PROTECT Redmond's natural beauty through a vibrant system of Parks and Open Space

Table A
FACILITY GUIDELINES

<u>Facility</u>	<u>Guideline</u>
Tennis Courts	1 per 2500 of population
Soccer Fields (Game Grade)	1 per 3000 of population
Soccer Fields (Practice Grade)	1 per 3000 of population
Gymnasiums	1 per 1000 of population
Baseball/Softball Fields (Game Grade)	1 per 3000 of population
Baseball/Softball Fields (Practice Grade)	1 per 3000 of population
Trails	1.1 miles per 1000 population
Children's Play Area	1 per 3000 of population
Swimming Pools	1 per 15,000 of population
Golf Courses	1 per 50,000 of population

Park classifications are also provided with guidelines to act as a measure of adequacy of supply. Again, these are only guidelines and are tempered by the density of development and the capability of the land.

Table B
PARK CLASSIFICATION GUIDELINES

Neighborhood Parks	1 to 2 acres per 1000 population
Community Parks	5 to 8 acres per 1000 population
Open Space	10% of the total planning area*

* The open space guideline includes public and private lands which are considered free from development. This includes parks, school property, lands from which the development rights have been purchased, and land zones and/or designated as unbuildable.

Table C
 PLANNING AREA RECREATION FACILITY DEMAND AND NEED SUMMARY

<u>Facility</u>	<u>1989 Demand</u>	<u>2000 Demand</u>	<u>1989 Demand</u>	<u>2000 Need</u>
Tennis Courts	25	39	6	20
Soccer Fields (G)	20	33	6	19
Soccer Fields (P)	20	33	16	29
Gymnasiums	6	10	3	7
Base/Softball Fields (G)	20	33	8	21
Base/Softball Fields (P)	20	33	10	23
Children's Play Area	20	33	6	19
Swimming Pools	4	7	3	6
Golf Course's	1	2	1	2
Trail Miles	68	108	50	91

PROVIDE citizens of all ages wholesome recreational and cultural opportunities in clean, safe, and accessible facilities; and

PRESERVE our quality living environment for future generations.

20B.50.040 COMMUNITY GOAL

Protect Redmond's natural beauty through a vibrant system of Parks and Open Space.

50.040(05) Policy - Encourage the acquisition of greenbelts and park land to maintain the sense of open space, protect environmental resources, provide circulation linkages, and ensure adequate separation and buffers between various land uses.

50.040(10) Policy - Reduce the impacts of development and ensure provisions for open space by encouraging private developments to provide land for parks, trails, walkways, and open space.

50.040(15) Policy - Coordinate Park planning and land acquisition with other city plans for roads, utilities and buildings, therefore maximizing the benefits available from public lands for parks, programs and recreational activities.

50.040(20) Policy - Encourage parks, beautification areas, and open space throughout the Central Business District by coordinating planning efforts with other City departments and private businesses.

50.040(25) Policy - Promote a street tree program throughout the City.

50.040(30) Policy - Actively pursue private dedication of land through a variety of methods to facilitate public access to parks and provide a continuous system of parks and open space.

50.040(35) Policy - Seek funding opportunities from a variety of sources (Federal, State, and private) for the acquisition and development of parks and open space.

50.040(40) Policy - Develop a Park Plan which is financially feasible and can be funded at a level which allows for a reasonable implementation schedule.

50.040(45) Policy - Utilize quasi-public land, where possible, and dual-use facilities (e.g. Puget Power lines, water department land).

50.040(50) Policy - Encourage a more formal urban park on the Sammamish River Trail between the Municipal Campus and Marymoor Park.

20B.50.050 COMMUNITY GOAL

Provide citizens of all ages wholesome recreational and cultural opportunities in clean, properly maintained, safe and accessible facilities.

50.050(05) Policy - Provide recreation and leisure programs that are comprehensive, enriching, and affordable for all citizens.

50.050(10) Policy - Encourage citizen involvement in the development of policies and guidelines for the Parks and Recreation Open Space Plan.

50.050(15) Policy - Create a balanced system of active and passive recreation opportunities for all ages by providing:

(a) A community center for indoor recreation programs (e.g. athletics, gymnasiums, recreation classes, meeting rooms, social services, and special populations).

(b) Athletic facilities (e.g. multi-use ball fields, tennis courts, lighted play fields, and tournament facilities.)

(c) Facilities to support the cultural arts (e.g. program rooms, performing arts theater, and outdoor concert space).

(d) Facilities and land for passive recreation, environmental education, and outdoor recreation programs.

(e) A linkage system (e.g. bicycle lanes and multi-use trails which connect the park system, schools, and other important public facilities in the City).

(f) Outdoor plazas and squares within the City Center Neighborhood for Community and Civic events, public gatherings, programmed activities and entertainment.

50.050(20) Policy - Monitor existing parks and facilities to ensure that they meet acceptable standards for safety and performance.

50.050(25) Policy - Renovate all parks and facilities in a manner that will, where feasible, provide safe and accessible use by the physically impaired.

50.050(30) Policy - Establish funding to permit the orderly ongoing repair and rehabilitation of existing parks and facilities.

50.050(35) Policy - Reduce maintenance and operation costs by upgrading existing park facilities in a manner which will maximize efficient maintenance practices and conserve resources.

50.050(40) Policy - Develop policies that give City residents priority of preference in registration.

50.050(45) Policy - Maintain a Parks, Recreation and Open Space Plan which is flexible and can respond to changes in user population or recreational preference.

50.050(50) Policy - Develop planning guidelines which address diverse recreational needs, accessibility, service radius, and requirements for open space acreage.

50.050(55) Policy - Focus planning efforts on portions of the city which are experiencing rapid growth or currently lack recreational service.

50.050(60) Policy - Distribute parks and recreation facilities throughout the City in a manner which provides an equitable service radius.

50.050(65) Policy - Encourage the acquisition and development of property which will provide access to water resources such as Lake Sammamish, the Sammamish River, and local streams. Promote the creation of open space corridors along these water resources to provide for recreation and wildlife.

50.050(70) Policy - Work in cooperation with the Lake Washington School District to fully utilize existing recreational facilities and surplus school facilities as they are available.

50.050(75) Policy - Encourage acquisition and development of a multi-use trail system which will provide for several trail uses in one linear space or right-of-way. These primary trails are incorporated into the equestrian trails plan. The multi-use trail system is not only part of the transportation system but a vital part of the recreational equestrian trails plan (see 20B.60.050(15) Multi-Use Trail Plan).

50.050(80) Policy - Provide an equestrian trail system which links parks, schools and public facilities, including connections to trails developed by other agencies in the region.

50.050(85) Policy - Coordinate planning of bike-ways with the planning of the multi-use trails plan and the Equestrian Trails Plan.

50.050(90) Policy - Coordinate development of a bikeway plan with other departments and other jurisdictions. The bikeway plan is located in the Transportation Chapter 20B.60.060(10).

50.050(95) Policy - Work with private organizations and service clubs to encourage the development of special purpose recreation facilities (e.g. ice arenas, swimming pools, golf courses, theaters, etc.)

20B.50.052 COMMUNITY GOAL

Preserve our quality living environment for future generations.

50.052(05) Policy - Conserve existing public lands currently in a natural state through careful planning and cooperative agreements between appropriate government agencies and private enterprise.

50.052(10) Policy - Designate appropriate uses within the capability of the parks, natural areas and greenbelts (e.g. trails and outdoor education, as determined by site analysis).

50.052(15) Policy - Use of appropriate acquisition methods to protect natural areas which are sensitive to urban development or represent a significant natural and visual asset.

50.052(20) Policy - Design and construct park facilities in a manner which is compatible with the neighborhood and is sensitive to the environment.

50.052(25) Policy - Work with other governmental agencies and private organizations to provide a complete Parks and Recreation Open Space System for the City and surrounding region.

20B.50.054 COMMUNITY GOAL

Encourage the retention of open space as an important component of Redmond's character.

50.054(05) Policy - The City should provide for an open space system within and between neighborhoods. An open space system includes the following:

(a) Fragile and valuable elements such as natural drainage features and areas unsuitable for development due to natural hazards;

(b) Forest, agricultural and fisheries resources;

(c) Physical and/or visual buffers within and between areas of urban development;

(d) Outdoor recreation areas, including passive and resource parks and trails.

50.054(10) Policy - Encourage the retention of wetlands, steep slopes, hazardous and other sensitive areas as open space.

50.054(15) Policy - Encourage the joint use of open space for compatible recreation purposes.

50.054(20) Policy - Encourage the donation to the public of those parcels of land that are not buildable and would enhance the open space or protect sensitive areas.

50.054(25) Policy - The City should obtain open space throughout the community.

50.054(30) Policy - Develop and implement a long-term and comprehensive acquisition, dedication, and management program for open spaces.

50.054(35) Policy - Preserve Redmond's semi-rural characteristics, such as abundant open space, by allowing clustered residential development that provides a significant percentage of usable open space where compatible in style and function with surrounding neighborhoods.

20B.50.060 PARKS, RECREATION AND OPEN SPACE PLAN

Redmond's parks, recreation and open space system provides for land and facilities to meet the demand for a variety of park types and recreation activities; it provides for the conservation of important environmental resources; and it limits the development of areas that have severe or very severe natural constraints.

The existing and projected demand for parks and recreation facilities is based on park land guidelines. Existing schools, King County facilities and private facilities are credited with meeting a portion of the net park demand. This is due to the competition for use of these facilities and the limited hours available for City programming. Table D provides the relative supply and demand for park facilities on a city-wide basis. The demand for facilities within each planning district (neighborhood) is found in PRO PLAN 2000.

Figure 1 illustrates the parks, recreation and open space plan for the Redmond Planning Area. Included in the plan are existing and proposed parks, and open spaces, school facilities, King County Parks and major private recreation facilities.

Figure 3 and Table G illustrate a more detailed parks, recreation and open space plan for the City Center Neighborhood, and identifies existing and planned additions to the system. The planned locations are illustrative, and may vary, depending upon availability of land at the time of improvement.

Figure 4 identifies planned routes for an "Art Loop Trail" through the City Center Neighborhood. The art loop concept is further described within the document titled City of Redmond Public Artwork Site Plan and Inventory, March 1992.

20B.50.070 TRAIL PLAN

The Trails Plan provides a system of multi-purpose trails which link public lands with residential, employment and shopping areas throughout the neighborhoods. In addition, Redmond's trails plan provides connections with the regional trail system which links Redmond with surrounding communities and unincorporated King County.

Table D

PARKS, RECREATION, AND OPEN SPACE PLAN SUMMARY

Neighborhood	1989 Neighborhood Characteristics	1989 Park Facilities & Acreage	1989 Population	2000 Population	Planned Future Neighborhood Characteristics	Top Priority Parks and Recreation Needs
Willows	Sparsely low to medium density residential; some multi-family residential; some industrial uses	2.5 AC; undeveloped	2,312	4,700	Limited low to medium density residential steep slopes in open space	Tennis court, soccer fields, children's play area, baseball/ soccer fields
Grass Lawn	Medium density residential; one high density P.U.D.	1 Community Park 28.5 acres	5,814	7,000	Medium density	Soccer and base/softball fields
Sammamish Valley	Mainly very low density agricultural and open space soccer facility 60 acres	King Co. owned privately leased	476	1,800	Continued agricultural and open space use	Trails development
City Center	Commercial-Office residential mix	3 Neighborhood/Community parks (17 acres); 3 Open Space parks (80 acres)	850	2,200	Commercial, office & residential	Trail linkages to river, urban plazas and gathering places, neighborhood parks *
Overlake	Developing comm'l, and industrial park uses; medium density residential	2 Neighborhood 8 acres	6,086	10,200	New multi-family residential; comm'l industrial park dev.	Tennis courts, soccer fields, base/softball fields, trails,
Education Hill	Medium density suburban residential 2 Neighborhood Parks 10 acres 1 Resource Park	1 Community Park 40 acres	11,084	14,700	Medium density residential	Soccer, base/softball fields
Bear Creek	Industrial/commercial uses/ multi-family residential 523 acres	15 ac. undeveloped Marymoor Park	1,127	2,300	Industrial, commercial, some residential	Trail linkages

Neighborhood	1989 Neighborhood Characteristics	1989 Park Facilities & Acreage	1989 Population	2000 Population	Planned Future Neighborhood Characteristics	Top Priority Parks and Recreation Needs
8. Viewpoint	Medium density suburban residential 1 Resource park 10 acres	1 Neighborhood Park 5 acres	6,256	7,500	Medium density residential	Tennis courts, base/softball fields, soccer fields
9. Hollywood Hill	Low density ranch estate type residential	No facilities	4,050	9,000	Low to medium density residential children's play areas	Tennis courts, soccer fields, base/softball fields, trails,
10. North Union Hill	Mixed use/density	No city facilities	5,900	11,500	Mixed use/density	Need all facilities
11. South Union Hill	Mixed use/density	No city facilities	6,150	14,000	Mixed use/density	Need all facilities
12. Sahalee/Inglewood	Mixed use/density	No city facilities	11,400	13,500	Mixed use/density	Need all facilities

Table E

Projected Demand for 1989 and 2000
Summary by Neighborhood

NEIGHBORHOOD	1989 Projected <u>Demand</u>						2000 Projected <u>Demand</u>					
	Tennis	Soccer	Soccer	Baseball	Baseball	Child's	Soccer	Soccer	Baseb	Baseball	Child's	
	Courts	Fields (G)	Fields (P)	Fields (G)	Fields (P)	PlayTennis AreaCourts	Fields (G)	Fields (P)	Fields (G)	Fields (P)	Play Area	
Willows	1	1	1	1	1	1	2	2	2	2	2	2
Grass Lawn	2	2	2	2	2	1	3	2	2	2	2	2
Sammamish V.	0	0	0	0	0	0	1	1	1	1	1	1
City Center	0	0	0	0	0	0	1	1	1	1	1	1
Overlake	2	2	2	2	2	2	4	3	3	3	3	3
Education Hill	4	4	4	4	4	4	6	5	5	5	5	5
Bear Creek	0	0	0	0	0	0	1	1	1	1	1	1
View Point	2	2	2	2	2	2	3	2	2	2	2	2
S. Hollywood	2	1	1	1	1	1	1	4	3	3	3	3
N. Union Hill	2	2	2	2	2	2	5	4	4	4	4	4
S. Union Hill	2	2	2	2	2	2	6	5	5	5	5	5
Sahalee/Ingle	5	4	4	4	4	4	5	4	4	4	4	4

(G) = Game Grade Facility

(P) = Practice Grade Facility

Table F

Projected Need for 1989 and 2000
Summary by Neighborhood

NEIGHBORHOOD	Tennis Courts	1989 Projected Need		2000 Projected Need		Child's Play Area	Tennis Courts	Soccer Fields (G)	Soccer Fields (P)	Baseball Fields (G)	Baseball Fields (P)	Child's Play Area
		Soccer Fields (G)	Soccer Fields (P)	Baseball Fields (G)	Baseball Fields (P)							
Willows	1	1	1	0	1	1	2	2	2	1	2	2
Grass Lawn	0	1	2	0	2	0	0	1	2	0	2	0
Samamish V.	0	0	0	0	0	0	1	0	1	1	1	1
City Center	0	0	0	0	0	0	0	1	0	0	0	1
Overlake	2	2	2	2	1	0	4	3	3	3	2	1
Education Hill	0	3	1	0	1	0	4	3	3	3	2	1
Bear Creek	0	0	0	0	0	0	1	0	1	0	1	0
View Point	2	2	1	2	1	0	3	2	2	2	1	0
S. Hollywood	2	1	1	1	1	1	4	3	3	3	3	3
N. Union Hill	2	2	2	2	2	1	5	4	4	4	4	3
S. Union Hill	1	2	1	1	0	2	5	5	4	4	3	5
Sahalee/Ingle	5	4	3	4	3	4	5	5	4	5	4	5

(G) = Game Grade Facility

(P) = Practice Grade Facility

Note: The need for facilities city-wide compared to the composite neighborhood need will differ because several neighborhoods have the majority of the supply. Subtracting the effective supply of facilities from the demand does not consider the surplus of facilities in the neighborhood. Therefore, some facilities are lost thus increasing the apparent neighborhood need. Actual need is a judgment using a comparison of the neighborhood and city-wide data.









TABLE G
CITY CENTER PARK PLAN

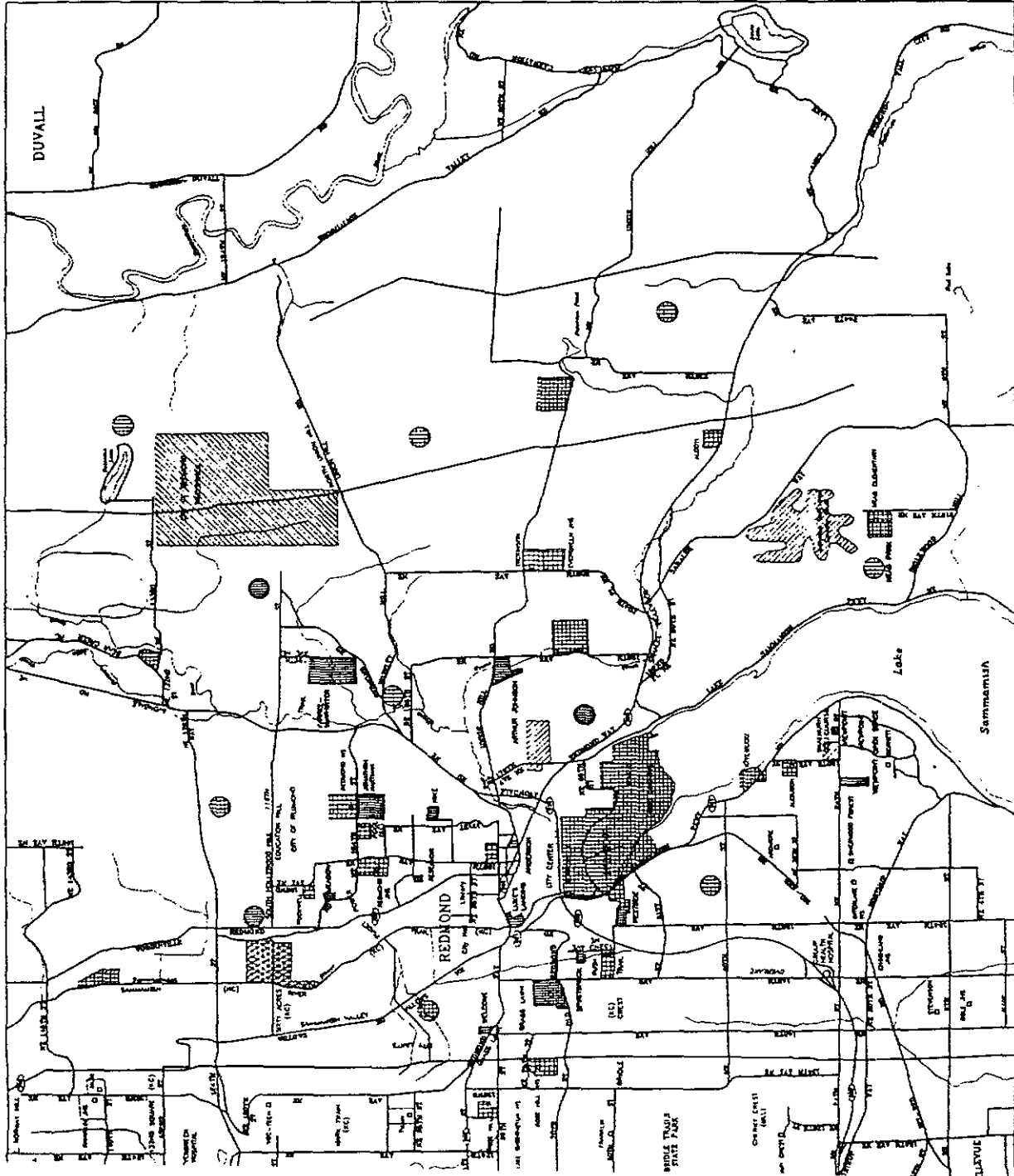
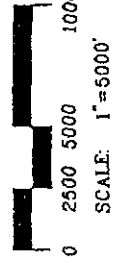
<i>Park Name</i>	<i>Location</i>	<i>Acreage</i>	<i>Type</i>	<i>Status</i>
<i>Municipal Campus</i>	<i>City Hall/N.E. 85th</i>	<i>10.5</i>	<i>Open Space</i>	<i>Improved</i>
<i>Redmond Elementary School</i>	<i>166th Ave N.E./ N.E. 80th Street</i>	<i>11.8</i>	<i>Neighborhood</i>	<i>Improved</i>
<i>Albert Anderson</i>	<i>168th Avenue N.E./ Redmond Way</i>	<i>2.8</i>	<i>Neighborhood</i>	<i>Improved</i>
<i>Luke McRedmond Landing</i>	<i>Redmond Way/159th</i>	<i>2.5</i>	<i>Resource Park</i>	<i>Improved</i>
<i>Sammamish River Trail</i>	<i>Sammamish River</i>	<i>18.8</i>	<i>Multi-Use Trail</i>	<i>Improved</i>
<i>Bear Creek Park</i>	<i>Bear Creek/ Union Hill Road</i>	<i>4.5</i>	<i>Resource Park</i>	<i>Partially Improved</i>
<i>Vista View Point</i>	<i>Between N.E. 85th/ 169th Avenue N.E.</i>	<i>(.5)</i>	<i>Bike/Pedestrian Trail</i>	<i>(Future)</i>
<i>O'Leary</i>	<i>Leary Way/ Redmond Way</i>	<i>.12</i>	<i>Beautification Area</i>	<i>Improved</i>
<i>Flag Pole Plaza</i>	<i>Redmond Way/ Leary Way</i>	<i>.12</i>	<i>Beautification Area/Artwork</i>	<i>Improved</i>
<i>Anderson Park Triangle</i>	<i>Avondale Road/ Redmond Way</i>	<i>.18</i>	<i>Beautification Area</i>	<i>Improved</i>
<i>Golf Course Open Space</i>	<i>Old Redmond Golf Course/Leary Way/ Bear Creek</i>	<i>(60)</i>	<i>Open Space</i>	<i>(Future)</i>
<i>Trestle</i>	<i>Both Sides of Sammamish River Between Redmond Way/Railroad Bridge</i>	<i>(2-4)</i>	<i>Passive Garden/ Open Space</i>	<i>(Future)</i>
<i>River Point Park</i>	<i>North of N.E. 90th</i>	<i>(7)</i>	<i>Neighborhood</i>	<i>(Future)</i>
<i>Downtown Park CENTRAL</i>	<i>N.E. 83rd Street/ 161st Avenue N.E.</i>	<i>1.4 existing (2-5)</i>	<i>Open Space Urban Plaza / Park</i>	<i>Unimproved (FUTURE)</i>
<i>Central Park</i>	<i>Between 158th Ave. and 160th Ave.</i>	<i>(4-7)</i>	<i>Multi-use Plaza/ Urban Park</i>	<i>(Future)</i>

PARKS, RECREATION AND OPEN SPACE PLAN

PLAN MAP
FIGURE 1

LEGEND

-  CITY OF REDMOND PARKS
-  KING COUNTY PARKS
-  LAKE WASHINGTON SCHOOL DISTRICT SCHOOLS
-  REDMOND WATERSHED
-  PRIVATE
-  PROPOSED NEIGHBORHOOD PARKS
-  PROPOSED COMMUNITY PARKS
-  QUASI-PUBLIC

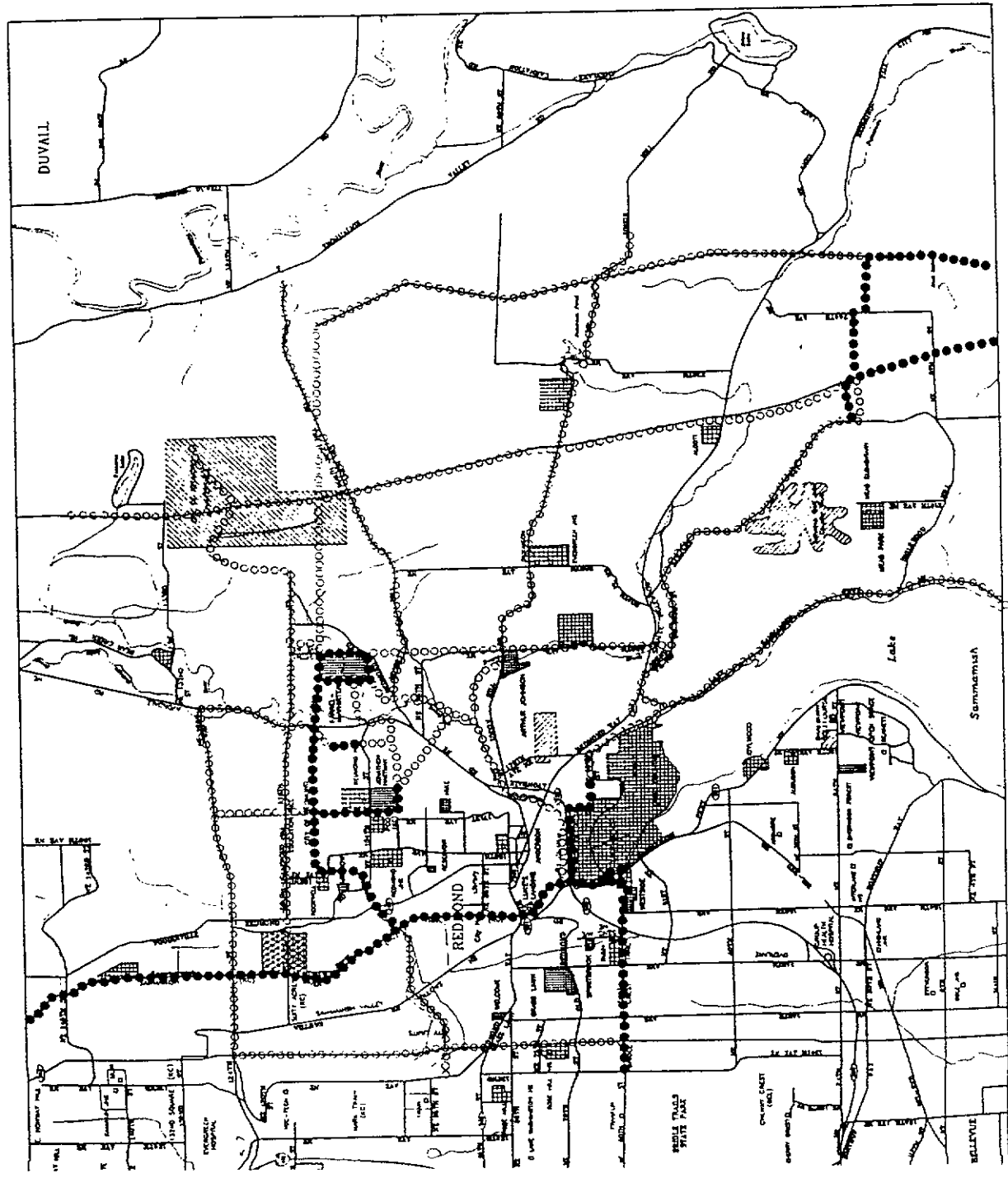
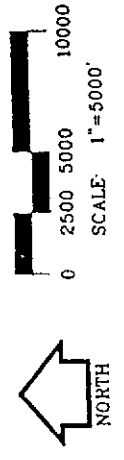


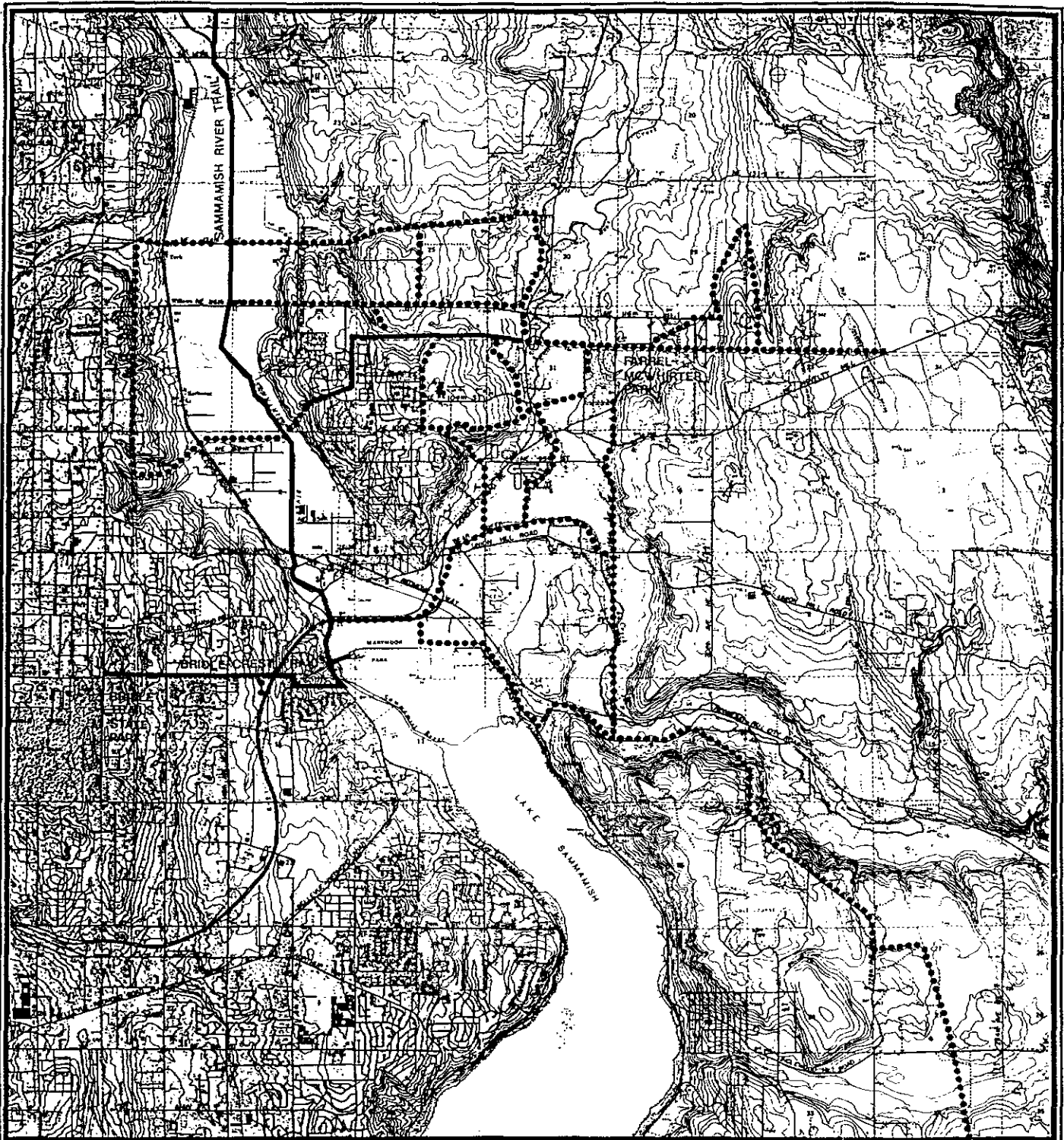
PARKS, RECREATION, AND OPEN SPACE PLAN

TRAILS PLAN
FIGURE 2

LEGEND

- CITY OF REDMOND PARKS
- KING COUNTY PARKS
- LAKE WASHINGTON SCHOOL DISTRICT SCHOOLS
- REDMOND WATERSHED
- PRIVATE
- EXISTING TRAILS
- PROPOSED TRAILS
- QUASI-PUBLIC





EQUESTRIAN TRAIL PLAN

- existing trail
- proposed trail
- park with equestrian activity area



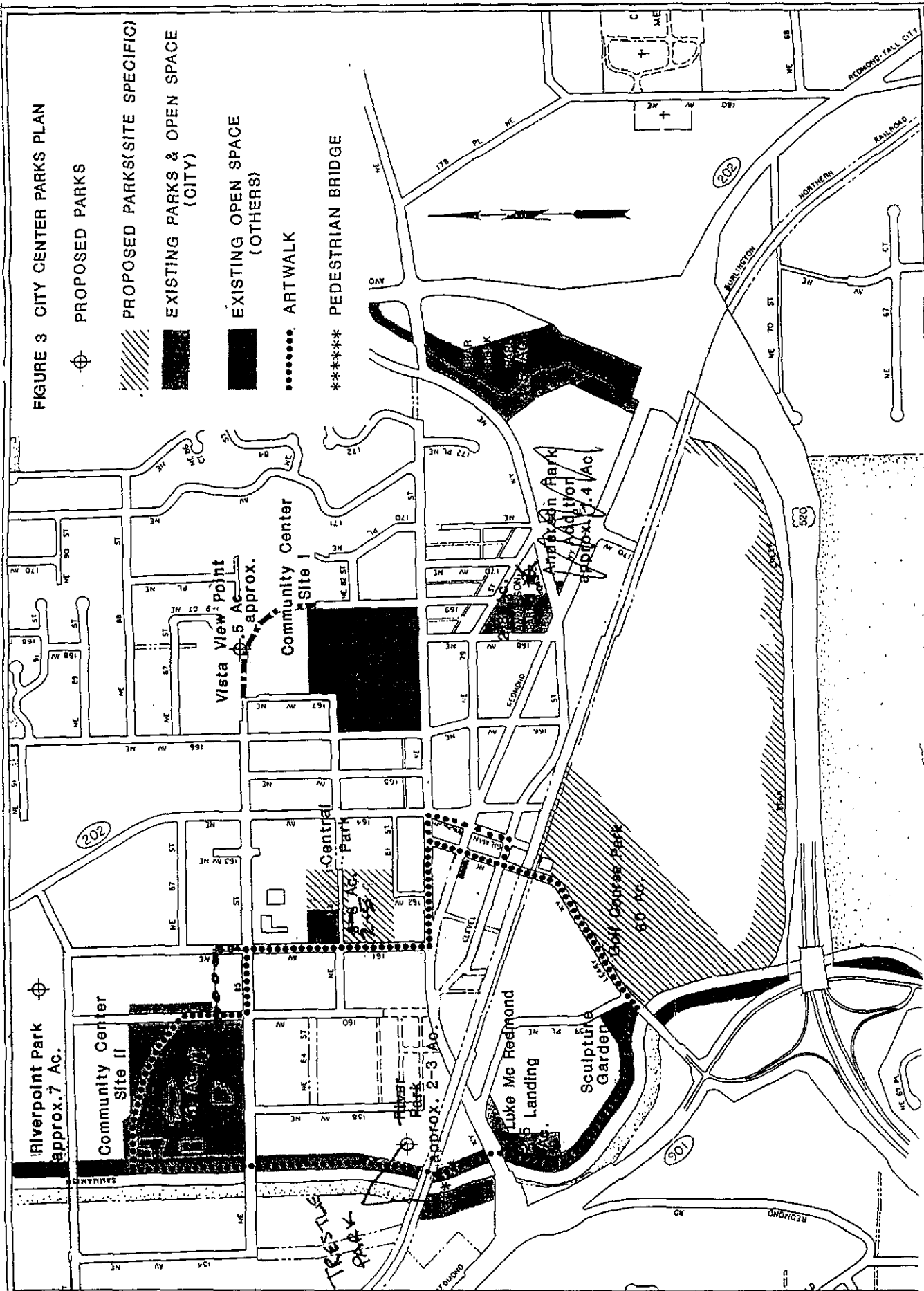
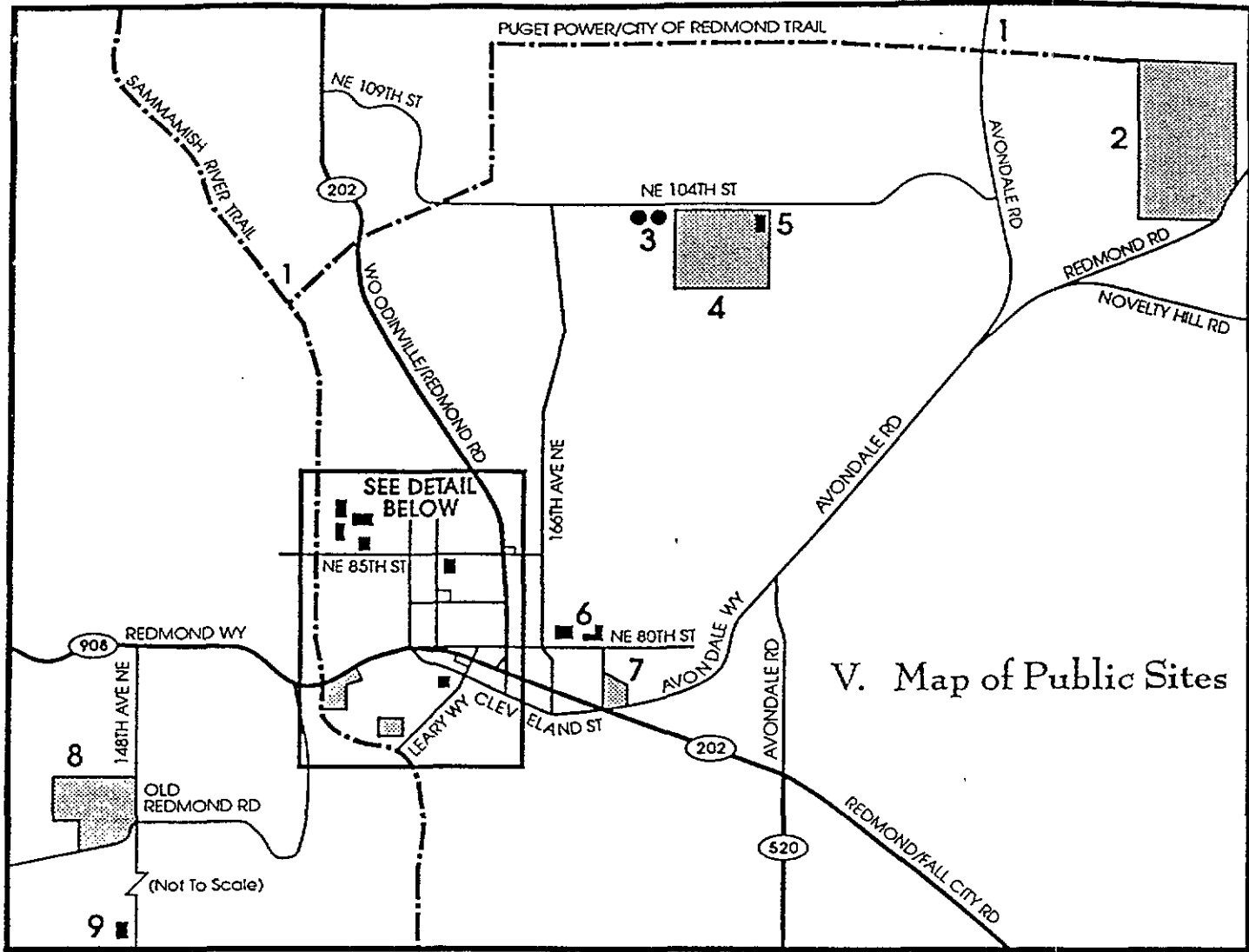
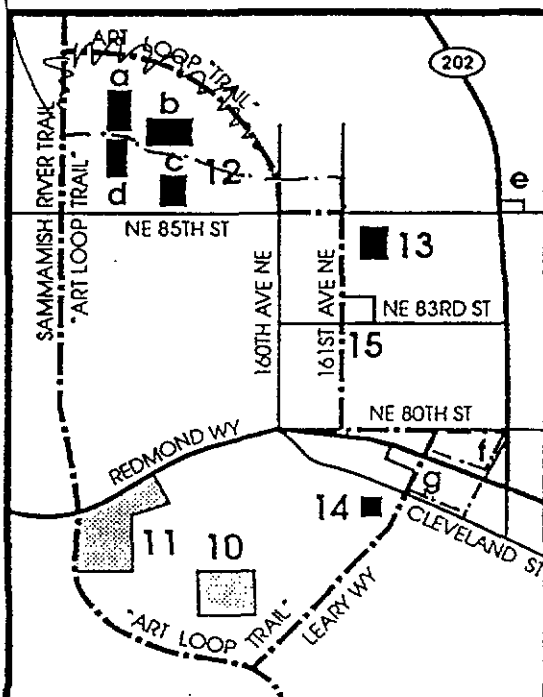


FIGURE 4 ART LOOP TRAIL



Revised

DOWNTOWN AREA DETAIL



ARTWORK SITE KEY

- | | |
|--|-------------------------------------|
| 1 Puget Power/City of Redmond Trail Heads | 12 Redmond Municipal Campus/Commons |
| 2 Farrel - McWhirter Park | a Senior Center |
| 3 Education Hill Water Towers | b Public Safety Building |
| 4 Hartman Park | c Library (King County) |
| 5 Redmond Pool (King County) | d City Hall |
| 6 Redmond Elementary School (Lake Washington School Dist.) | 13 Fire Station 11 |
| 7 Anderson Park | 14 King County Shop Site |
| 8 Grasslawn Park | 15 83rd Street Open Space |
| 9 Fire Station 12 | Open Spaces |
| 10 Slough House (King County) | e 164th Ave NE and NE 85th Street |
| 11 Luke McRedmond Landing | f 164th Ave NE and Redmond Way |
| | g Leary Way and Redmond Way |

LEGEND

■ Building

--- Art Loop Trail

20F.20

4/20/92

AMEND SECTIONS 20F.20.160 AND 20F.20.170
TO INCLUDE REFERENCES TO GENERAL AND
SPECIAL DEVELOPMENT PERMITS FOR SOME
CITY CENTER LAND USES.

20F.20.160 SPECIAL DEVELOPMENT
PERMIT

20.160(05) Purpose - This section defines the procedure for considering special development permits and applying standards defined in the guidelines for special development permits.

20.160(10) Scope - The procedure in this section shall be used for planned unit developments and special uses. Special uses can be projects or non-projects as identified in Section 20C.10.240, "Permitted Land Uses", and Section 20C.10.230(10) ⁰⁵ "City Center Permitted Land Uses". The scope of review and approval is governed by Section 20C.20.235, Special Developments. Special review criteria are provided for in Section 20C.20.235(70) for some specific uses.

In addition to PUD's and special uses identified in the Permitted Land Use Chart and the City Center Permitted Land Uses Chart, the Technical Committee may require a Special Development permit when a permitted use by nature of its scale or type may create significant impacts such as increased traffic, loss of significant vegetation, nuisances, severe disruption of storm drainage and water quality, large increments of population, or other impacts identified by the Technical Committee in the environmental review process.

20.160(15) Procedure - Applications for Special Development Permits shall follow the procedures defined in Section 20F.20.030, Development Permit Procedures.

20F.20.170 GENERAL DEVELOPMENT
PERMIT

20.170(05) Purpose - This section defines the procedure for considering General Development permits and provides for the administrative staff approval for certain actions.

20.170(10) Scope - The General Development Permit procedures shall apply where required by the Development Guide, by Section 20C.10.240, "Permitted Uses", Section 20C.10.230(10), "City

Center Permitted Land Uses", and for short subdivisions and shoreline development approval.

20.170(15) Optional Hearing - When the Technical Committee determines that a public hearing is necessary because of the nature of the application, the Hearing Examiner shall hold the hearing and take final action on the application.

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INCLUSIONARY REQUIREMENTS FOR AFFORDABLE HOUSING

In order to institute affordable housing regulations in the City Center Neighborhood, the following regulations are proposed to be included in Section 20C.20. These are written to be easily amended to include other areas of the City. That is why it is proposed to be included in the General Section, and not in the City Center sections of the code.

20C.20.016 Affordable Housing

(A) Purpose: The purpose of this Section is to implement through regulations the responsibility of the City under State law to provide for housing opportunities for all economic segments of the community, and to assure that the impacts of new development will be mitigated to the extent feasible to assure an adequate affordable housing supply in the City.

(B) General: This Section applies to: all new residential development (Paragraph i); all new subdivisions and condominiums (Paragraph ii); and all new senior housing development (Paragraph iii) within the City Center Neighborhood. In areas where density limitation is expressed as a floor area ratio (FAR), density bonuses will be calculated as an equivalent FAR bonus.

(i) Multifamily Development: At least 10% of the units in all new multifamily development proposals of ten units or greater must be affordable units. In addition, at least one bonus market rate unit is permitted for each affordable unit provided, up to 15% above the maximum density permitted on the site. For example, if the maximum allowable density for the site is twenty units per acre, the density bonus shall not exceed three units per acre, yielding a total allowable density, with bonus, of 23 units per acre.

(ii) Subdivision and Condominium Development: At least 10% of the units in all new subdivision and condominium proposals of ten lots (or units) or greater must be affordable units. In addition, at least one bonus market rate unit or lot is permitted for each affordable unit provided, up to 15% above the maximum density permitted on the site.

(iii) Senior Housing: At least 10% of the units in all new senior housing development

proposals of ten units or greater, including senior citizen dwellings and congregate care senior dwellings, but not including nursing homes, must be affordable units. In addition, at least one bonus market rate unit is permitted for each affordable unit provided, up to 15% above the maximum density permitted on the site. The bonuses granted under this provision are in addition to any bonus granted for senior housing under Section 20C.20.235(70)(c) Senior Citizen Housing Developments - Bonus Density.

(iv) Affordable Housing Calculation: The number of required affordable housing units is determined by rounding fractional numbers up to the nearest whole number from .5. The project proponent may propose to provide alternative payments for fractional portions of units, as provided for in Paragraph (C)(ii)(b).

(v) Duration: An agreement in a form approved by the City must be recorded with King County Department of Records and Elections to stipulate conditions under which required affordable housing units will remain as affordable housing for the life of the project. This agreement shall be a covenant running with the land, binding on the assigns, heirs, and successors of the applicant. Prior to the issuance of any building permit, the owner shall sign any necessary agreements with the City to implement these requirements. The City may agree, at its sole discretion, to subordinate any affordable housing regulatory agreement for the purpose of allowing the owner to obtain financing for development of the property, consistent with any applicable provision of the Community Development Guide in effect at the time of the issuance of the development permit(s).

(vi) Low cost units: Each low cost affordable unit provided counts as two affordable units for the purpose of satisfying the affordable unit requirement under paragraphs i, ii and iii. For purposes of computing bonus market rate units under paragraphs i, ii and iii, two bonus market rate units are permitted for each low cost affordable unit provided, up to 20% above the maximum density permitted on the site.

(vii) City Center Neighborhood. The definitions of and requirements for affordable housing for projects in the City Center Neighborhood shall be as provided in the following table. This paragraph ,vii, shall apply to those projects which

meet the affordability requirements on-site or off-site, but within the boundaries of the City Center Neighborhood, but shall not apply to those projects which elect to use an alternate payment method as authorized in Paragraph (C)(ii)(b).

The affordable housing requirements for projects vested on or after the effective date of this ordinance must be targeted for households whose incomes do not exceed the following:

Number of Total Units Median Income Level

First 250 units	Inclusionary requirements optional
Second 250 units	90% of median income
All subsequent units	80% of median income

Number of Total Units shall mean the total number of housing units (affordable and otherwise) permitted to be constructed within the City Center Neighborhood and to which this Paragraph, vii, shall apply. In establishing an affordable rent or sales price, Median Income Level shall be adjusted for household size, as determined by the United States department of Housing and Urban Development for the Seattle MSA, and shall assure that no more than thirty percent of household income is used for housing expenses.

(C) Alternative Compliance Methods

(i) General: The Technical Committee may approve a request for satisfying all or part of the affordable housing requirements with alternative compliance methods proposed by the applicant, if they meet the intent of this Affordable Housing Section.

(ii) Alternatives: The project proponent may propose one or more of the following alternatives, and must demonstrate that any alternative compliance method achieves a result equal to or better than providing affordable housing on-site. Housing units provided through the alternative compliance method must be based on providing the same type of units as the units in the project which gives rise to the requirement.

a. Affordable housing units may be provided off-site provided the location chosen does not lead to undue concentration of affordable

housing in any particular area of the City and the site is within close proximity to employment opportunities and transit services..

b. Cash payments in-lieu of providing actual housing units may be provided. The formula for alternative payments will be established by an Administrative Order. The payment obligation will be established at the time of issuance of building permits or preliminary plat approval for the project.

c. The Technical Committee will consider other options for satisfying the affordable housing requirements, as proposed by the project proponent.

ADD THE FOLLOWING DEFINITIONS

20A.060.145 Affordable Housing "Affordable Housing" means:

- A. Housing renting for a monthly rent of not more than thirty percent of the total monthly household income of low income households (defined to be a household earning less than eighty percent of the median annual income, adjusted for household size, as determined by the United States Department of Housing and Urban Development for the Seattle Metropolitan Statistical Area); or
- B. Housing that may be purchased with monthly payments including: principal, interest, taxes, insurance, homeowners association fees, and assessments, that do not add up to more than thirty percent of the total monthly household income of low income households (defined to be a household earning less than eighty percent of the median annual income, adjusted for household size, as determined by the United States Department of Housing and Urban Development for the Seattle Metropolitan Statistical Area).

20A.060.146 Affordable Housing, Low Cost "Low cost affordable housing" means:

- A. Housing renting for a monthly rent of not more than thirty percent of the total monthly household income of very low income households (defined to be a household earning less than fifty percent of the median annual income, adjusted for household size, as determined by the United States Department of Housing and Urban Development for the Seattle Metropolitan Statistical Area); or
- B. Housing that may be purchased with monthly payments including; principal, interest, taxes, insurance, homeowners association fees, and assessments, that do not add up to more than thirty percent of the total monthly household income of very low income households (defined to be a household earning less than fifty percent of the median annual income, adjusted for household size, as determined by the United States Department of Housing and Urban Development for the Seattle Metropolitan Statistical Area).

20A.060.356 Household is one or more adults and their dependents who will be residing in the affordable unit. For ownership units title will be held in the name of all adults, other than specifically identified dependents.

20A.060.357 Household Income shall include all income from all household members over the age of 18 residing in the household. Household income consists of all income that would be included as income for Federal income tax purposes (e.g. wages, interest income, etc.) for household members over the age of 18. Income of dependents who reside within a household for less than 3 months of the year will not be counted toward household income.