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Rev. 06/28/95

DGA94-004
Redmond Town Center

ORIGINAL

ORDINANCE NO. 1841

AN ORDINANCE OF THE CITY OF REDMOND, WASHINGTON, AMENDING CERTAIN SECTIONS OF CHAPTERS 20B.60, 20B.70, 20B.85, 20C.10, 20C.20 AND 20C.30 OF THE REDMOND MUNICIPAL CODE AND COMMUNITY DEVELOPMENT GUIDE IN ORDER TO AMEND GOALS, POLICIES, PLANS AND REGULATIONS FOR A MIXED USE COMPLEX ON CERTAIN PROPERTY COMMONLY KNOWN AS REDMOND TOWN CENTER, DGA 94-004.

WHEREAS, the property commonly known as the Redmond Town Center site, consisting of the former Redmond Golf Links property and the Butler-Walls property, was annexed to the City of Redmond in 1988, and

WHEREAS, in conjunction with said annexation, the City established a Mixed Use/Shopping Center zoning classification for the property, which authorized the development of an enclosed regional shopping mall subject to an approved master plan and certain terms and conditions, and

WHEREAS, the property owners have applied to the City to alter the zoning designation, master plan and conditions in order to enable development of a 1.375 million square feet open air mixed use complex, and

WHEREAS, an addendum to prior environmental impact statements relating to the property and to various land use actions related to the proposal was issued on April 11, 1995, and

WHEREAS, between April 12, 1995 and June 7, 1995, the

Redmond Planning Commission met eight times in public hearings and study sessions regarding the proposal and on June 7, 1995 voted to recommend approval of the proposal to the City Council, and

WHEREAS, the City Council held its own public hearing on the proposal commencing on June 26, 1995 and ending on June 27, 1995, and at the conclusion of said hearing, determined to approve those amendments to the zoning regulations regarding the property hereinafter set forth, NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF REDMOND, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. Arterial Street Plan Map Amended. The City Center Arterial Street Plan Map adopted by subsection 20B.60.060(05) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as set forth on Attachment 1A attached hereto and incorporated herein by this reference as if set forth in full.

Section 2. Functional Arterial Classification Summary. The Functional Arterial Classification Summary for the Long Range Transportation Plan adopted by subsection 20B.60.060(05) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as set forth on Attachment 2A attached hereto and incorporated herein by this reference as if set forth in full.

Section 3. City Center Bike Plan. The City Center Bike Plan Map adopted by subsection 20B.60.060(10) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as set forth on Attachment 3A attached hereto and incorporated

herein by this reference as if set forth in full.

Section 4. Sewer Facility Plan. A new subsection 20B.70.085(05) is hereby added to the Redmond Municipal Code and Community Development Guide in order to adopt the Redmond Town Center Sanitary Sewer Basin Study prepared by KPFF Consulting Engineers dated March 30, 1995 and to read as set forth on Exhibit A attached hereto and incorporated herein by this reference as if set forth in full.

Section 5. Mixed-Use Center Design Area Goals and Policies. Subsections 20B.85.130(75)-(90) of the Redmond Municipal Code and Community Development Guide are hereby amended to provide goals and policies for the Mixed-Use Center Design Area and to read as set forth on Exhibit A attached hereto and incorporated herein by this reference as if set forth in full. Attachments 4A and 5A referred to on said Exhibit are also adopted, attached hereto and incorporated herein by this reference as if set forth in full.

Section 6. City Center Permitted Land Uses. Footnote 5 of the City Center Permitted Land Uses Chart adopted by subsection 20C.10.230(05) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as set forth on Exhibit B attached hereto and incorporated herein by this reference as if set forth in full.

Section 7. City Center Site Requirements. The City Center Site Requirements Chart adopted by subsection 20C.10.230(16) of the Redmond Municipal Code and Community Development Guide is hereby amended to delete the language found in the box formed by

the intersection of the column labeled "Mixed Use Center" and the row labeled "Maximum Building Height (Stories/Feet)" and to insert in its place the language set forth on Exhibit B attached hereto and incorporated herein by this reference as if set forth in full. Footnote 8 to the City Center Site Requirements Chart is also amended to read as set forth on Exhibit B.

Section 8. City Center Pedestrian System. The City Center Pedestrian System Map adopted by subsection 20C.10.230(25)(a) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as set forth on Attachment 2B attached hereto and incorporated herein by this reference as if set forth in full.

Section 9. Sign Requirements. The Sign Requirements Per Zoning District Chart adopted by subsection 20C.20.230(30) of the Redmond Municipal Code and Community Development Guide is hereby amended to remove the language found in the box formed by the intersection of the second column (labeled "CC-1; CC-2 . . . ") and the row labeled "Maximum Size Per Sign Face (square feet)" and substitute in its place the language set forth on Exhibit B attached hereto and incorporated herein by this reference as if set forth in full.

Section 10. Mixed-Use Center Design Area Requirements. Subsection 20C.30.050(30) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as set forth on Exhibit B attached hereto and incorporated herein by this reference as if set forth in full, and Figures 1-10 attached hereto

and incorporated herein by this reference as if set forth in full are hereby adopted and incorporated into subsection 20C.30.050(30).

Section 11. Effect of Amendments on Existing Zoning and Master Plan. It is the intent of the City Council in adopting the amendments set forth in this ordinance to modify the zoning regulations applicable to the Butler-Walls and Redmond Town center properties established by Ordinances 1416 and 1551 so that the above amendments, and other applicable sections of the Redmond Municipal Code and community Development Guide will control development on the site, and so that the master plan and conditions of approval adopted by Ordinances 1416 and 1551 shall no longer control.

Section 12. Severability. If any section, sentence, clause or phrase of this ordinance should be held to be invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity or constitutionality of any other section, sentence, clause or phrase of this ordinance.

Section 13. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum, and shall take effect five (5) days after passage and publication of an approved summary thereof consisting of the title.

CITY OF REDMOND



MAYOR ROSEMARIE IVES

ATTEST/AUTHENTICATED:



CITY CLERK, BONNIE MATISON

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY: 

FILED WITH THE CITY CLERK: 06/22/95
PASSED BY THE CITY COUNCIL: 06/27/95
SIGNED BY THE MAYOR: 07/06/95
PUBLISHED: 07/12/95
EFFECTIVE DATE: 07/17/95
ORDINANCE NO. 1841

EXHIBIT A

PLANNING COMMISSION RECOMMENDED DEVELOPMENT GUIDE AMENDMENTS FOR REDMOND TOWN CENTER - DGA94-004

SECTION 20B.

RCDG Section 20B.60

The following language would amend the Transportation Plan:

Section 20B.60.060(05), Arterial Street Plan. The map entitled "Redmond City Center Arterial Street Plan" shall be replaced with a new map entitled "City Center Arterial Street Plan" attached hereto as Attachment 1A and incorporated herein by this reference.

Section 20B.60.060(05), Arterial Street Plan, page B-48e, shall be modified by incorporating the modifications to the "Functional Arterial Classification Summary for Long Range Transportation Plan" attached hereto as Attachment 2A and incorporated herein by this reference.

Section B.60.060(10), Bike Plan, shall be modified by substituting the new map entitled "City Center Bike Plan" attached hereto as Attachment 3A and incorporated herein by this reference, for the existing map entitled "Redmond City Center Bike Plan."

RCDG Section 20B.70

The following language would be added to the Sewer Facility Plan:

Section 20B.70.085(05)
The Redmond Town Center Sanitary Sewer Basin Study prepared by KPFF Consulting Engineers dated March 30, 1995 identifies proposed sewer system improvements including gravity collector and lateral sewers and a sanitary sewer pump station for an area generally enclosed by the Burlington Northern Railroad tracks on the north, Bear Creek on the east and south, the Sammamish River on the west, and Leary Way on the northwest. The recommended improvements within this report are hereby incorporated into the City Sewer Facility Plan.

RCDG Section 20B.85

The following section replaces 20B.85.130(75)-(90).

20B.85.130(75) Mixed-Use Center Design Area Goal - Encourage the development of a mixed-use retail, office, and residential complex that will provide a focus for the existing downtown and contribute to the social and economic base of the City.

(a) **Policy** - The development should include a mix of uses such as residential, office, hotel, cinema, open space and recreational activities along with a mixed-use center.

(b) **Policy** - This Design Area shall have a maximum commercial building area of not more than 1,375,000 square feet of gross leasable area (GLA) including a mixed-use retail area of 600,000 to 745,000 square feet of GLA, an office park of up to 430,000 square feet of GLA, and other retail up to 200,000 square feet of GLA as shown in Attachment 4A.

(c) **Policy** - This Design Area shall have a minimum of 600,000 square feet of Gross Leasable Area dedicated to retail uses at build-out.

(d) **Policy** - Retail uses should be directly linked visually and functionally to other retail uses in the City Center.

(e) **Policy** - Encourage development of a compact center which minimizes use of land area and generates a high level of pedestrian activity between the center and existing downtown.

(f) **Policy** - To minimize visual impacts, structured parking shall be incorporated into the development.

(g) **Policy** - Provide for circulation, land use and parking linkages with the existing downtown which will attract or encourage shoppers into the downtown.

(h) **Policy** - The Justice White House, the Saturday Market and other features of community and historic significance should be preserved.

(i) **Policy** - Assure that the size and scale of the center are compatible with the scale of the City Center and that economic activities are interrelated.

20B.85.130(80) Mixed-Use Center Design Area Goal - Provide transportation facilities that will maintain acceptable levels of mobility, minimize impacts on the existing system from new development, integrate the area with the City Center, and improve traffic circulation.

(a) **Policy** - Traffic related improvements which are consistent with Section 20B.60.030(05) shall be made to provide access to the project site and meet City Level of Service standards (LOS). Improvements are required at specific intersections in accordance with City standards. The project will contribute, on a pro-rata basis, toward short and long term street improvement projects planned by the City.

(b) **Policy** - Provide a continuous pedestrian/bicycle transportation system constructed through the design area which is linked with the regional trail system, downtown, and Marymoor Park.

(c) **Policy** - Incorporate a transportation program for the design area which addresses bicycle, pedestrian, truck and automobile circulation, including construction traffic.

(d) **Policy** - Provide a Transportation Demand Management Program that is consistent and complies with State mandated standards.

(e) **Policy** - Provide an on-site street design that is similar to adopted pedestrian linkage system requirements and is pedestrian oriented, contains visual and functional sidewalk amenities, street trees, and has

distinctive pedestrian crossings while maintaining functional on-site traffic circulation.

20B.85.130(85) - Mixed-Use Center Design Area Goal - Assure that a site plan retains the site's significant natural and aesthetic features and protects the natural environment.

(a) Policy - Retain indigenous vegetation, particularly adjacent to Bear Creek and the Sammamish River.

(b) Policy - Preserve existing natural features, particularly healthy mature trees and stream courses.

(c) Policy - Assure that significant areas of natural open space (approximately 44 acres of the site) are provided to protect and maintain the sensitive natural systems along Bear Creek and the Sammamish River. This natural area shall be preserved by easement to the City that would permanently assure the open space on the site.

(d) Policy - Assure that a monitored and maintained storm water disposal system adequately controls runoff, and removes pollutants prior to discharge, consistent with requirements of Chapter 20E.75 of the

Redmond Community Development Guide - "Storm Water Management."

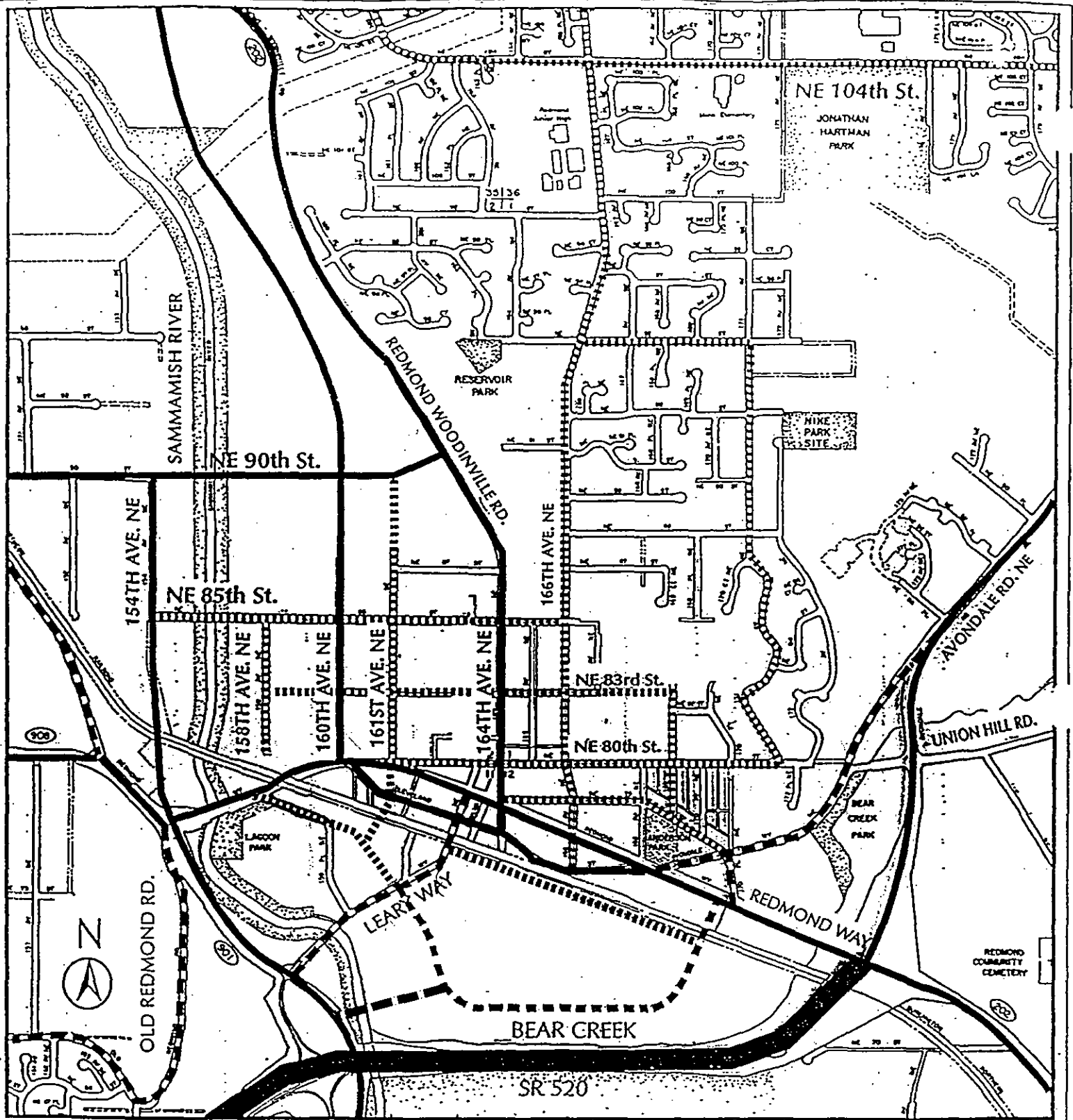
20B.85.130(90) Mixed-Use Center Design Area Goal - Create opportunities for recreation and leisure activities that complement other uses in the City Center and generate pedestrian activity.

(a) Policy - At least 44 acres shall be preserved by easement to the City or controlled by other methods that would permanently assure the open space on the site. This downtown Public Access Open Space (as shown on Attachment 5A) shall serve as visual amenities, passive recreational opportunities, open space plazas, and natural areas.





(b) Policy - Encourage development of plazas, pedestrian malls and other amenity open spaces that promote outdoor activity and encourage pedestrian and bicycle circulation between the mixed-use center and the balance of the City Center area.

(e) Policy - Encourage residential development on the Mixed-Use Center site by providing for housing square footage in addition to the maximum commercial building area allowed.

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REDMOND CITY CENTER ARTERIAL STREET PLAN

-  FREEWAY
-  PRINCIPAL ARTERIAL
-  MINOR ARTERIAL
-  COLLECTOR ARTERIAL

Functional Arterial Classification Summary for the Long Range Transportation Plan						
STREET	TRAFFIC LANES*		SIDEWALKS		BICYCLE FACILITIES**	
	BUILT	PLANNED	BUILT	PLANNED	BUILT	PLANNED
PRINCIPAL ARTERIALS:						
AVONDALE RD. N.E.	2	4	0	2	1	2
REDMOND WAY (except couplet)	4/5	4/5	0-2	2	0	0
REDMOND WAY (couplet)	2-3	2-3	2	2	0	2
CLEVELAND ST. (couplet)	2-3	2-3	2	2	0	0
REDMOND-WOODINVILLE RD/ 164TH AVE. N.E. - north of NE 90TH ST.	2-4	2-4	0-2	1	0	1
REDMOND-WOODINVILLE RD/ 164TH AVE. N.E. - south of NE 90TH ST.	2-4	3-4	0-2	2	0	2
SR 520	2-4	4	0	0	2	2
SR 901 EXTENSION/W. LK. SAMMAMISH PKWY. N.E. EXTENSION	0	4/5	0	0	0	0
W. LK. SAMMAMISH PKWY. N.E. (BELLVUE-REDMOND RD. TO REDMOND WAY)	2	4	0	2	0-2	2
N.E. 24TH ST. (148TH TO 156TH AVE. N.E.)	4	4	2	2	0	0
N.E. 90TH ST. (WILLOWS RD. TO SR 202)	0-2	3-4	0	2	0	2
N.E. 124TH ST.	2	2-4	0	0-2	0	2
148TH AVE. N.E.	4	4	2	2	0	0
148TH AVE. N.E. EXTENSION	0	4	0	2	0	0
160TH AVE. N.E.	2	2-4	0	2	0	0
MINOR ARTERIALS:						
BELLEVUE-REDMOND RD.	2-4	4	0-2	2	0	2
E. LK. SAMMAMISH PKWY. N.E.	2	4	0	0	0	2
LEARY WAY N.E.	2	3	0-2	1-2	0	2
N.E. UNION HILL RD.	2-4	2-4	0-2	0-2	0	0-2
NOVELTY HILL RD.	2	2	0	0	0	1-2
OLD REDMOND RD.	2	2-4	0-2	2	0-2	1
W. LK. SAMMAMISH PKWY. N.E.	2	2	0	0	1	1-2
WILLOWS RD.	2	2	0-2	1-2	1-2	2
N.E. 24TH ST. (156TH TO W. LK. SAMMAMISH PKWY. N.E.)	2	2	2	2	1	2
N.E. 40TH ST.	2-4	2-4	0-2	2	0-2	2
N.E. 51ST ST.	2-4	2-4	1-2	2	0	0-2
N.E. 72ND ST.	0	4	0	1	0	0
N.E. 85TH ST.	4	3	1-2	2	2	2
132ND AVE. N.E.	2	2	0-2	1-2	0	1-2
140TH AVE. N.E.	2	2	0	2	0-1	1
156TH AVE. N.E. (N.E. 20TH to N.E. 51ST ST.)	4	4	2	2	2	2
170TH PL./AVE. N.E.	2	3-4	0-2	2	0	0
BEAR CREEK PARKWAY	0	4-5	0	1-2	0	1-2
162ND AVE. N.E.	0	3-5	0	1-2	0	0

(Continued):

STREET	TRAFFIC LANES*		SIDEWALKS		BICYCLE FACILITIES**	
	BUILT	PLANNED	BUILT	PLANNED	BUILT	PLANNED
COLLECTOR ARTERIALS:						
AVONDALE WAY N.E.	4	4	2	2	0	0
N.E. 20TH ST.	4	4	0-1	2	0	0
N.E. 80TH ST. (132ND TO 140TH AVE. N.E.)	2	2	1	2	1	1
N.E. 80TH ST. (REDMOND WAY TO 171ST AVE. N.E.)	2	2-4	0-2	2	0	2
N.E. 83RD ST.	0-2	2-3	0-2	2	0	2
N.E. 95TH ST.	2	2	1	2	0	1
N.E. 100TH ST.	2	2	2	2	0	2
N.E. 104TH ST.	2	2	2	2	1	1
N.E. 116TH ST.	2	2	0	1	0	2
156TH AVE. N.E. (N.E. 51ST ST. TO OLD REDMOND RD.)	2	2	0-2	2	0	1
158TH AVE. N.E. (N.E. 86TH ST. TO REDMOND WAY)	0-2	4/5	0-1	2	0	0
159TH AVE. N.E.	2	2	-	0-2	0	0
161ST AVE. N.E. (N.E. 80TH ST. TO CLEVELAND ST.)	0-4	3	0-1	2	0	2
166TH AVE. N.E.	2-4	2-4	2	2	0-2	0-2
169TH AVE. N.E.	2	2-3	0-2	2	0	2
171ST AVE. N.E.	2	2	1-2	1-2	1	1
172ND AVE. N.E.	2	2	2	2	1	1
180TH AVE. N.E. (SOUTH OF N.E. UNION HILL RD.)	2	2	0	2	2	2
164TH AVE. N.E. (CLEVELAND ST. TO RAILROAD ST.)	0	3-4	0	2	0	0
RAILROAD ST.	0	2	0	1	0	2

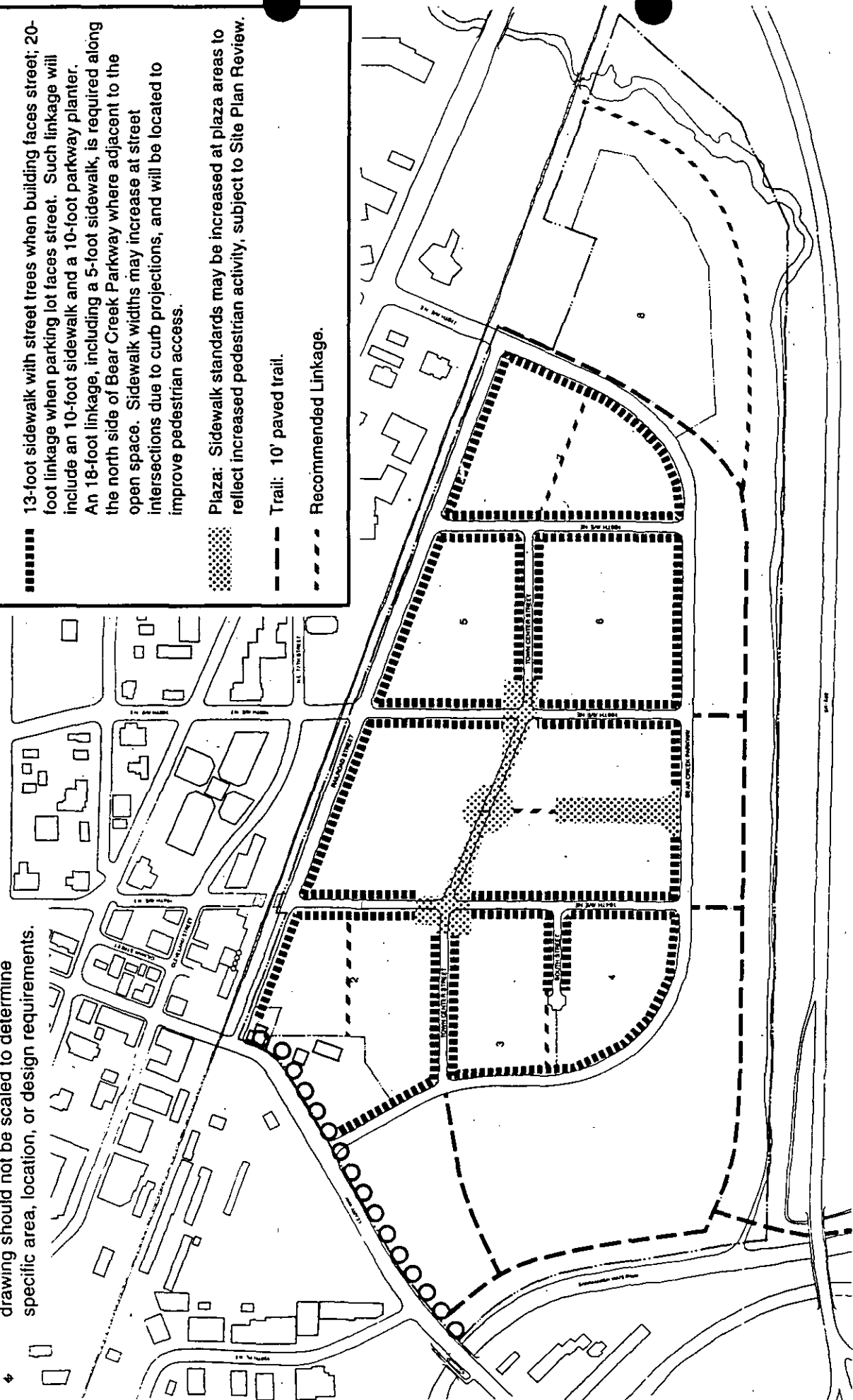
* May include left turn lanes or two-way left-turn lanes except as noted by 2/3 or 4/5.

** A Class I, II or III bicycle designation is planned on or near the arterial as indicated. See the bicycle plan maps.

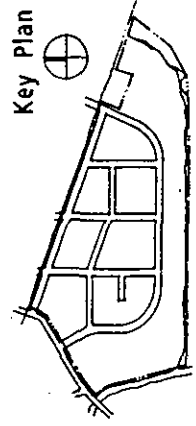
General Note: Information provided is intended for graphic reference only. This drawing should not be scaled to determine specific area, location, or design requirements.

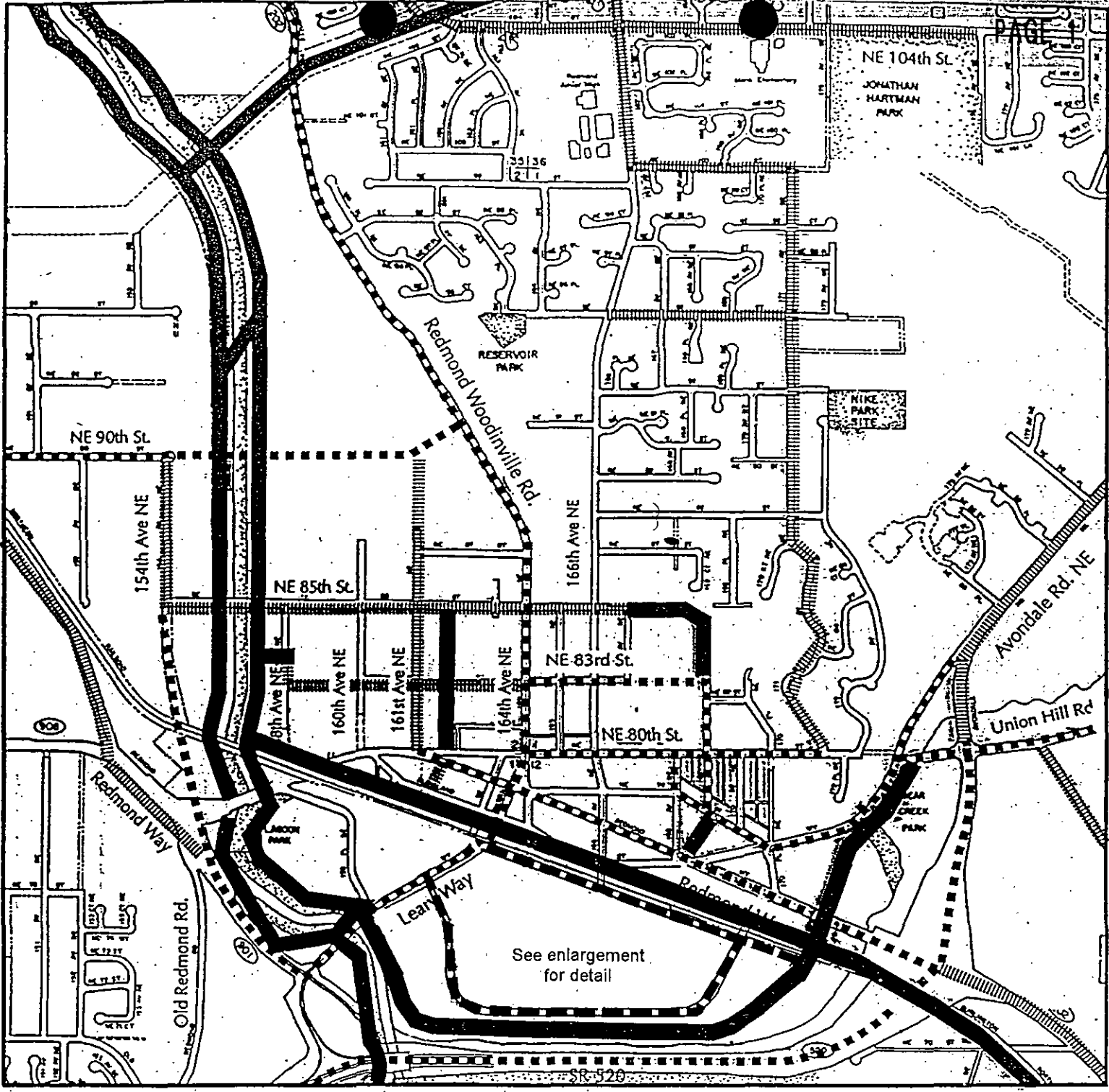
SIDEWALK STANDARDS:

- ○ ○ Type 1 Walkway (per RCDDG)
- ▬▬▬▬▬ 13-foot sidewalk with street trees when building faces street; 20-foot linkage when parking lot faces street. Such linkage will include an 10-foot sidewalk and a 10-foot parkway planter.
- ▬▬▬▬▬ An 18-foot linkage, including a 5-foot sidewalk, is required along the north side of Bear Creek Parkway where adjacent to the open space. Sidewalk widths may increase at street intersections due to curb projections, and will be located to improve pedestrian access.
- ▬▬▬▬▬ Plaza: Sidewalk standards may be increased at plaza areas to reflect increased pedestrian activity, subject to Site Plan Review.
- - - Trail: 10' paved trail.
- - - Recommended Linkage.






Pedestrian Circulation System - Required Locations and Standards





REDMOND CITY CENTER BICYCLE PLAN



- 
BIKEWAY (Class I): A bicycle facility physically separated from motorized vehicular traffic by an open space or barrier and either within road right-of-way or within an independent right-of-way. The minimum width when exclusively used by bicycles is 5 ft. for one-way travel and 8 ft. for two-way travel. The minimum width when shared with pedestrians is 12 ft.
- 
BICYCLE LANE (Class II): A portion of a roadway which has been designated by striping, signing and pavement markings for the preferential use of cyclists. Bicycle lanes are one-way facilities. The minimum width when abutting the curb is 5.5 feet. When adjacent to parking, the minimum width is 5 feet.
- 
BICYCLE ROUTE (Class III): A roadway shared by bicycles and motor vehicles with a wide shared curb lane or paved shoulder, and signage. The desirable width of a curb lane is 12 to 14 feet.

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REDMOND TOWN CENTER BICYCLE PLAN

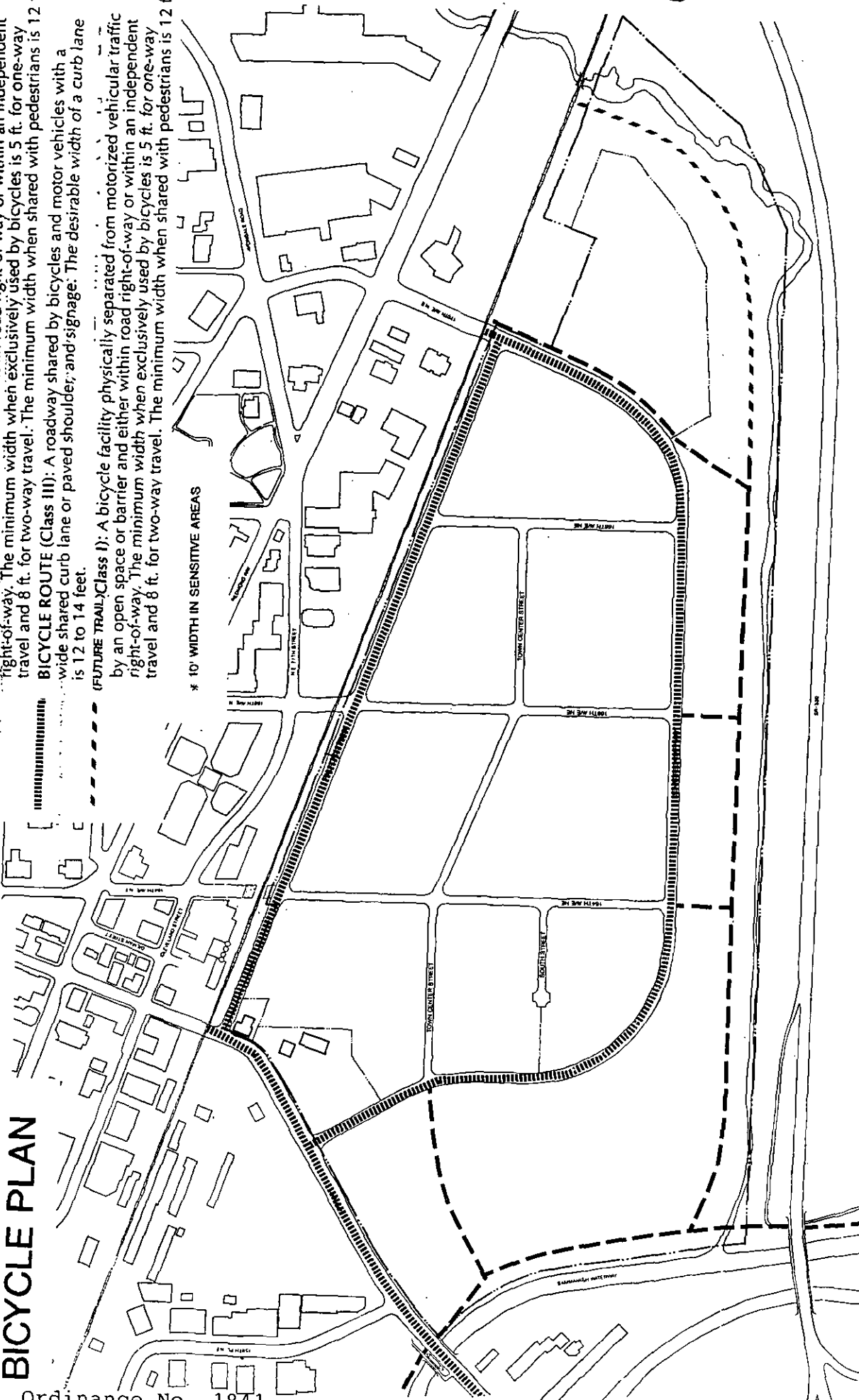
Ordinance No. 1841

BIKEWAY (Class I): A bicycle facility physically separated from motorized vehicular traffic by an open space or barrier and either within road right-of-way or within an independent right-of-way. The minimum width when exclusively used by bicycles is 5 ft. for one-way travel and 8 ft. for two-way travel. The minimum width when shared with pedestrians is 12 ft.*

BICYCLE ROUTE (Class III): A roadway shared by bicycles and motor vehicles with a wide shared curb lane or paved shoulder, and signage. The desirable width of a curb lane is 12 to 14 feet.

(FUTURE TRAIL) (Class I): A bicycle facility physically separated from motorized vehicular traffic by an open space or barrier and either within road right-of-way or within an independent right-of-way. The minimum width when exclusively used by bicycles is 5 ft. for one-way travel and 8 ft. for two-way travel. The minimum width when shared with pedestrians is 12 ft.*

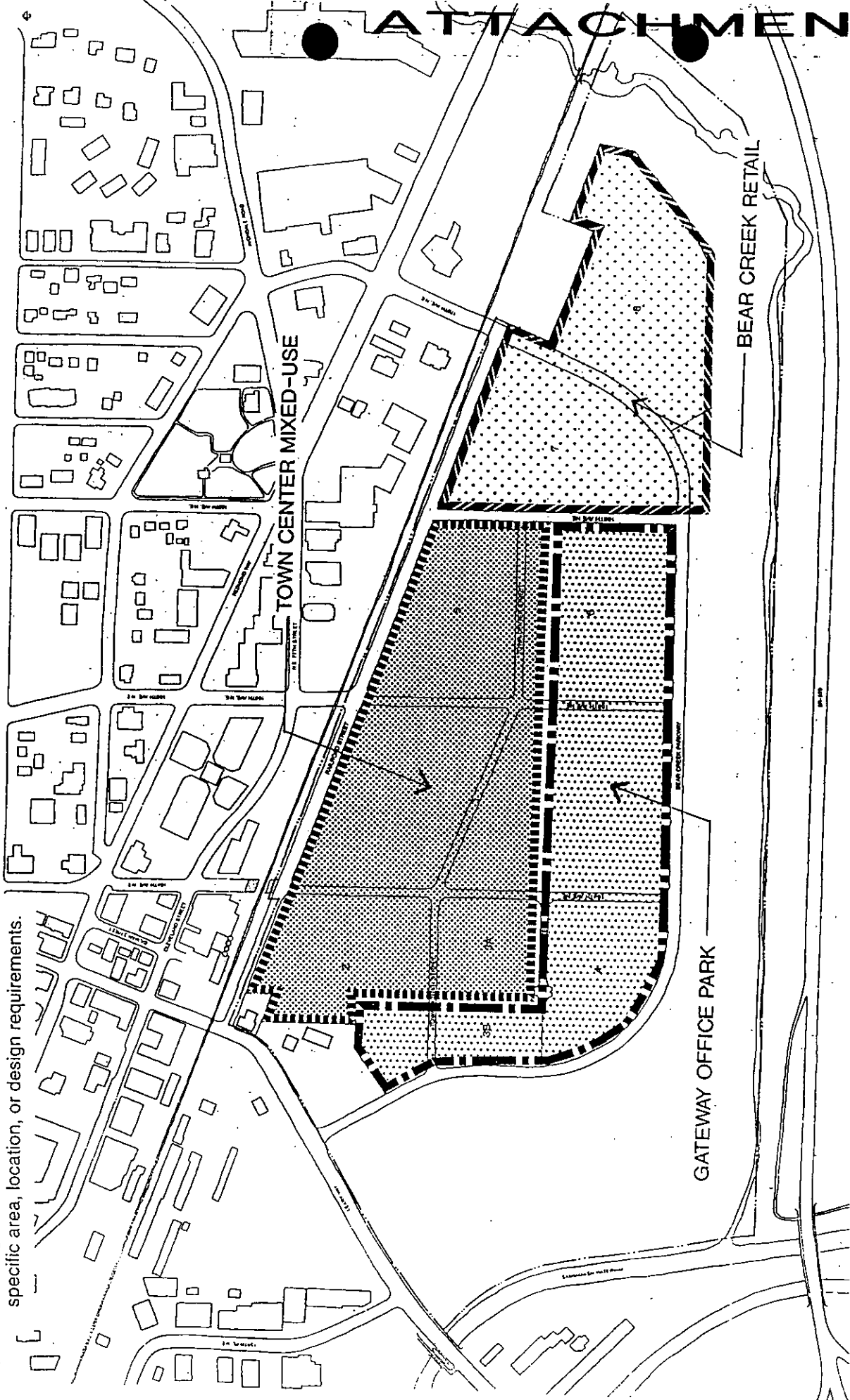
* 10' WIDTH IN SENSITIVE AREAS



REDMOND
TOWN CENTER



General Note: Information provided is intended for graphic reference only. This drawing should not be scaled to determine specific area, location, or design requirements.

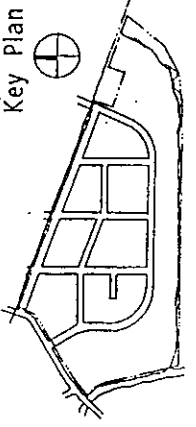


Design Area Overlay

TOWN CENTER

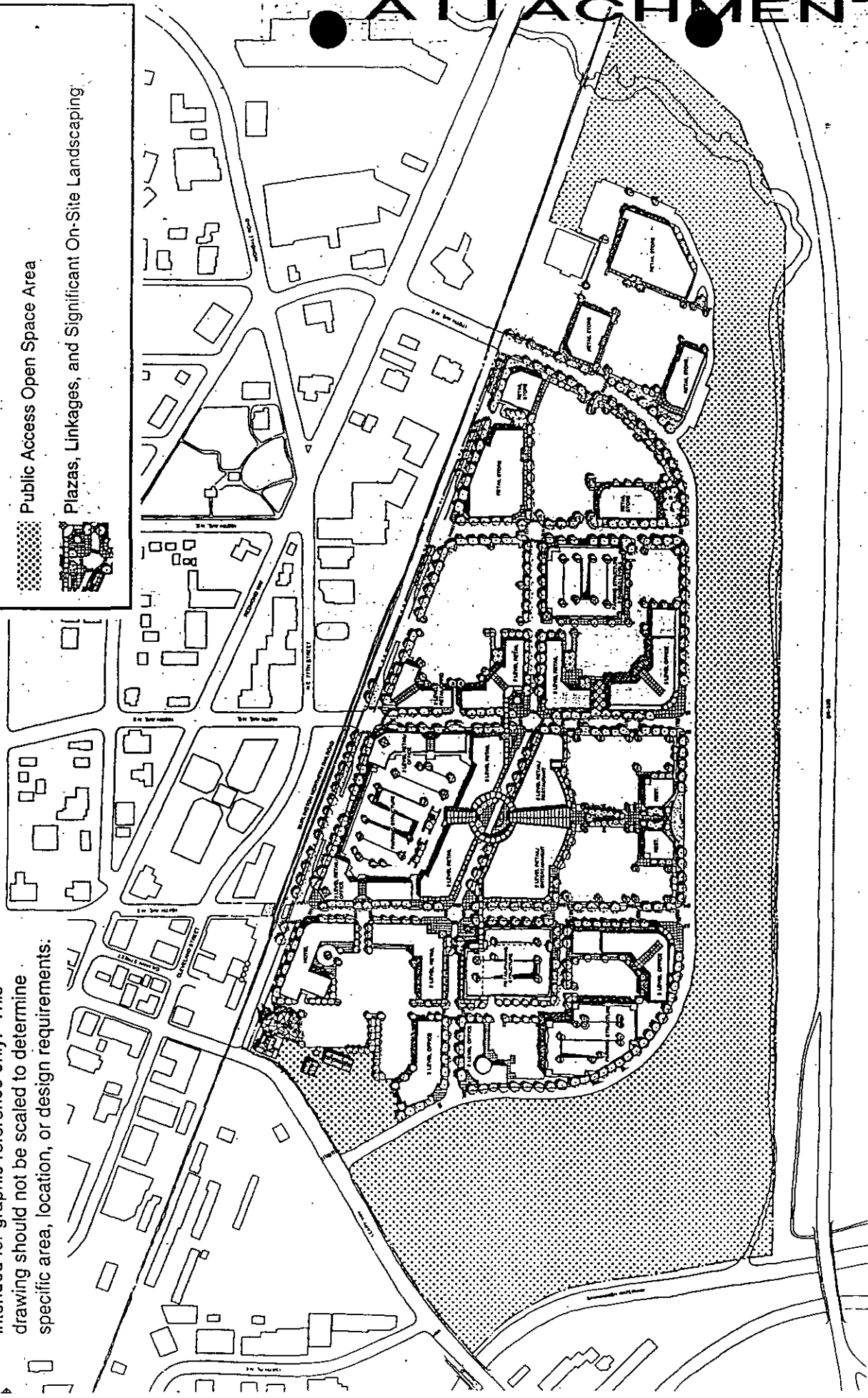
REDMOND

Key Plan

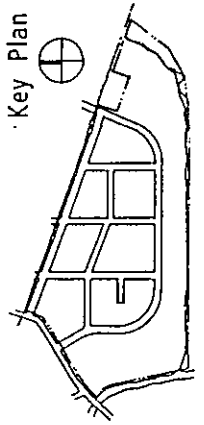


General Note: Information provided is intended for graphic reference only. This drawing should not be scaled to determine specific area, location, or design requirements.

OPEN SPACE AREAS AND CLASSIFICATIONS:
Public Access Open Space Area
Plazas, Linkages, and Significant On-Site Landscaping



Key Plan



Public Access Open Space Area Plan

REDMOND TOWN CENTER

**PLANNING COMMISSION RECOMMENDED
DEVELOPMENT GUIDE AMENDMENTS
FOR REDMOND TOWN CENTER - DGA94-004**

SECTION 20C.10

Section 20C.10.

Section 20C.10.230(05), City Center Permitted Land Use. Footnote number 5 of the table entitled "City Center Permitted Land Use Chart" shall be revised to read as follows:

Prior to approval of any clearing, segregation or development permits; a conceptual Master Plan which shows, at a minimum, the building sites, streets, open space, required building frontages and plazas of the proposed development shall be approved by the Technical Committee and Design Review Board.

The chart entitled "City Center Land Use and Site Requirements", referring to Maximum Building Height (stories/feet) shall be modified to read as follows:

- a. Mixed-Use Area: 3 stories (60 feet); hotel - 6 stories (75 feet) or 8 stories (100 feet) with meeting, conference and banquet facilities in hotel.
- b. Office Park Area: 5 stories (75 feet)
- c. Bear Creek Retail Areas: 3 stories (60 feet)

Footnote number 8 will be read:
The hotel will be located within the northwestern portion of the Design Area.
(Please see Attachment 1B.)

Section 20C.10.230(25)(a)

The map entitled "City Center Pedestrian System" shall be replaced with a new map. (Please see Attachment 2B.)

SECTION 20C.20

Section 20C.20.230(30) Sign

Requirements per Zoning District. The chart entitled "A Part of Subsection 20C.20.230(30) Sign Requirements per Zoning District", second column (labeled "...CC-2) of the row entitled "Maximum Area (Square Feet)", shall be modified as follows:

The larger of 30 square feet or 15% of the facade to which the sign is attached up to a maximum of 300 square feet.

The following will replace Sections 20C.30.050(30) of the RCDG.

SECTION 20C.30
20C.30.050(30) Mixed-Use Center Design Area

(a) Urban Design

The Mixed-Use Center Design Area shall consist of three sub-areas as follows:

- a. Mixed-Use Retail area design concept stresses a pedestrian oriented, open-air complex that mirrors the existing downtown transportation network and the architectural character and scale of the historic portion of the downtown neighborhood. Primary design features for the Mixed-Use Retail area include storefronts along roadways, curbside parking, pedestrian plazas, and sidewalk designs that integrate into building architecture. (See Figures 3, 4, and 7.)
- b. Office Park area design features multi-level office buildings and an urban gateway facing SR520. Building height, location, and architectural character is intended to create a strong urban perimeter and a varied urban texture, connecting the site with the downtown. (See Figure 9.)
- c. Bear Creek Retail area is planned for auto-oriented retail tenants. The freestanding buildings with surface parking would be distinct from the other two areas. However, architectural character, featured design elements, and pedestrian linkages are intended to incorporate a design commonality with the rest of the site. (See Figure 10.)

2. Gateway to Downtown - The Design Area functions as a gateway to the City from SR 520. Development in this area should complement the other components of this gateway, Marymoor Park, and Bear Creek, by providing attractive, interesting urban activity. Development should be consistent with the natural environment by minimizing glare, providing indirect lighting, avoiding intense signage, and providing a soft edge where the urban and natural environments meet. (See Figure 1.)

3. Downtown Integration - Connection to existing roads including landscape treatment, road surface, sidewalk size and placement should respect and extend the existing grid system, streetscape, and character consistent with current standards and regulations. Development in the Design Area - should further City goals for the following subareas:

- a. Leary Way - Leary Way between the Sammamish River and the Burlington Northern railroad tracks should remain as a "green gateway" to the City of Redmond.
- b. Northern Boundary - Leary Way to 164th - Building siting should maintain continuity of building frontage in order to integrate new development with Old Town Design District.
- c. Northern Boundary - Leary Way to 170th Avenue - This area should provide linkage capability between existing public roadways north of Burlington Northern RR and private roadways south of same. These new alignments should provide extension of the established visual corridors.

New connections on the site to existing north/south roads in this area should be compatible with the character of the existing older improvements.

Retail buildings located at the northern edge of the site within the Mixed-Use Retail area will establish functional and visual continuity with the downtown. The character of the new buildings will be compatible with older existing buildings. (See Figures 2 and 5.)

d. Bear Creek - The edge along Bear Creek should be kept as a natural area, with uses limited to passive activity and trail/pathway connections.

Signage in this area should be limited to traffic, safety and directional information, or be consistent with the public recreational use of the area.

Structures consistent with and supporting passive use of this area may be allowed, but should be kept to a minimum.

e. Sammamish River - The edge along the Sammamish River should serve as an extension of existing activity on the Sammamish River Trail just north of this Design Area. Uses should include trail and pathway activities.

Signage should be limited to traffic, safety and directional information or be consistent with the public recreational use of the area.

Structures consistent with and supporting trail/pathway activities may be allowed, but should be kept to a minimum.

f. Railroad - Pedestrian Crossings: Design and construct City approved architectural/urban design features, walkways and landscaping on Leary Way, 164th, 166th Avenue N.E. and other

locations as determined to be necessary as the result of traffic studies. The connection at 164th will be made when the vehicular access to the site is constructed.

(b) Architectural Guidelines

1. Siting of Buildings - Buildings should be sited to enclose either a common space or provide enclosure to the street.

Consideration should be given to the relationship between buildings and adjacent open space areas. All design should appear as an integrated part of an over-all site plan. (See Figures 3, 4, and 7.)

2. Building Form - The roofline of buildings should be modulated to avoid large areas of flat roof and should include interesting architectural features. Consideration should be given to the appearance from nearby hillsides. (See Figures 3, 4, 6, and 10.)

Encourage varieties of shapes, angles, and reliefs in the upper stories of structures over four stories.

Large buildings should avoid continuous, flat facades.

Avoid the use of false fronts and large blank walls.

Facades should be divided into increments through the use of architectural features such as bay windows, offsets, recesses and other devices which break or minimize scale.

The ground floor of buildings should provide pedestrian interest and activity. The use of arcades, colonnades, or awnings to provide pedestrian protection is encouraged. Column and bay spacing along street fronts should be provided no greater than 36 feet apart in order to maintain a pedestrian

oriented scale and rhythm. (See Figures 2, 3, 7, and 8.)

Building Design should complement the character of adjoining Old Town.



Building Design must relate in material, colors, scale and form, which are harmonious with the surrounding environment in Old Town.

Buildings and facades in the Mixed-Use Retail area should be a combination of brick, stucco-like finishes, smooth finished concrete, and architectural metals. Building facades in the Mixed-Use Retail and Office Park areas should have a greater proportion of voids (windows) than solids (blank walls) on pedestrian levels. Buildings and facades in the Bear Creek Retail area should be primarily masonry products with concrete and architectural metals used for detailing if desired. In all design, there should be emphasis upon the quality of detail and special form in: window treatments, columns, eaves, cornices, lighting, signing, etc.

Buildings and the spaces between them should relate easily and openly to the external public areas or plazas.

The scale of all structures in relationship to other structures and spaces is important. The scale should be two to three stories in the retail core. Some variation in heights contributes to the variety and complexity of the environmental experience, and is encouraged. (See Figures 3 and 4.)

The development of ground level view points on each building level which take advantage of solar access and views of the site's open spaces is encouraged.

The integrity of a building as an individual structure or as part of a series of buildings should be respected.

Buildings as a whole should be seen as significant architectural elements.

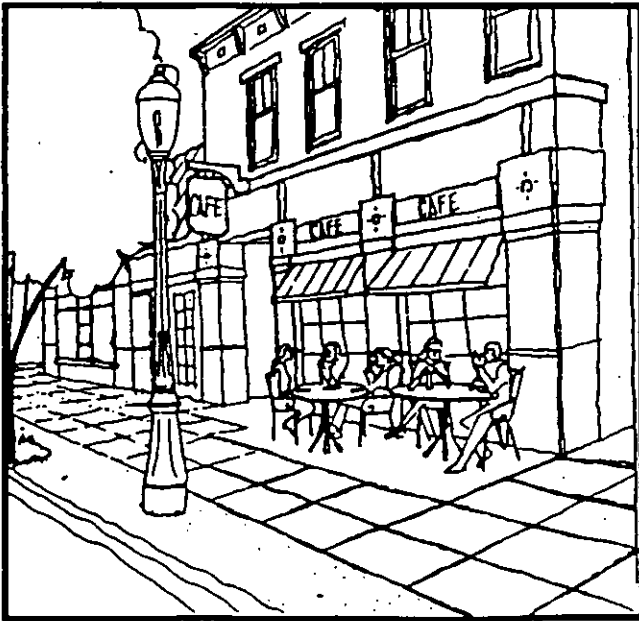
Storefront design and materials should be allowed to be unique while maintaining the character of the building facade of which they are a part.

3. Building Entry - Orient building entrances to the street in a manner which provides easily identifiable and accessible pedestrian entry ways. Highlight building entrances through landscape or architectural design features. Building entries should be designed in conjunction with the landscape treatment of pedestrian ways in the parking areas that directly relate to the entry.

4. Public Art - Encourage public art in public areas of the design area particularly in and around the Mixed-Use Retail area.

5. Building Orientation - Uses in the center should be oriented externally as well as internally (as is applicable) by using outward facing building facades, malls, entrances and other design techniques.

Buildings in the Mixed-Use Retail and Office Park areas should abut the sidewalks on at least one side and orient the primary entrance, or entrances, toward the street.



6. Building Colors and Materials - Building colors and materials shall be selected to integrate with each other, other buildings in the Old Town, and other adjacent commercial areas, while allowing a richness of architectural expression for the various buildings.

Buildings should be constructed of materials that minimize light reflection and glare.

Care should be taken to avoid clashing colors on individual buildings and between adjacent buildings.

7. Windows and Displays - Windows and display areas shall be located along pedestrian routes to enhance the pedestrian experience. (See Figures 2, 7, and 8.)

Store fronts should be visually open wherever practical. Stores should use enough glass so that the activity inside the store is obvious to the passerby. In all cases merchandise should be easily visible to pedestrians.

Windows shall be provided on the street level in the Mixed-Use Retail buildings rather than blank walls to encourage a visual and economic link between the business and passing pedestrians. A minimum of 60% of ground floor facades facing streets in the Mixed-Use Retail area shall be in non-reflective, transparent glazing. Where windows cannot be provided, art work in window boxes may be used with Site Plan Review approval. (See Figures 2, 7 and 8.)

8. Future Development Pads - Future development pads shall be designed to relate to the rest of the project's architecture and will provide pedestrian scale exterior features.

9. Design Consistency - Each phase of the development shall be designed to be consistent with, but not necessarily the same as, the balance of the project architecture, including materials, colors, and general style.

10. Pedestrian Features - Provide pedestrian scale external features including such items as window and glass display cases, street furniture, covered walkways, and avoid large blank walls.

11. Outdoor Pedestrian Areas - The outdoor pedestrian areas shall include special paving treatments, landscaping and seating areas. (See Figures 1 and 3.)

Outdoor and ground floor areas should be designed to encourage outdoor activities such as vendors, art displays, seating areas, outdoor cafes, abutting retail activities, and other features of interest to pedestrians.

12. Site Entrances - Entrances to Town Center development shall be emphasized with landscape treatments to strongly indicate the pedestrian orientation of these areas.

Architectural/urban design treatment of 166th shall encourage pedestrian circulation from the project to the Cleveland Street retail area.

13. Rooftop Mechanical Equipment - Rooftop mechanical equipment will be screened in a manner which enhances the overall architectural design, and rooftops will be of a color that reduces glare and other types of visual impact on the adjacent residentially developed hillsides.

Rooftops should incorporate features which soften rectilinear forms and effectively screen mechanical equipment from view.

(c) Transportation Guidelines

1. VEHICULAR

a. Roadway Configurations - Roadways that are above existing-grade should be designed in a manner to reduce visual impact of pavement area such as using landscaping or berms.

Encourage alignment of all roadways to minimize the removal of all existing significant, healthy trees.

Streets should not be wider than 4 travel lanes with the appropriate number of lanes at intersections between the design area, and areas targeted for integration with the downtown.

Vehicular circulation shall relate the various uses on the site to each other. Roads shall be designed to enhance viability of the project components.

Vehicle access to parking areas should be designed to minimize interaction of vehicles with pedestrians.

b. Parking - Surface - Where possible, locate parking behind buildings and away from areas of high public visibility. Landscape and screen surface parking areas visible to the public.

Parking areas should include landscape areas and be designed to minimize long, straight, monotonous rows. The size and location of parking areas should be minimized and related to the group of buildings served.

Parking areas should include a clear circulation network to guide pedestrians to the building entrances.

Visual impact of surface parking areas should be minimized from the SR 520 corridor.

Landscaping shall be provided to screen surface parking areas and provide transition between the project and surrounding areas particularly when viewed from SR 520, Leary Way and adjacent hillsides.

Parking lots should be landscaped to screen parking and provide visual relief from large asphalt surfaces.

Pedestrian access from parking areas should be safe and clearly defined.

Landscaped medians should be provided where access and traffic movements allow.

Conflict between pedestrians and automobiles should be minimized by designing streets to provide well defined pedestrian walkways and crosswalks that reduce vehicle speeds.

Open space and landscaping should be coordinated and linked wherever possible, particularly in relation to public areas and the pedestrian system.

To minimize visual impacts where surface parking is utilized, project design should incorporate screening and landscape treatment of surface parking areas.

Design and locate parking areas in a manner that will break up large areas of parking and encourage shared parking with existing downtown uses.

Patrons of the retail center shall be allowed to use parking while patronizing other businesses in City Center. No rules, signage or penalties shall be enacted by Town Center to preclude this parking allowance.

c. Parking - Structured - Structured parking should be designed to avoid undifferentiated planes. The scale of parking structures should be modulated by interruptions of the facades, setbacks, and lowering the first level below the existing grade (where the water table allows) to reduce total height.

Facades of parking structures should include a landscape treatment in addition to architectural screening from the SR520 corridor.

Parking structures shall have landscaping around the perimeter which will correspond to adjacent land uses and activities. Landscaping shall include, but not be limited to, a combination of shade trees, evergreen trees, shrubs, groundcovers, deciduous native and ornamental shrubs, and vines to further screen the structures.

The top floor of parking structures should include landscape screening in areas such as along the cornice and on the deck, either by trees or a screening trellis treatment.

Provide walkways in parking floors which have curbs or other barriers to protect from vehicular intrusion.

For security, pedestrian routes should be visible and avoid enclosed, hidden areas. Emergency call boxes should be available.

At least 50% of the parking provided for the entire site should occur in parking structures. The ratio of minimum structured parking shall be maintained for all phases of development of the Mixed-Use Retail area and the Office Park.

Parking structures should be enclosed with retail or office uses on the exterior or where this enclosure is not feasible, the visual impact should be softened with landscaping or screening.

2. Pedestrian

a. Linkages - Link proposed development to walkways, trails, and bicycle systems in the surrounding area by connecting and lining up directly to existing linkages, closing gaps and treating crossings of barriers (such as the railroad, Bear Creek Parkway, and driveways) with special design treatment, minimizing barriers, designing with consistent materials, widths and locations, and providing safe, easy and clearly identifiable access to and along the linkages. Safe, convenient and attractive connections to Marymoor Park, the Sammamish River Trail, and the Bear Creek trail system should be provided.

The sidewalk system shall be emphasized with landscape treatments to provide readily perceived pedestrian pathways through and around the Mixed-Use Center Design Area.

b. Sidewalks - When extending an existing sidewalk, the new walkway should meet current standards and regulations where there is sufficient right-of-way, and should be constructed of a material and dimension which is compatible with and improves upon the existing character.

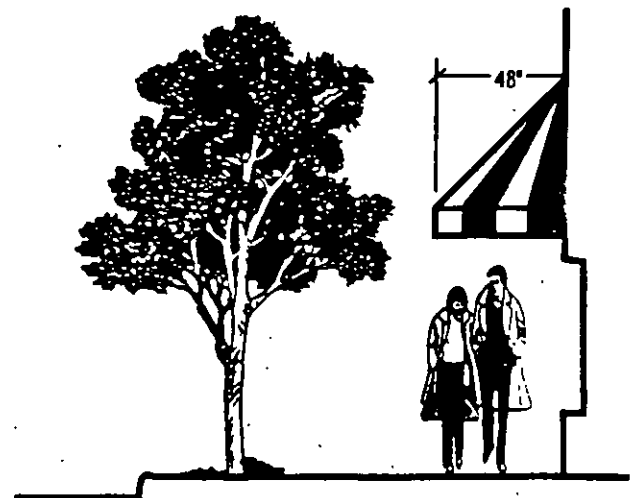
Sidewalks should meet similar standards to those of the approved Pedestrian Linkage System.

Paving of sidewalks and pedestrian crossings should be safe and constructed of a uniform material that is compatible with the character of the design district. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.

Encourage alignment of new sidewalks to minimize the removal of all existing significant, healthy trees.

c. Arcades, Colonnades, Canopies - In areas of high pedestrian activity, devices for protection from weather are encouraged. Consistent treatment within a single area is also encouraged in order to provide a strong identity of space. (See Figures 2, 3, 7 and 8.)

Buildings should be designed to provide for weather and wind protection at the ground level. Buildings fronting sidewalks should provide pedestrian weather protection by way of arcades, colonnades, or canopies a minimum of 48 inches in depth. The elements should be complementary to the building's design and design of contiguous weather protection elements on adjoining buildings. Materials and design should engender qualities of permanence and appeal.



Awnings or sunshades should be harmonious to the design of the building to which attached, and should be in keeping with the character of the building. Materials should be durable, long lasting, and require low maintenance. Back-lit awnings are discouraged.

d. Enclosed Malls - The design of enclosed malls should allow pedestrians through access during hours of retail center operation while still maintaining security.

e. Trails - Pedestrian - Special design treatment and appropriate safety features should be afforded the pedestrian trail crossings at public rights-of-way and at the Burlington Northern railroad tracks.

f. Trails - Bicycle - Facilities for parking and locking bicycles should be provided and be readily accessible from bicycle trails.

g. Trails - Equestrian - Width of the trail should be adequate for two riders side-by-side in order to avoid earth compaction and vegetation deterioration. Equestrian trails should be separate from pedestrian and bicycle trails. Equestrian trails do not have to be constructed until offsite linkage is constructed to the proposed development.

h. Plazas/Pedestrian Malls - Plazas, pedestrian malls and other amenity open spaces shall be developed that promote outdoor activity and encourage pedestrian circulation between the Mixed-Use Center and the balance of the City Center area.

(d) Landscape Guidelines

1. Urban Landscape Treatment - Building entries, primary vehicular entries and building perimeters should be enhanced with landscaping which could include ornamental vines, groundcovers, shrubs and/or trees selected for their screening, canopy, spatial enclosure and seasonal variation.

2. Site Furnishings - Benches, kiosks, signs, bollards, waste receptacles, street vending carts, water fountains, lighting standards, perch walls, sidewalks, pathways, trails and special water features should be designed to

be compatible elements of like materials and design.

3. Perimeter Landscaping - Landscaping on the perimeter of the site will create a transition between the project and the surrounding area.

4. Landscaping on Streets - Landscaping on streets should be simplified to allow adequate visibility from automobiles to businesses.

5. Trees, plants and flowers - The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.

(e) Open Space Guidelines

1. Tree Retention and Open Space Landscaping - Preserve existing natural features, particularly healthy mature trees and stream courses.

Preserve a minimum of 100 percent of all trees within the Public Access Open Space as identified in Attachment 3B. This area includes the cluster of trees along the east side of Leary Way for the purpose of preserving the corridor's green gateway image, and the healthy trees along the Bear Creek and Sammamish River corridors. Trees that cannot be retained due to approved street and/or utility construction shall be replaced with native nursery stock of similar or like variety at a 1:1 ratio, with tree sizes in accordance with Section 20C.20.090(25)d), pursuant to a landscape plan approved in conjunction with Site Plan Review. Trees removed as a result of construction activities which are intended to be preserved shall be replaced per Section 20C.20.090(25). Replacement trees shall be located in the immediate vicinity as is practical.

Minimize new grading in this area.

Install landscape screening between this open space area and adjacent parking areas.

Encourage passive recreation including a walking trail, bicycle trail, seating and rest areas, pedestrian lighting, and site furnishings. Connections should be provided to the Justice White House, Mixed-Use Retail area, Marymoor Park, Sammamish River trail system and other open space areas.

The "soft edge" landscape treatment to the south of Town Center along Bear Creek shall provide for a true transition between the natural, riparian area of the creek to the more urban Mixed-Use Design Area.

The informal nature of the west, south and east portion of the site should be maintained by retaining native materials and random planting of compatible plant materials consistent with the City Center Neighborhood Program.

2. Justice White House/Saturday Market - The areas around the Saturday Market and Justice White House shall be retained as open space. Areas at the Justice White House should encourage active and passive recreation. These areas should connect to other open spaces, trails, and the Mixed-Use Retail area.

3. Sammamish River - Open space should be retained along the Sammamish River and enhanced by:

- a. Providing grade separation for trails at all appropriate and feasible locations.
- b. Making connections to other open space zones.

c. An ongoing stormwater outflow monitoring program for private drainage systems shall be designed and, upon approval by the Technical Committee, be implemented by the applicant. The monitoring program shall consider specific contaminants which may likely be present in the runoff and shall be revised periodically as appropriate.

4. Bear Creek. Open space along Bear Creek should be retained and enhanced by:

a. Encouraging passive recreation areas and activities and discouraging active recreation.

b. All stormwater swales and recharge areas should be integrated with the natural environment.

c. Protecting vegetation of the riparian habitat in this zone by limiting access to the creek to designated access points.

d. Providing connections to Marymoor Park, the Sammamish River, mixed-use center and other open spaces.

e. Facilities within this area should include a pedestrian pathway, bicycle path, equestrian trail when required, passive water access area, seating, and site furnishings.

f. An ongoing stormwater outflow monitoring program for private drainage systems shall be designed and, upon approval by the Technical Committee, be implemented by the applicant. The monitoring program shall consider specific contaminants which may likely be present in the runoff and shall be revised periodically as appropriate.

5. Public Access Open Space - Public Access Open Space should be retained, enhanced and made available for public use in this design area as shown on Attachment 3B.

At least 44 acres shall be preserved by easement to the City or controlled by other methods that would permanently assure the open space to the City. This downtown Public Access Open Space shall serve as a visual amenity and passive recreation open space.

6. Open Space Acreage - Public Access Open Space as shown on Attachment 3B shall include a minimum of 44 acres. This will include natural areas inclusive of the floodway, and the areas around the Justice White House and the Saturday Market.

(f) Signage Guidelines

1. Sign kiosks - Sign kiosks, which provide direction to groups of tenants are encouraged as a means for discouraging numerous individual directional tenant signs and providing clear directions. These kiosks shall be located in high traffic areas and be consistent with the design theme of the development.

2. Symbols and colors - International symbols and colors should be used whenever applicable.

3. SR 520 Signage - Signage for the subject proposal shall comply with the SR 520 Corridor Signage Policy adopted by the City Council.

Signs will be evaluated according to the overall impact, size, shape, texture, lettering style, method of attachment, color, and lighting in relation to use, the building and street where the sign will be located, and the relationship of the sign to other signs and other buildings in the vicinity. Adherence to or enhancement of sight lines both parallel and perpendicular to the sidewalks will be considered. The primary reference will be to the average pedestrian's eye-level view,

although views into or down the street from adjacent buildings should be an integral feature of any review.

Signs should be simple, clear and direct. Extruded aluminum or plastic signs are not appropriate. Careful use of neon may be acceptable.

Signs should not hide or obscure the architectural elements of the building.

Signs attached or hung from the arcades, colonnades, and canopies perpendicular to the sidewalk should be encouraged.

Projecting, double-faced or three-dimensional signs may be allowed if they are integrated into the overall streetscape and do not destroy sight lines. Symbolic three-dimensional signs such as a shaving mug, barber pole, pawn shop symbol, or other symbols illustrating the product being sold on the premises are acceptable provided they meet other sign guidelines. When possible, signs should reflect the character and the use within the structure.

Sign lighting should be energy efficient.

(g) Lighting Plan - A lighting plan shall be provided and approved, which encourages night time pedestrian movement between the adjacent commercial areas, particularly Leary Way and 166th Avenue NE.

Parking lot and security lighting shall be screened by buildings and other means to minimize light intrusion onto the Sammamish River and Bear Creek.

Street lighting should relate in scale to the pedestrian characters of the area. The design of the light standards and luminaries should enhance the design theme.

20C.10.230(16) CITY CENTER SITE REQUIREMENTS

DESIGN AREA	Old Town	Mixed Use Center	Valley View Bear Creek Trestle	Town Square	Sammamish Trail	Leary	River Bend	Foot Hill	North Point East Hill Carter
DESIGN CRITERIA (20C.30)	See Section 20C.30.050(25)	See Section 20C.30.050(30)	See Section 20C.30.050(10)	See Section 20C.30.050(15)	See Section 20C.30.050(15)	See Section 20C.30.050(35)	See Section 20C.30.050(40)	See Section 20C.30.050(50)	See Section 20C.30.050(45) 20C.30.050(50)
SITE STANDARDS	FAR 1.75 See Table 20C.10.230(16)(A) 2nd floor or above	55 du/ac See Table 20C.10.230(16)(A) 2nd floor or above	30 du/ac See Table 20C.10.230(16)(A) 2nd floor or above	FAR 2.25 See Table 20C.10.230(16)(A) 2nd floor or above	FAR 1.75 See Table 20C.10.230(16)(A) 2nd floor or above	FAR 1.75 See Table 20C.10.230(16)(A) 2nd floor or above	FAR 1.75 See Table 20C.10.230(16)(A) 2nd floor or above	FAR 2.0 See Table 20C.10.230(16)(A) No Special Restrictions	55 du/ac See Table 20C.10.230(16)(A) No Special Restrictions
MINIMUM YARD SETBACKS	2, 2a 14 feet	3	1a, 2a 20 feet	2, 2a 14 feet	2, 2a 14 feet	2b 20 feet	2a 20 feet	2a 28 feet	2a 28 feet
- Front Street	2 14 feet	3	1a 20 feet	2 14 feet	2 14 feet	2c 0 feet	2c 0 feet	See Section 20C.10.230(17)(b)	See Section 20C.10.230(17)(b)
- Side Street	3	3	0 feet	0 feet	0 feet	2c 5 feet min.	N/A	See Section 20C.10.230(17)(b)	See Section 20C.10.230(17)(b)
- Side Yard	5 feet min.	5 feet min.	5 feet min.	5 feet min.	5 feet min.	5 feet min.	N/A	10 feet	10 feet
- Side Yard - Residential uses above ground floor commercial	3	3	0 feet	0 feet	0 feet	2c	0 feet	10 feet	10 feet
- Rear Yard	0 feet	3	10 feet	10 feet	10 feet	2c	N/A	5 feet	5 feet
- Rear Yard - Residential uses above ground floor commercial	3	3	5 feet	5 feet	5 feet	2c	5 feet	15 feet	15 feet
- Yard Adjoining Residential District	5 feet	5 feet	N/A	N/A	N/A	N/A	N/A	See Section 20C.10.230(H)	See Section 20C.10.230(H)
- Between Buildings On-Site	N/A	3	5	5	5	5	5	5a	See Section 20C.10.230(H)
- Parking	5	3	2/30'	6/92'	5/75'	Same as Set Backs	5/75'	7	4/48'
MAXIMUM BUILDING HEIGHT (Stories/Feet)	5/75'	one stories (INSERT LANGUAGE)	10 3/45'	6/92' 8/120'	5/75'	5/75'	5/75'	6/90'	4/48'
LOT COVERAGE	11a 100%	11, 11b 80%	11a 80%	11a 100%	11a 100%	11a 100%	11a 100%	11a 80%	See Table 20C.10.230(16)(a)
PARKING SPACES REQUIRED	12 See Section 20C.20.150	See Section 20C.20.150	13 See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150	See Section 20C.20.150

(Ord. 1756)

NOTES:

- 1 - Measured from face of curb. See, also, Section 20C.10.230 (25) City Center Pedestrian System. See also Section 20C.20.220, Sight Clearance at Intersections.
- 1a - Thirty (30) feet; if designated per Section 20C.10.230(25) City Center Pedestrian System.
- 1b - Measured from property line. In case of alley, measured after dedication, if necessary. See also Type VI Pedestrian System in Section 20C.10.230(25).
- 2 - On streets fronting Type V Pedestrian walkways per Section 20C.10.230(25) 14 feet is the average required. Building fronts shall be developed to the sidewalk and/or plaza edges.
- 2a - Front setback for residential use above ground floor commercial shall be regulated by Section 20C.10.230(25) City Center Pedestrian System.
- 2b - Where a grove of trees (6 or more significant trees within a quarter-acre area) lie along a street front, the setback shall be 100 feet.
Otherwise the set back shall be per Section 20C.10.230(25) City Center Pedestrian System requirements.
- 2c - Where a grove of trees (6 or more significant trees within a quarter-acre area) lie along the easterly design area boundary, the setback shall be 30 feet.
Otherwise the set back shall be zero (0) feet.
- 3 - Subject to Master Plan and Site Plan Review Approval.
- 4 - Within 100 feet of adjoining Residential Districts, bay doors other than for loading may not open directly toward (without a building in between) the residential district.
- 5 - Governed by Section 20C.20.150(15)(c) Parking Garages.
- 5a - Same as 5 above, except standards apply regardless of Pedestrian System Type, and sub-paragraph (iv) does not apply.
- 6 - Six (6) floors is maximum height without bonus. Bonus to eight floors granted for provision of 20% on-site usable open space in the form of plazas/arcades accessible to public during extended business hours.
- 7 - Height limit overlay districts may apply; see Section 20C.10.230(11) Height Limit Overlay Zones.
- 8 - ~~Regional Shopping Center - 3 stories (60 feet); Hotel on Town Square - 6 stories (75 feet); or 8 stories (100 feet) with meeting conference and banquet facilities on Town Square - 4 stories (50 feet) at other permitted uses - 3 stories (40 feet) or 4 stories (50 feet) with parking structure.~~
- 9 - Both stories and height standard must not be exceeded, and height shall be measured to the top of mechanical equipment enclosures.
- 10 - The maximum height may be increased to 3 stories and 45 feet when a grocery store is a major tenant on the project site.
- 11 - Governed by Section 20B.85.130(105)(a) Mixed Use Shopping Center Design Area Goal and Policy.
- 11a - For developments with residences above ground floor commercial/office, Lot Coverage shall be governed by this table.
- 11b - For residential development without ground floor commercial/office, Lot Coverage shall be governed by Section 20C.10.230(17)(A).
- 11c - Lot coverage percentage equals: The total site area measured to the PROPERTY LINE, less Pedestrian Systems measured to the CURB LINE, on-site-sidewalks, landscaping, and plazas; DIVIDED by the site area measured to the CURB LINE.
- 12 - A density bonus for contribution to In-Lieu Parking Fund is available per Section 20C.20.150(20)(b).
- 13 - Surface parking is prohibited within 150 feet of the river's top-of-bank.

**PLANNING COMMISSION
RECOMMENDED LANGUAGE FOR**

SECTION 20C.10.230(05), CITY CENTER PERMITTED LAND USE (Chart)

- a. Mixed-Use Retail area: 3 stories (60 feet); hotel - 6 stories (75 feet) or 8 stories (100 feet) with meeting, conference, and banquet facilities in hotel.
- b. Office Park area: 5 stories (75 feet)
- c. Bear Creek Retail area: 3 stories (60 feet)

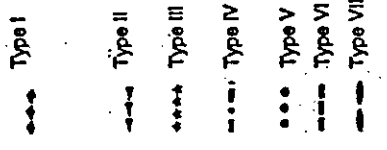
Footnote 8 to read:

The hotel will be located within the northwestern portion of the Design Area.

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- Type I A 30-foot landscaped walkway with 10-foot planter strip for trees, 5-foot sidewalk and 15 feet of planting area when along streets; or 8 feet of sidewalk with 11 feet of landscaping/plaza on each side when providing mid-block connections.
- Type II A 20-foot landscaped walkway which includes an 8-foot sidewalk and 12 feet of planting.
- Type III A 20-foot landscaped walkway with an 8-foot parkway for street trees, 5-foot sidewalk, and 7 feet of planting area.
- Type IV A 28-foot landscaped walkway with 8-foot parkway for trees, 5-foot sidewalk, and 15 feet of planting area.
- Type V A 14-foot sidewalk with buildings up to the sidewalk edge.
- Type VI A 30-foot wide combination walkway/vehicular lane.
- Type VII A 10-foot sidewalk from the Sammamish River Trail to and along the building front.
- Type VIII A 13-foot sidewalk with street trees when building faces street. 20 feet when parking lot faces street to include a 10-foot sidewalk and 10 feet of planting. A 5-foot sidewalk along the north side of the Bear Creek Parkway.

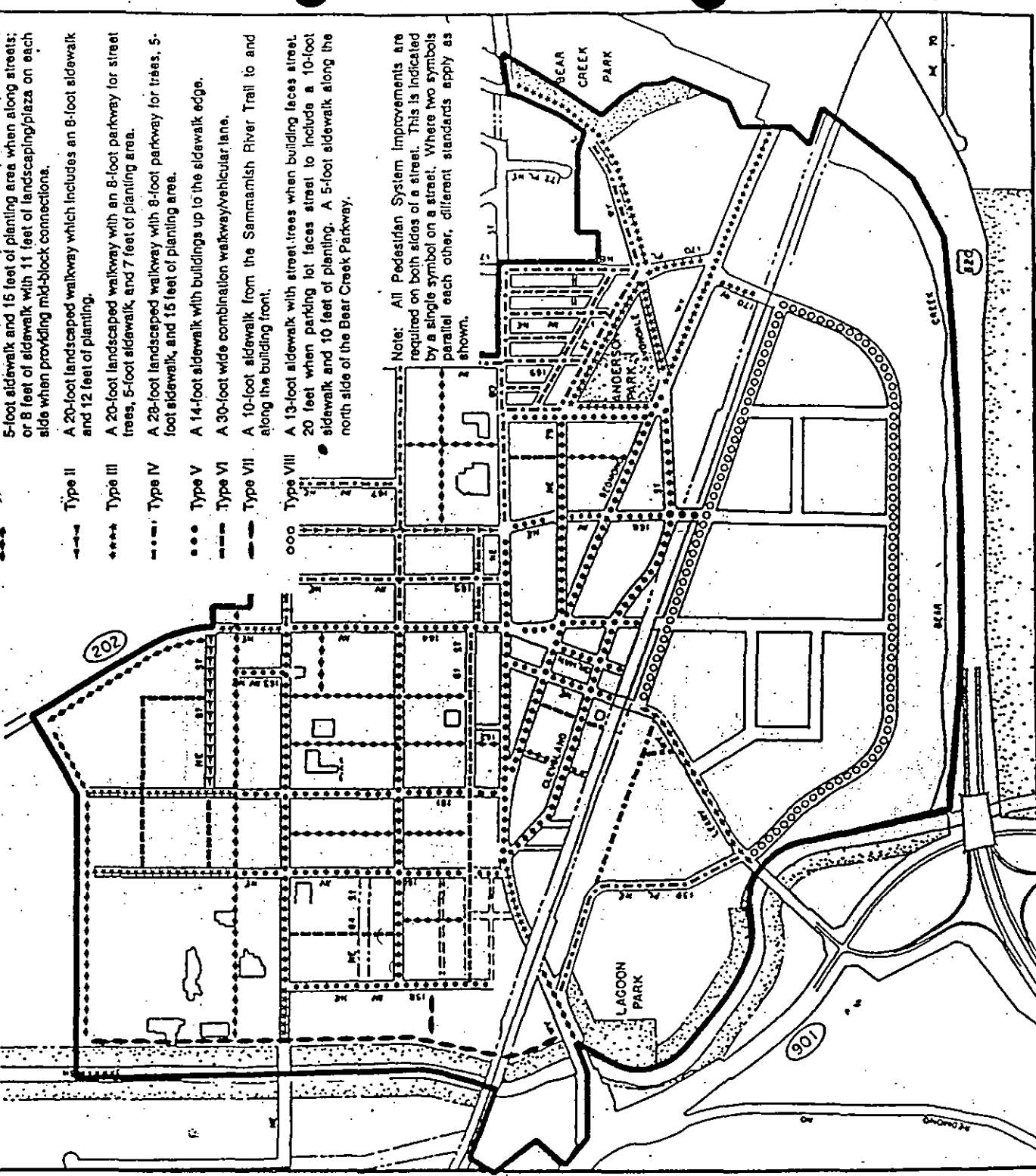
Note: All Pedestrian System Improvements are required on both sides of a street. This is indicated by a single symbol on a street. Where two symbols parallel each other, different standards apply as shown.



202

901

BEAR CREEK PARK
ANDERSON PARK
LAGOON PARK
BEAR CREEK
CITY



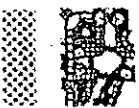
CITY CENTER PEDESTRIAN SYSTEM

PROPOSED PLAN

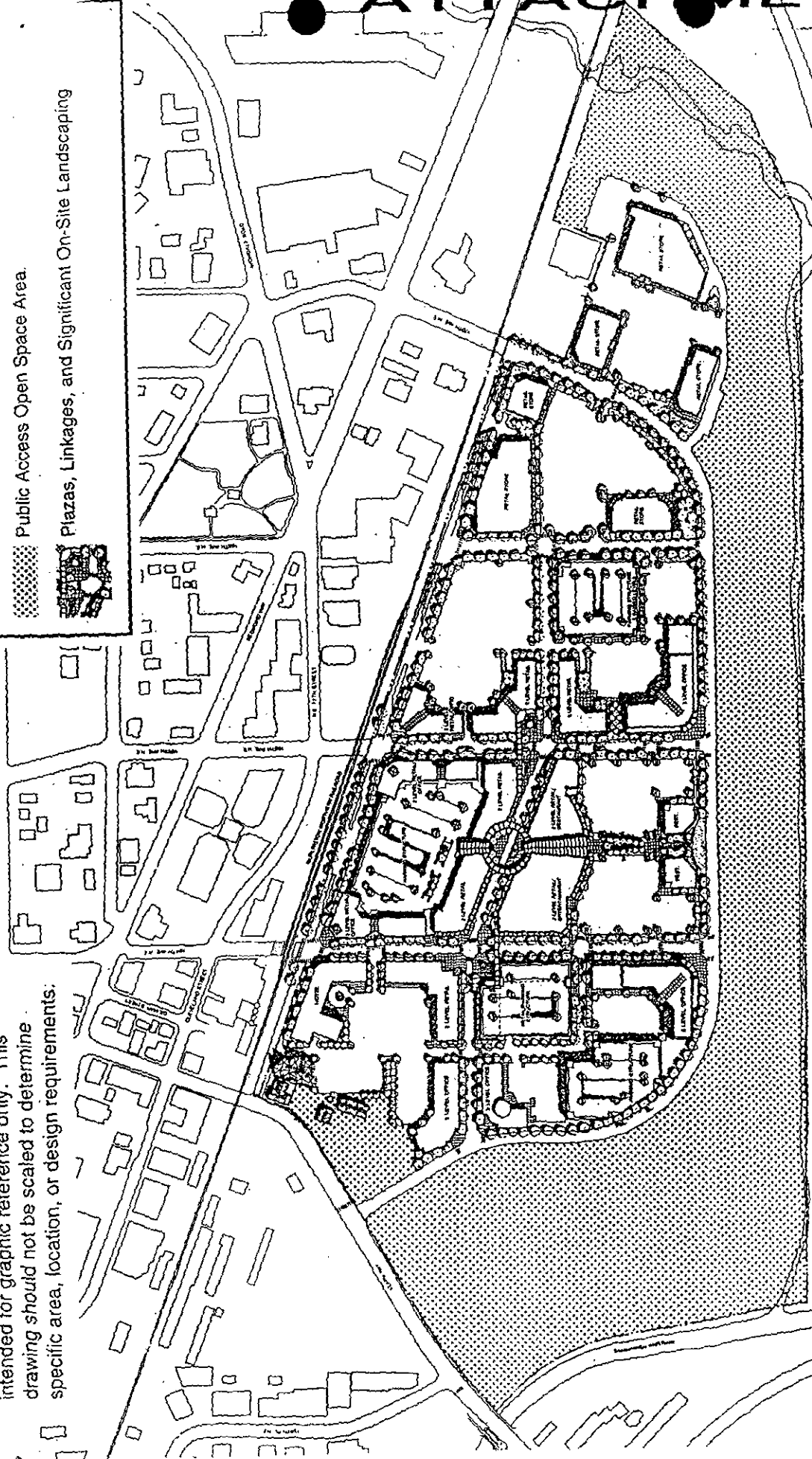
OPEN SPACE AREAS AND CLASSIFICATIONS:

Public Access Open Space Area

Plazas, Linkages, and Significant On-Site Landscaping

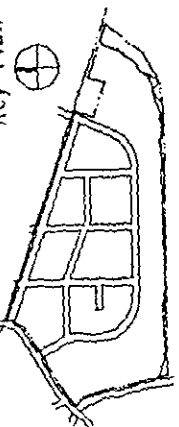


General Note: Information provided is intended for graphic reference only. This drawing should not be scaled to determine specific area, location, or design requirements.

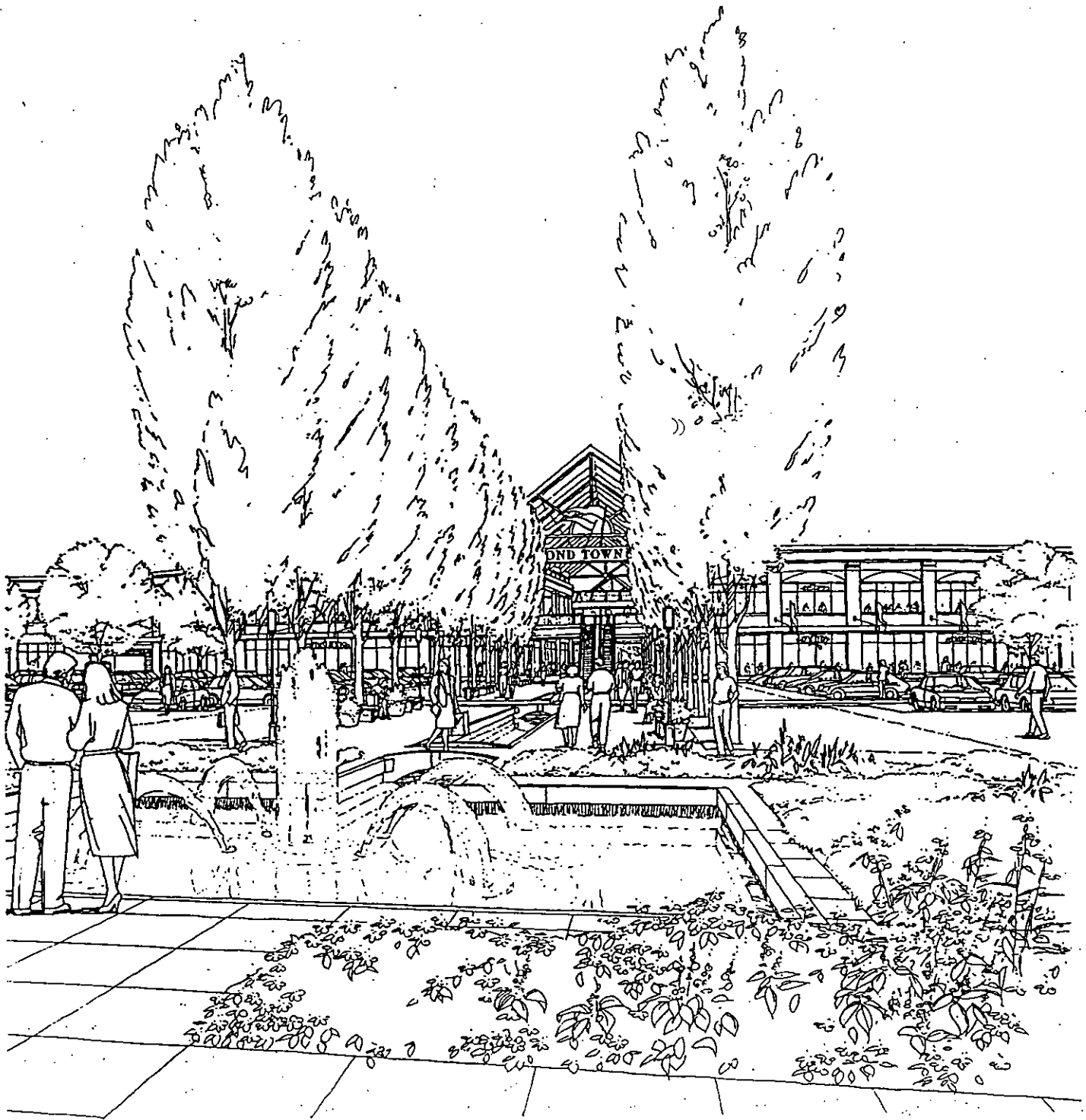


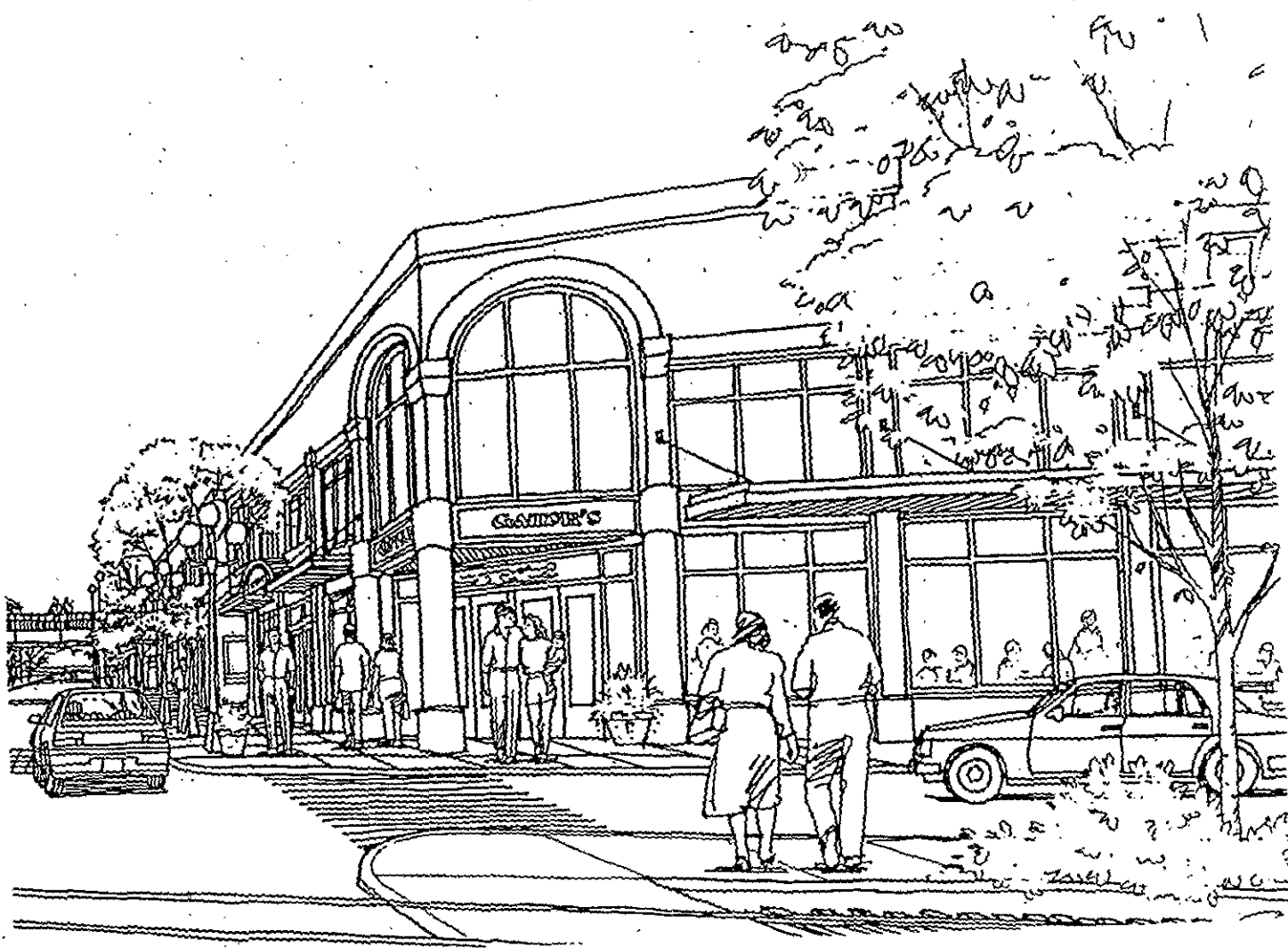
REDMOND TOWN CENTER

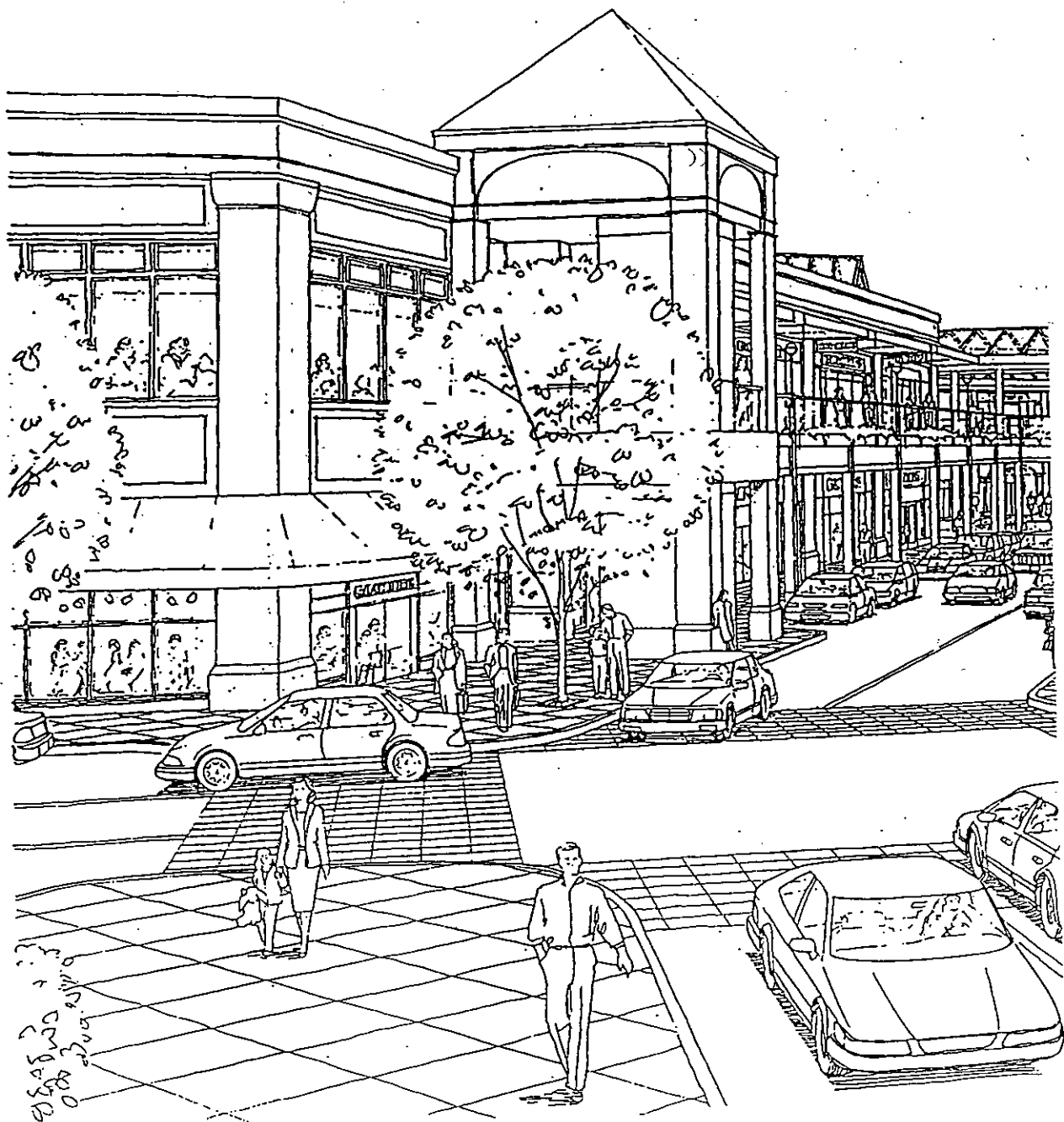
Key Plan

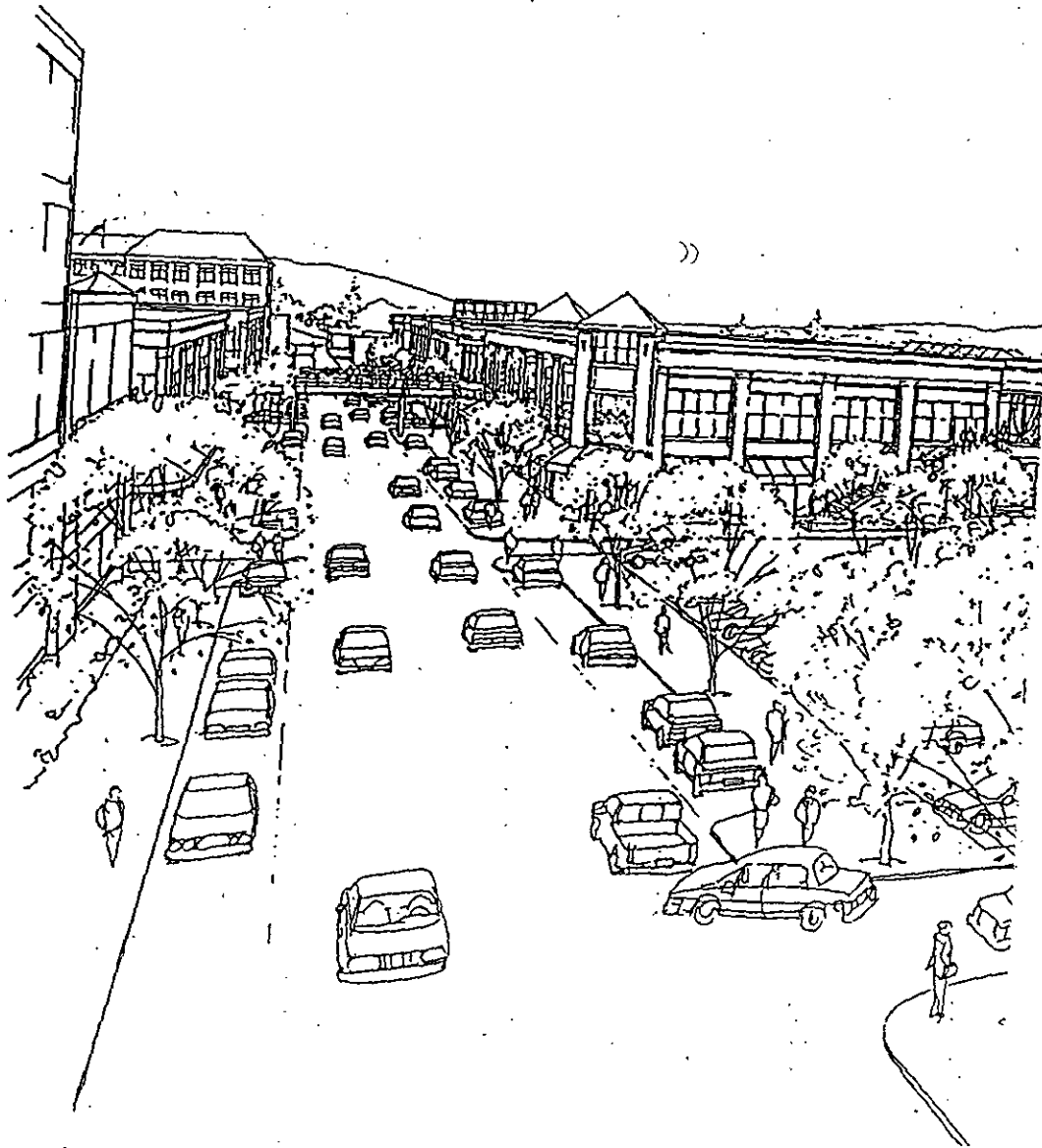


Public Access Open Space Area Plan





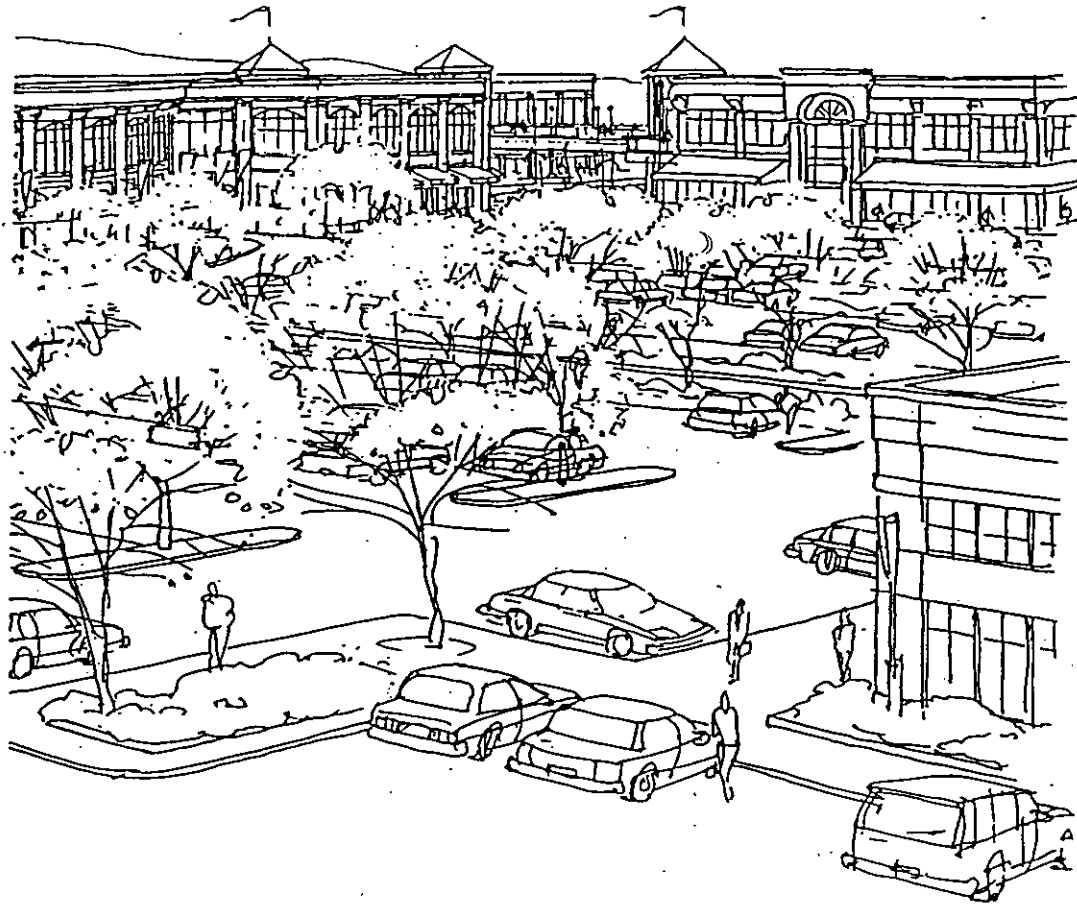


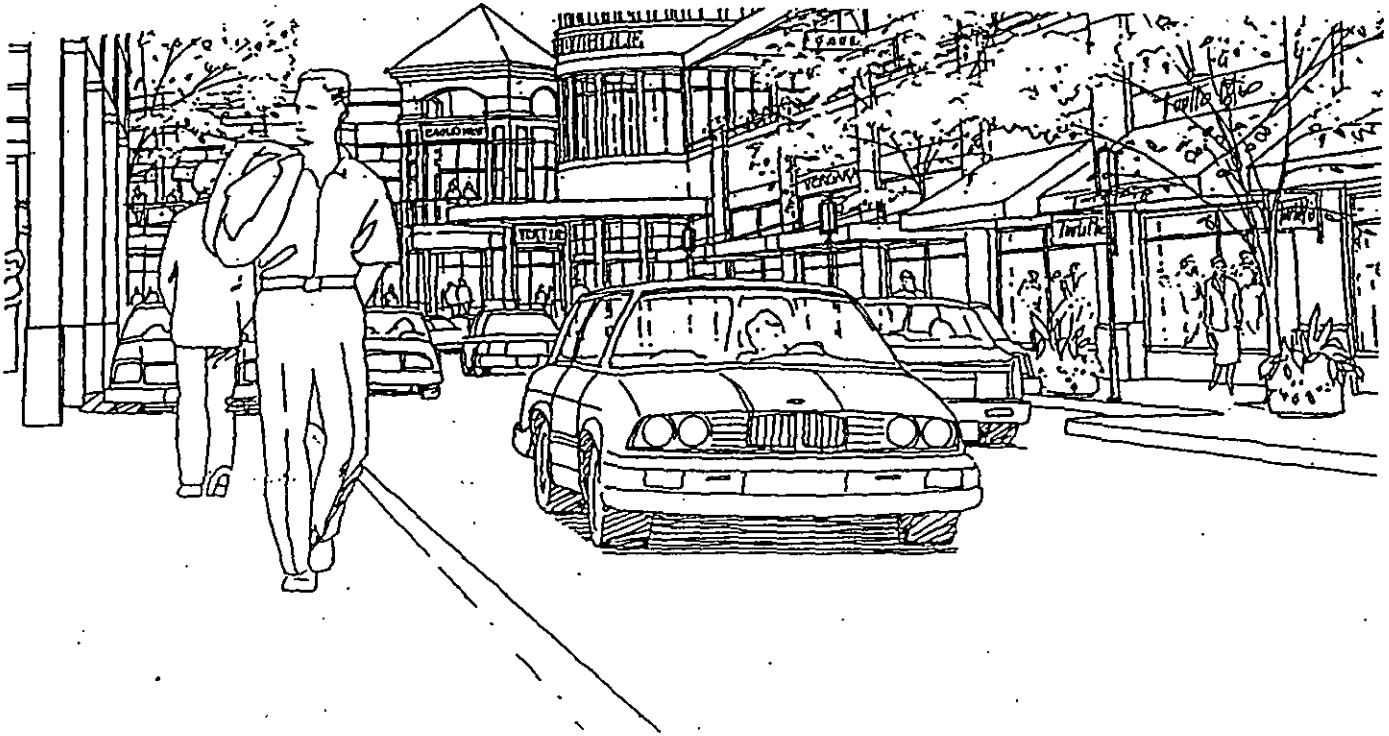


Ordinance No. 1841

FIGURE 4







Ordinance No. 1841

FIGURE 7



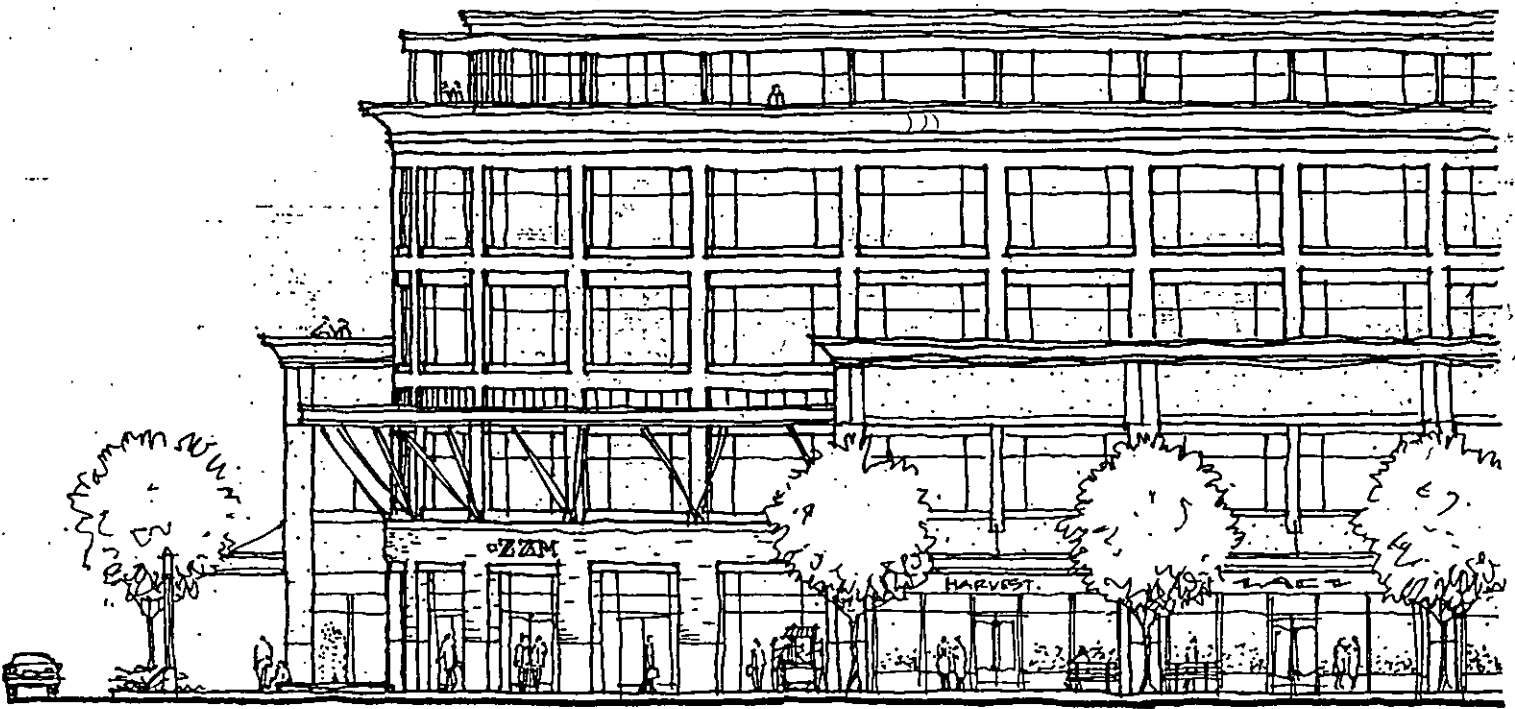
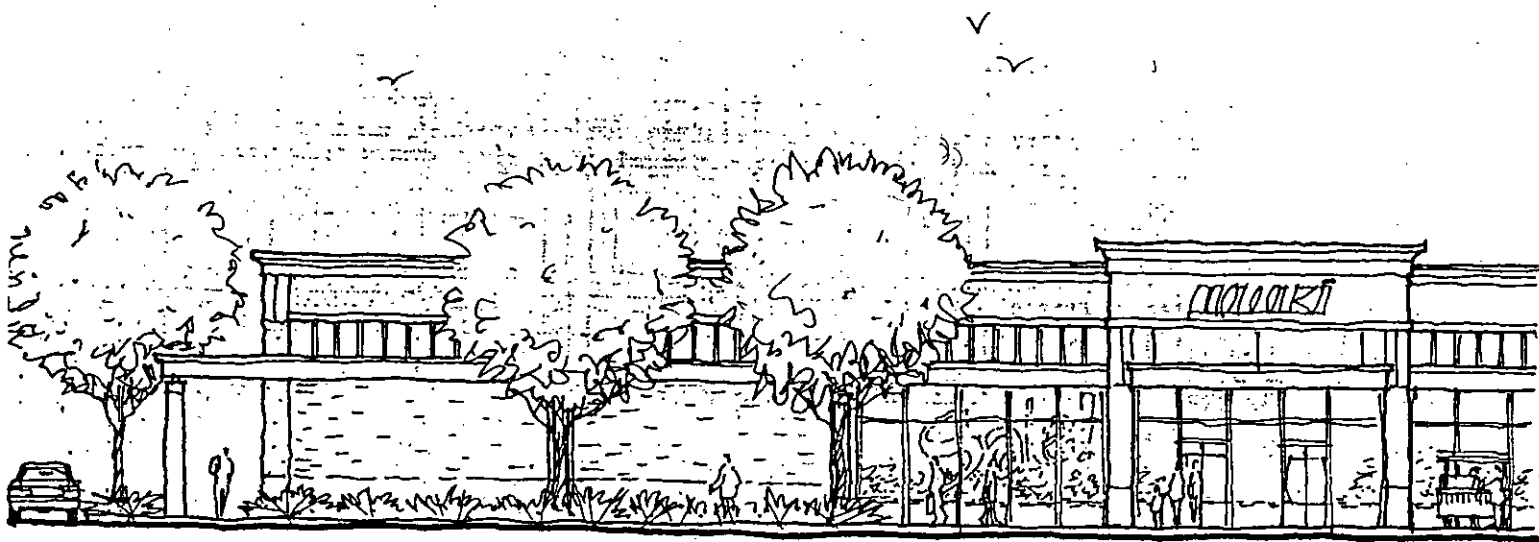


FIGURE 9



June 6, 1995 -

Comments on Town Center
Holly Plackett, Planning Commissioner

I'm voting against DGA94-004 Redmond Town Center Development Guide Amendments, as referred to in the Technical Committee Proposal dated 4/10/95. A few reasons for my decision:

The proposed development for the former Redmond Golf Course is one of the most highly contested decisions that has ever been before this city. For 18 years the residents of the City of Redmond have discussed, voted, and attempted to move forward on this issue. The vision for Redmond is intensely intertwined in the development or non-development of this piece of property.

During the seven years following the City Council's decision in 1988 to move forward with a politically touchy, carefully crafted (some would say twisted), ordinance that allowed a regional shopping mall on this property, Winmar has still developed nothing on this property.

The City Council had another opportunity in 1991 to review the proposal for this property and choose not to initial a thorough careful review of the status of the proposal or the property.

Now, in September of 1994, (11 months to the final deadline for cities in Washington state to complete their comprehensive plan to comply with the Growth Management law) Winmar produced another flurry of ideas, charts, graphs, traffic studies, and expensive brochures displaying a new proposal - an outdoor, niche-market-oriented 1.3 million square feet of shopping and office park for the city of Redmond. This proposal is to be discussed and decided on prior to July 1, 1995, and prior to the completion of Redmond's new Comprehensive Plan -- because once the new Comp Plan is in place, it will take more time to make a decision of this magnitude for the City of Redmond, as after the new Comprehensive Plan takes shape, changes to the plan can only be made once each year. Why the rush? Why the hurry? Can the Winmar proposal not stand up to a long community discussion of the plans and goals?

My first objection to the proposal is based on the events that have happened since Winmar was given the green light to develop this land. Since 1988, the City of Redmond has gone ahead with development of Target and Mervyn's to answer and solve the burning question of where residents can purchase underwear in Redmond. We will complete the movie complex at Bella-Botega this summer, so entertainment will be available in Redmond. These were two primary goals of the 1988 proposal from Winmar, and Redmond has already met these goals.

I object to this development because of the traffic congestion it will bring to downtown Redmond. The best case scenario as described by Winmar still does not answer questions of how Burlington Northern Railroad will cooperate with this development so traffic can cross from Town Center to the older part of downtown Redmond. With the planning commissions recommendations about slowing traffic on the proposed Bear Creek Parkway, and if the City Council concurs, it will only be a matter of time, when peoples' frustration rise after a year of the "planned bottleneck" that Winmar will again petition the planning commission and the city council to make the necessary widening of Leary Way, the connection to 162nd to Redmond Way and a widening of Bear Creek Parkway itself to relieve the congestion in the area. By putting development on this property, the barn door is opened. We will never be able to hold back the demands of the residents for a better flow of traffic through that area.

I further object to this development because it provides no additional housing for Redmond. If there is a single clear idea we learned from the drafting of the Comprehensive Plan is that Redmond is out of balance with jobs and housing. When this planning commission drafted the South East Redmond plan, we put housing in that area - and the City Council followed our recommendations about that housing, with some reduced numbers. But housing was included. The downtown plan calls for a wide variety of housing to be included - Winmar's proposal calls for no housing units.

I object to this development because it does not guarantee the continuation of the Saturday market nor does it provide any family or sports oriented activity on this property, other than bike trails. There is no small playground for small children, there are no batting cages for little leaguers, no performing arts stage, not even an open air chess game board is planned. Redmond is a family community and if this development is built, family oriented activities need to be included in this development.

This is a major decision for the City of Redmond. The City Council should call for a complete full review of this site and look at it after we have completed the Comprehensive Plan for Redmond. It should be looked at, discussed and reviewed in full detail, and with great input from this community. To put a development on that property, that is designed to attract 325,000 people from this region to our city every year, is a major decision and calls for extra time, extra review and extra reflection before a decision is made.